



THE CITY OF  
GREATER GEELONG

# MARSHALL DEVELOPMENT CONTRIBUTIONS PLAN

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FINAL DRAFT  
AUGUST 2023



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## **ACKNOWLEDGMENT OF COUNTRY**

The City of Greater Geelong acknowledges the Wadawurrung People as the Traditional Owners of the land, waterways and skies of the North and Western Growth Areas. We pay our respects to their Elders, past and present.

We acknowledge all Aboriginal and Torres Strait Islander people who are part of our Greater Geelong communities today.

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# SUMMARY OF CHARGES

1

# SUMMARY OF CHARGES

Table 1 provides an overview of the project categories and charges included within this development contributions plan (DCP). A more detailed explanation of apportionment, methods of calculation, and the description and costs of individual projects is included within the document.

## T1. SUMMARY OF CHARGES

### SUMMARY - NET DEVELOPABLE AREA (NDA) BY CHARGE AREA

CHARGE AREA	TOTAL COST OF CONTRIBUTION	CONTRIBUTION PER NET DEVELOPABLE HECTARE
Charge Area 1	\$60,300,432.55	\$989,683.61
Charge Area 2	\$8,471,481.01	\$858,828.16
<b>TOTAL</b>	<b>\$68,771,913.56</b>	

### SUMMARY - DEVELOPMENT INFRASTRUCTURE LEVY

PROJECTS	TOTAL COST OF PROJECTS (MCA)	CONTRIBUTION PER NET DEVELOPABLE HECTARE (NDHA)
Transport	\$4,561,238.57	\$64,430.64
Drainage	\$31,411,219.06	\$443,705.16
Open Space	\$7,972,891.35	\$130,855.44
Land	\$24,225,985.78	\$342,208.77
Planning Costs	\$600,578.80	\$8,483.59
<b>TOTAL</b>	<b>\$68,771,913.56</b>	

### SUMMARY - BREAKDOWN OF DEVELOPMENT INFRASTRUCTURE LEVY

PROJECTS	TOTAL COSTS OF PROJECTS	CONTRIBUTIONS PER NET DEVELOPABLE HECTARE (NDHA)
Land	\$24,225,985.78	\$342,208.77
Construction	\$44,545,927.78	\$647,474.83
<b>TOTAL</b>	<b>\$68,771,913.56</b>	

### SUMMARY - COMMUNITY INFRASTRUCTURE LEVY

	COMMUNITY INFRASTRUCTURE TOTAL	ESTIMATED LEVY PER DWELLING
<b>TOTAL</b>	<b>2,173,781.97</b>	<b>\$1,397.93</b>

Note - The above table provides an overview of the project categories and charges included within this Development Contributions Plan (DCP). A more detailed explanation of apportionment, methods of calculation, and the description and costs of individual projects is included within the document.

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## 2.0 INTRODUCTION

The Marshall Development Contributions Plan (DCP) has been prepared by the City of Greater Geelong with the assistance of government agencies, service authorities, technical agencies and major stakeholders.

### The DCP:

- Outlines projects required to ensure that future residents, visitors and workers in the precinct can be provided with timely access to infrastructure and services;
- Establishes a framework for development proponents to make a financial contribution towards the cost of identified infrastructure projects;
- Ensures the cost of providing new infrastructure and services is shared equitably between various development proponents and the wider community;
- Provides the details of the calculation of financial contributions that must be made by future developments towards the nominated projects; and
- Provides developers, investors and the local community with certainty about development contribution requirements and how these will be administered.

## 2.1 REPORT STRUCTURE

The DCP document comprises five parts:

<b>1</b>	<b>STRATEGIC BASIS</b>	Clearly explains the strategic basis for the DCP
<b>2</b>	<b>JUSTIFICATION</b>	Provides justification for the various infrastructure projects included in the DCP
<b>3</b>	<b>CALCULATION OF CONTRIBUTIONS</b>	Sets out how the development contributions are calculated and cost apportioned
<b>4</b>	<b>ADMINISTRATION</b>	Focuses on administration of the DCP
<b>5</b>	<b>IMPLEMENTATION</b>	Focuses on implementation of the DCP

## 2.2 STRATEGIC BASIS

The DCP has been prepared in conjunction with the Marshall Precinct Structure Plan (PSP).

### The strategic basis for the DCP is informed by:

- State and Local Planning Policy Framework as set out in the Greater Geelong Planning Scheme;
- Precinct Structure Planning Guidelines (Victorian Planning Authority, 2021)
- Infrastructure Design Manual (Local Government Infrastructure Design Association);
- Armstrong Creek Integrated Infrastructure Delivery Plan, 2009.
- K2 Planning, 2012
- Councils Social Infrastructure Plan, 2020-2023
- Library Infrastructure Development Plan, 2019 (GRLC)

- Marshall Precinct Structure Plan and supporting documents; including:
  - Marshall Transport Infrastructure Assessment, Cardno July 2019
  - Marshall Transport Impact Assessment, Ratio March 2023
  - Marshall Stormwater Management Strategy, Spiire December 2022.
  - CBRE Land Valuation Report, August 2023.
  - Spiire Finalised Quantity Surveying and Costings Package, June 2023.

These documents set out a broad, long-term vision for the sustainable development of the precinct and its surrounds, and the estimated costs of infrastructure proposed to be funded under this DCP.

## 2.3 PLANNING AND ENVIRONMENT ACT 1987

The DCP has been prepared in accordance with Part 3B of the Planning and Environment Act 1987 (the Act) and has been developed in line with the State and Local Planning Policy Framework of the Greater Geelong Planning Scheme.

It is consistent with the Ministerial Direction on development contributions plans made under section 46M(1) of the Act and has had regard to the Victorian Government's Development Contributions Plan Guidelines, Amended 2007'.

The DCP provides for the charging of a development infrastructure levy (DIL) pursuant to section 46J(a) of the Act towards works, services and facilities. It also provides for the charging of a Community Infrastructure Levy pursuant to section 46J(b) of the Act as some infrastructure projects funded by the DCP are classified as community infrastructure.

The DCP forms part of the Greater Geelong Planning Scheme as required by section 46I of the Act and is an incorporated document under the Schedule to Clause 72.04 of the Greater Geelong Planning Scheme.

The DCP is implemented in the Greater Geelong Planning Scheme through Schedule 10 to the Development Contributions Plan Overlay (DCPO10) that applies to the 'main catchment area' illustrated on [Plan 1](#).

## 2.4 MARSHALL PRECINCT STRUCTURE PLAN

Geelong has been experiencing and planning for urban growth for many years. The Greater Geelong Settlement Strategy (August 2020) describes the long-term population growth forecasts and sets a strategic direction for where new houses for this increased population will be most appropriately delivered. The Greater Geelong Settlement Strategy was adopted by Council in August 2020 and gazetted into the Planning Scheme through C395.

The PSP identifies approximately 124 hectares of land for urban development as illustrated on [Plan 1](#). The PSP sets out the vision for how land should be developed, describes the objectives to be achieved by the future development and outlines projects required to support the future community. The need for the infrastructure set out in the DCP has been determined according to the anticipated development scenario as described in the PSP.

The DCP has a strong relationship to the PSP, as the PSP provides the rationale and strategic justification for infrastructure items that have been included within the DCP. Accordingly, the DCP is an implementation-based planning tool which identifies the infrastructure items required by the new community and apportions the cost of this infrastructure in an equitable manner across the plan area. The PSP has been developed following a comprehensive planning process which establishes the future direction of development within the precinct.

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## 2.5 THE AREA TO WHICH THE DEVELOPMENT CONTRIBUTIONS PLAN APPLIES

In accordance with section 46K(1)(a) of the Act, the DCP applies to land illustrated on [Plan 2](#); this area is known as the main catchment area (MCA). The area shall be identified as DCPO 10 in the Greater Geelong Planning Scheme.

The DCP applies to 70.793 hectares Net Developable area of land.

## 2.6 DCP TIMEFRAME

For the purposes of the DCP a 20 year life has been adopted. This period commences from the date that the DCP is incorporated into the Greater Geelong Planning Scheme.

# P1. FUTURE URBAN STRUCTURE



## KEY

The Future Urban Structure Plan sets out the land uses and primary street network of the precinct.

### CONTEXT

- Precinct Boundary
- Existing trees to be retained
- Railway and Station
- Flood Overlay
- Electrical Transmission Easement
- Station Carpark - TRZ 1
- Property Boundary

### MOVEMENT

- Arterial Road
- Connector Road
- Connector Road (Modified)
- Local Access Street
- Station Precinct Road (Refer to Concept Plan)
- Local Access Street (No through Road)
- Road Truncation
- Major Road Project
- Signalled Crossing


### LAND USE

- Conventional Residential
- Medium/High Density Residential
- Mixed Use - High Density Residential
- Road Reserve
- Commercial/Bulky Goods
- Credited (Unencumbered) Open Space
- Conservation Reserve
- Utilities Easements
- Drainage Reserve
- Constructed Waterway (Marshall Creek)

## P2. MAIN CATCHMENT AREA



### KEY

 Main Catchment Area

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## 3.0 INFRASTRUCTURE PROJECT JUSTIFICATION

The need for infrastructure included in the DCP has been determined on the basis of the development scenario as described in the PSP and its supporting documents. Items can be included in a DCP if the proposed development of an area is likely to create the need for infrastructure by its future community. New development does not have to trigger the need for new items in its own right. Furthermore, an item can be included in a DCP regardless of whether it is within or outside the DCP area.

Before inclusion in the DCP, all items have been assessed to ensure they have a relationship or nexus to proposed development in the PSP. The cost apportionment methodology adopted in the DCP relies on the nexus principle. A new development is deemed to have a nexus with an item if it is expected to make use of that item.

**The items that have been included in the DCP all have the following characteristics, namely that they:**

- Are essential to the health, safety and wellbeing of the community;
- Will be used by a broad cross-section of the community;
- Reflect the vision and strategic aspirations expressed in the PSP;
- Are not recurrent items; and
- Are the basis for the future development of an integrated network.

## 3.1 DEVELOPMENT INFRASTRUCTURE

### 3.1.1 ITEMS NOT INCLUDED IN THE DEVELOPMENT CONTRIBUTIONS PLAN (DEVELOPER WORKS)

Any items required for the development of the PSP that are not specifically funded through the DCP must be delivered as developer works. These works must be provided by developers as a part of the development of land pursuant to planning permits and/or pursuant to agreements with servicing agencies in implementing the PSP.

#### Developer works not funded by the DCP include:

- Connector streets and local streets (except where nominated in the DCP);
- Intersection works and traffic management measures along arterial roads, connector streets and local streets (except where nominated in the DCP);
- Local bus stop infrastructure (where locations have been agreed in writing by Public Transport Victoria);
- Landscaping of all existing and future roads and local streets;
- Local shared, pedestrian and bicycle paths along local streets, connector streets, utilities easements and waterways including bridges, intersections, and barrier crossing points (except those associated with other DCP items);
- All preparation earthworks associated with the local parks;
- Council approved fencing and landscaping along arterial roads, the railway corridor and shared paths, as required;
- Bicycle parking;
- Appropriately scaled lighting along all roads, major shared and pedestrian paths, and traversing the open space network;
- Local street or path crossings of waterways, unless included in the DCP or outlined as the responsibility of an agency in the PSP;
- Infrastructure as required by utility services providers, including water, sewerage, electricity, gas and telecommunications; and
- All designated conservation reserves except if located in existing or future redundant Council Road Reserves which are to contain key pedestrian corridors.

The items listed above are considered to be normal to the construction of a development and are not considered to warrant cost sharing arrangements beyond those set out in the DCP.

They may be further addressed and defined by an agreement under Section 173 of the Act and/or conditions in planning permits.

Upgrade of the existing adjoining road network to an urban standard will be implemented through subdivision permit conditions to the satisfaction of the responsible authority, except where specified as a DCP project.

### 3.1.2 COMMUNITY INFRASTRUCTURE VS. DEVELOPMENT INFRASTRUCTURE

In accordance with the Planning and Environment Act (1987) and the Minister's Direction on Development Contributions, the DCP is required to make a distinction between "development" and "community" infrastructure.

Items of infrastructure of a community or social nature are to be classified as community infrastructure, whilst all other infrastructure required under the DCP (being types of works set out in clause 5 of the Minister's Direction on Development Contributions) are classified as development infrastructure.

## 3.2 PROJECT IDENTIFICATION

The DCP uses a project identification system of project category and sequential number in its tables and plans.

The following types of project categories are included in the DCP:

### DEVELOPMENT INFRASTRUCTURE (DI)

Transport projects	IT – Intersections
Open Space Projects	OS – Local Parks and Linear Open Space
Drainage Projects	DR – Drainage Infrastructure Item
Land Items	LA – Land

### COMMUNITY INFRASTRUCTURE (CI)

Community Infrastructure	CF – Community Facilities OS – Open Space Community Infrastructure
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## 3.2.1 TRANSPORT PROJECT

The PSP outlines an expanded urban structure intended to support the future residential growth of the precinct, including connector streets, and local streets on a grid adjusted to meet the existing constraints of the area. Where the precinct requires a new or upgraded intersection on the periphery of the Marshall Precinct, the costs associated with the intersection have been included in the DCP and apportioned accordingly. The DCP will fund all the land required for these intersections within the Marshall Precinct. As identified within the Marshall Traffic Impact Assessments (2019) and (2023) the East/West and Reserve Connector Roads are the main internal roads within the precinct. However, due to the fragmented nature of land holdings within the precinct there are implementation risks associated with these roads. As a result, all land associated with the East/West and the Reserve Connector Roads beyond that of the existing road reserves will be included within the DCP. If required, this will enable Council to step in and acquire land holdings that cannot be purchased privately. The construction of the Connector Road network however is to remain developer works (and not funded via the DCP), as standardly applied in DCP's.

Shared user paths that serve the broader movement and access network in the Marshall precinct will only be included in the Development Contributions Plan (DCP) when they are associated with other DCP infrastructure items. Specifically, shared user paths will be included within the DCP when they intersect or are connected to other infrastructure items such as local parks, linear open spaces, and drainage items.

### The transport projects also include:

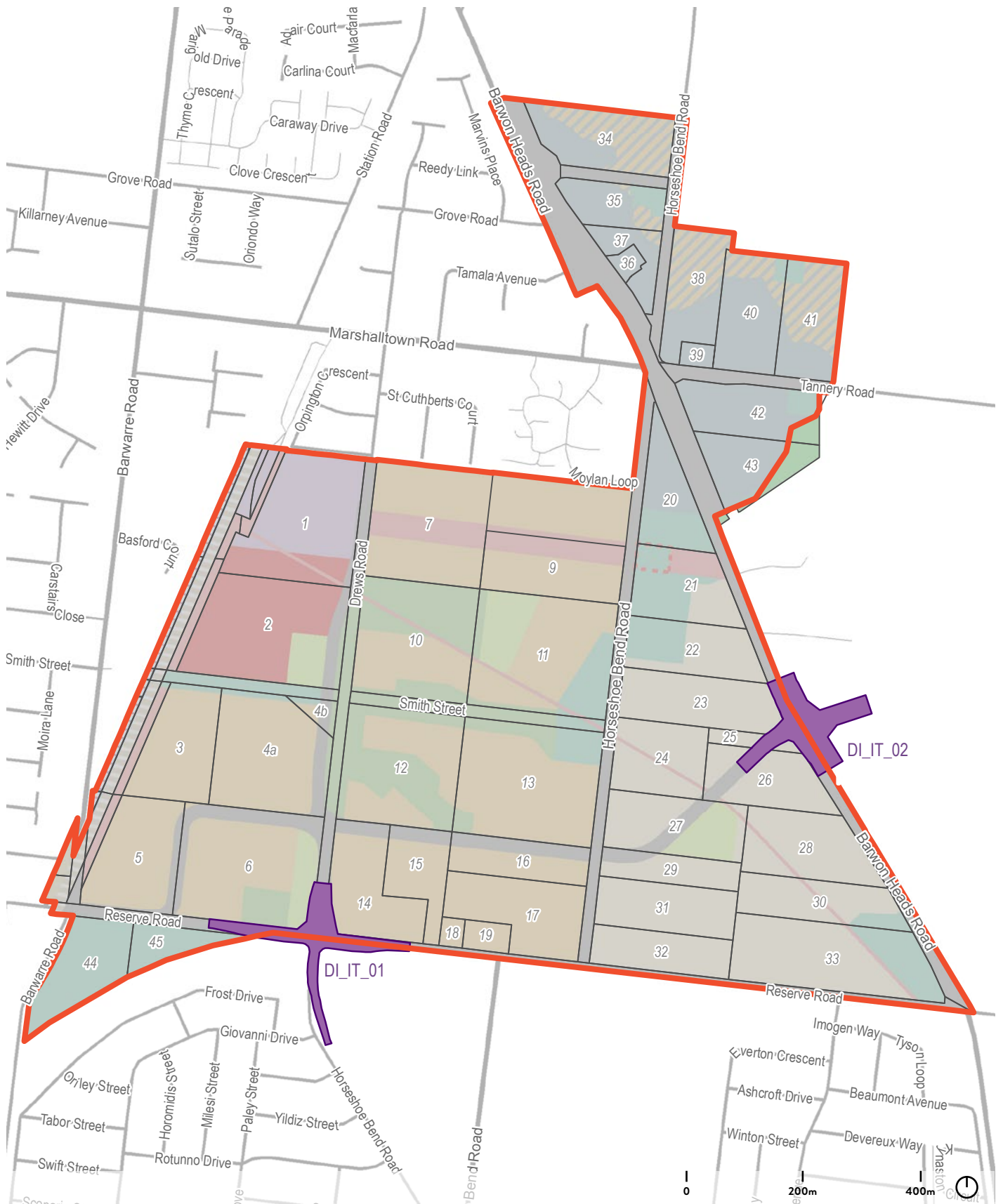
- Construction of controlled intersections with the existing road network bordering the precinct and associated works including bridge or culvert works across waterways / drainage catchments
- Connections between the new development and the existing shared-path network.
- Provision of land allocation to facilitate road construction listed in the DCP and consistent with the relevant cross-sections outlined in the PSP.

These projects are shown in [Plan 3](#) and described in [Table 2](#).

## T2. TRANSPORT PROJECTS

DCP PROJECT NUMBER/S	PROJECT DESCRIPTION	INDICATIVE PROVISION TRIGGER	STRATEGIC JUSTIFICATION
DI_IT_01	Construction associated with the Drews Road/Horseshoe Road and Reserve Road 4 way signalised intersection.	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Intersection Identified in transport and engineering planning as required for appropriate traffic management as identified within the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
DI_LA_13	Land required (2.03 ha) for the construction of DI_IT_01 and the Bellarine Link Intersection - Property 45 (0.014 ha), Property 6 (0.078 ha), Property 14 (0.111 ha)	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Land required for intersections as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
DI_IT_02	Construction associated with the East-West Connector/ Barwon Heads Road/NEIP 4 way signalised intersection	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Intersection Identified in transport and engineering planning as required for appropriate traffic management as identified within the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
DI_LA_14	Land required (0.311 ha) for the construction of DI_IT_02 - property 23 (0.071ha), property 25 (0.121 ha), property 26 (0.119 ha)	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Land required for intersections as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
DI_LA_15	Land required (0.430 ha) for the Reserve Road and E/W Connector Road Roundabout - property 4a (0.079 ha), property 6 (0.245 ha), property 12 (0.007 ha), property 14 (0.109 ha)	The first subdivision requiring the associated road section to be delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Land required to facilitate the delivery of the road network within the Marshall Precinct as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
DI_LA_16	Land required (2.655 ha) for the East/West Connector Road - property 5 (0.275ha), property 6 (0.628 ha), property 14 (0.130 ha), property 15 (0.266 ha), property 16 (0.607 ha), property 24 (0.004ha), property 26 (0.204 ha), property 27 (0.347 ha), property 29 (0.194 ha)	The first subdivision requiring the associated road section to be delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Land required to facilitate the delivery of the road network within the Marshall Precinct as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
DI_LA_17	Land required (0.839 ha) for the Drews Road Widening/Realignment - property 1 (0.110ha), property 2 (0.361ha), property 6 (0.005 ha), property 14 (0.009 ha), property 4a (0.248 ha), Property 4b (0.106 ha)	The first subdivision requiring the associated road section to be delivery in accordance with the Ratio Transport Impact Assessment, 2023)	Land required to facilitate the delivery of the road network within the Marshall Precinct as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)

# P3. TRANSPORT PROJECTS

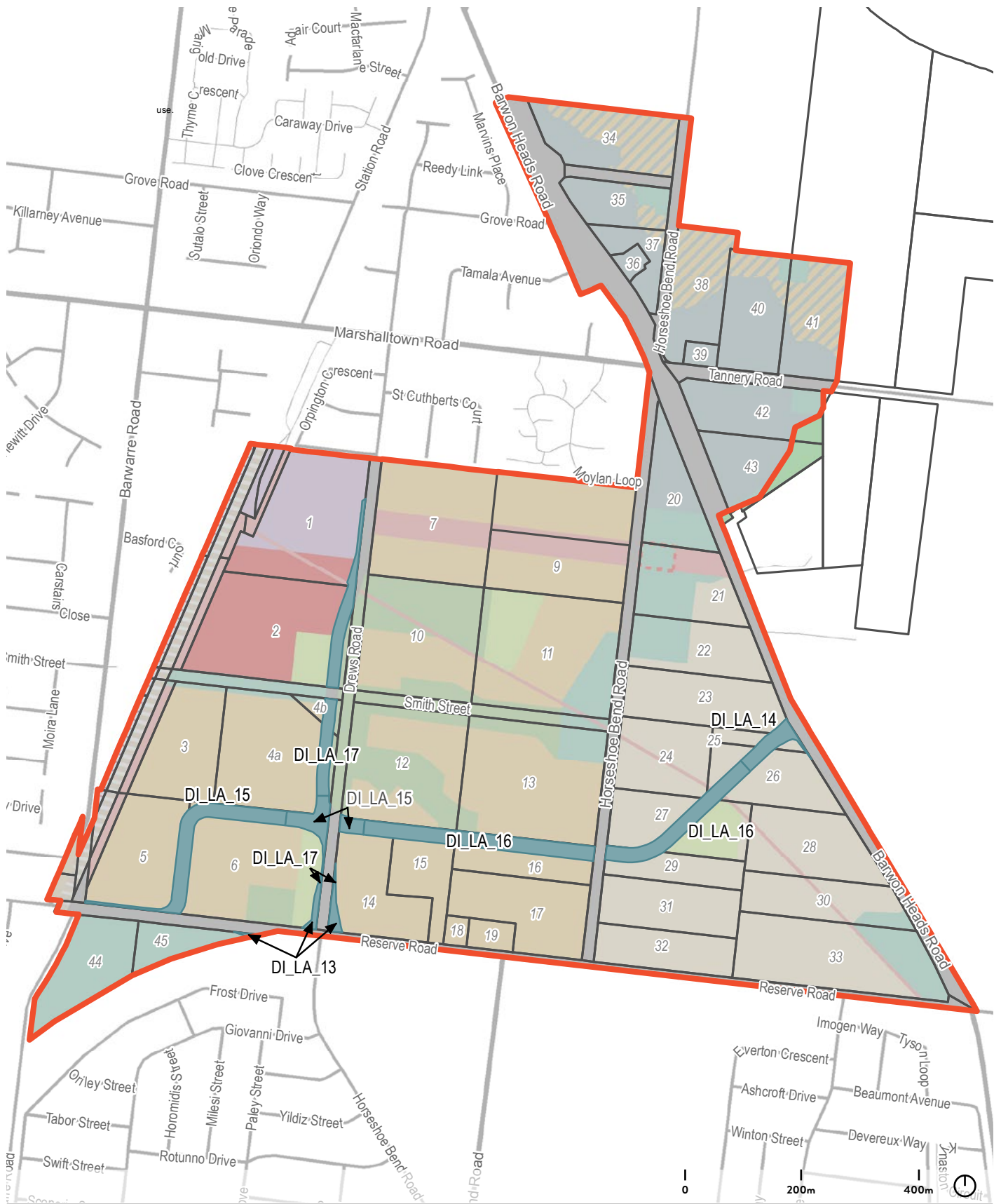


## KEY

- Main Catchment Area
- 13 Property Number

- Transport Project

# P4. TRANSPORT LAND ITEMS



## KEY

- Main Catchment Area
- 13 Property Number

- Transport Land Item

## 3.2.2 DRAINAGE PROJECTS

The DCP makes funding available for the construction of all major drainage infrastructure. In order to address the challenges posed by the fragmented nature of the Marshall precinct and promote the optimal development of the landholdings constrained by the drainage infrastructure, all types of drainage infrastructure as contained within the Marshall PSP are included within the DCP.

The drainage infrastructure has been strategically identified and designed as contained within the Marshall Stormwater Water Management Strategy prepared by Spiire and dated December 2022.

The drainage infrastructure is required to appropriately retard and treat stormwater flows from new urban development, in accordance with best practice principles to the satisfaction of the Responsible Authority and CCMA.

### In summary the drainage projects include:

- Land for and construction of wetlands, sediment basins and waterways.
- Channel works
- Piped drains
- Culverts
- Shared User Paths that serve the broader Marshall Movement and Access Network and are located integrated in with the relevant drainage asset.

These projects are described in [Table 3](#) and shown in [Plan 5](#).

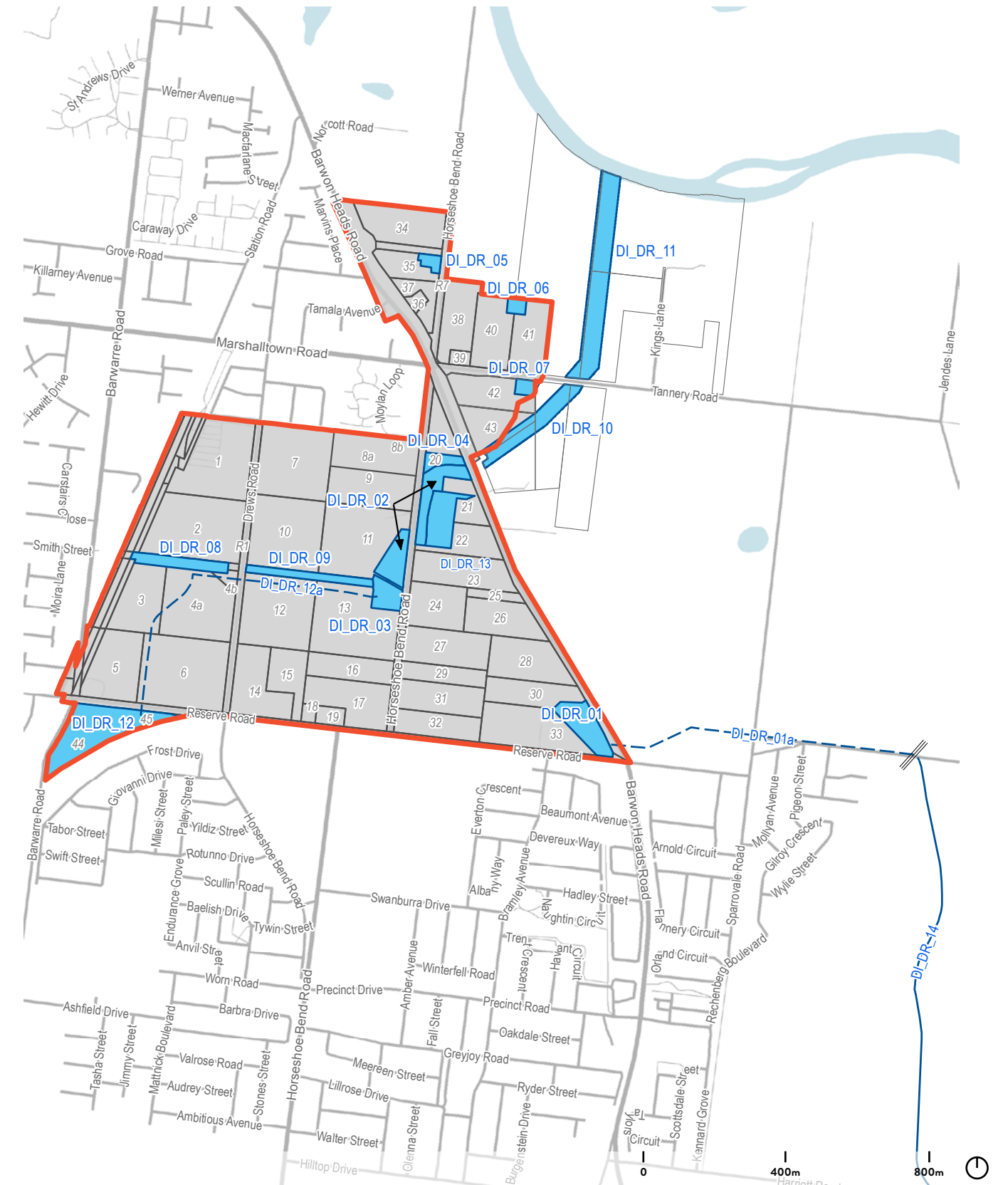


### T3. DRAINAGE PROJECTS

DCP PROJECT NUMBER	PROJECT DESCRIPTION	INDICATIVE PROVISION TRIGGER	STRATEGIC JUSTIFICATION
DI_DR_01	Construction of a sediment pond, wetland and retarding basin (WLRB02).	The first subdivision/development within South East Catchment K	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_1	Land required (1.190ha) for the Construction of WLRB02 - property 30 (0.208ha) and property 33 (0.982ha)	The first subdivision/development within South East Catchment K	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_01a	Construction of a piped Drainage Main in Between WLRB02 and WW06	The first subdivision/development within South East Catchment K, or to the satisfaction of the Responsible Authority	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_02	Construction of a Waterway from north of SB01 to Barwon Heads Road (WW03).	The first subdivision within Catchment A, B, C, D, E and F, whichever comes first.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_2	Land required (1.879 ha) for the construction of WW03 - property 11 (0.823 ha), property 20 (0.373 ha), property 21 (0.479 ha), property 22 (0.204 ha)	The first subdivision within Catchment A, B, C, D, E and F, whichever comes first.	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_03	Construction of Sediment Basin and Siphon and associated infrastructure (SB01) and portion of SUP04 surrounding the Basin	The first subdivision within catchment C or D, which ever comes first.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_5	Land required (0.501 ha) for the construction of SB01 - property 11 (0.026 ha), property 13 (0.475 ha)	The first subdivision within catchment C or D, which ever comes first.	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_04	Construction of the Sediment Basin, Waterway and associated infrastructure (SB02) and the portion of SUP02 surrounding the drainage basin.	The first subdivision within catchment A, F or B, whichever comes first	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_3	Land required (0.370 ha) for the construction of SB02 - property 20 (0.370 ha)	The first subdivision within catchment A, F or B, whichever comes first	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_05	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB01)	The first subdivision/development within Catchment G.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_6	Land required (0.243 ha) for the construction of SBRB01 - property 35 (0.253)	The first subdivision/development within Catchment G.	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_06	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB02)	The first subdivision/development within Catchment H.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_7	Land required (.231 ha) for the construction of SBRB02 -property 40 (0.096 ha), property 41 (0.135ha)	The first subdivision/development within Catchment H.	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_07	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB03)	The first subdivision/development within Catchment J.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_8	Land required (0.237 ha) for the construction of SBRB03 - property 42 (0.237 ha)	The first subdivision/development within Catchment J.	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.

DCP PROJECT NUMBER	PROJECT DESCRIPTION	INDICATIVE PROVISION TRIGGER	STRATEGIC JUSTIFICATION
DI_DR_08	Construction of Waterway inbetween the Rail Corridor and Drews Road (WW01), including the construction of SUP8	The first subdivision within Catchment C or D, whichever comes first.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_9	Land required (0.258 ha) for the construction of WW01 - property 3 (0.090 ha), property 4a (0.116 ha), property 4b (0.052 ha)	The first subdivision within Catchment C or D, whichever comes first.	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_09	Construction of Waterway abutting the Smith Street Greenway, including bi pass drain (WW_02).	The first subdivision within catchment C or D, whichever comes first.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_10	Construction of Waterway in between Barwon Heads Road and Tannery Road (WW04)	Upon 50% of the Marshall Catchment Developing	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_10	Land required (1.476 ha) for the construction of WW04 - Property 43 (0.288 ha), 401-419 Barwon Heads Road, Charlemont (0.502 ha), 30 Tannery Road, Charlemont (0.595 Ha), 46 Tannery Road, Charlemont (0.091 Ha)	Upon 50% of the Marshall Catchment Developing	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_11	Construction of the waterway in between Tannery Road and The Barwon River (WW05)	Upon 50% of the Marshall Catchment Developing	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_11	Land required (3.253 ha) for the construction of WW05 - 31-39 Tannery Road, Charlemont (0.392 ha), 41-49 Tannery Road, Charlemont (1.472 Ha), 81-89 Tannery Road, Charlemont (1.389 Ha)	Upon 50% of the Marshall Catchment Developing	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_11	Land required (3.253 ha) for the construction of WW05 - 31-39 Tannery Road, Charlemont (0.392 ha), 41-49 Tannery Road, Charlemont (1.472 Ha), 81-89 Tannery Road, Charlemont (1.389 Ha)	Upon 50% of the Marshall Catchment Developing	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_12	Construction of a Retarding Basin and associated infrastructure (WLRB03), including shared paths SUP_06 (portion) and SUP07	First subdivision within catchment D or to the satisfaction of the Responsible Authority	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_12	Land required (2.931 ha) for the construction of WLRB03 - property 44 (2.054ha), property 45 (.877 ha)	First subdivision within catchment D or to the satisfaction of the Responsible Authority	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_12a	Construction of the piped drainage main in Between WLRB03 and SB01	First subdivision within catchment D or to the satisfaction of the Responsible Authority	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_13	Construction of a Wetland and Sediment basin (SBWL01), including the construction of a portion of SUP02 (3m)	The first subdivision within catchment E	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_LA_4	Land required (1.171 ha) for the construction of SBWL01 - property 21 (0.634 ha), property 22 (0.537 ha)	The first subdivision within catchment E	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
DI_DR_14	Construction of the waterway outfall within the Sparrowvale wetlands (WW06)	The first Subdivision within Catchment K or to the satisfaction of the Responsible Authority.	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.

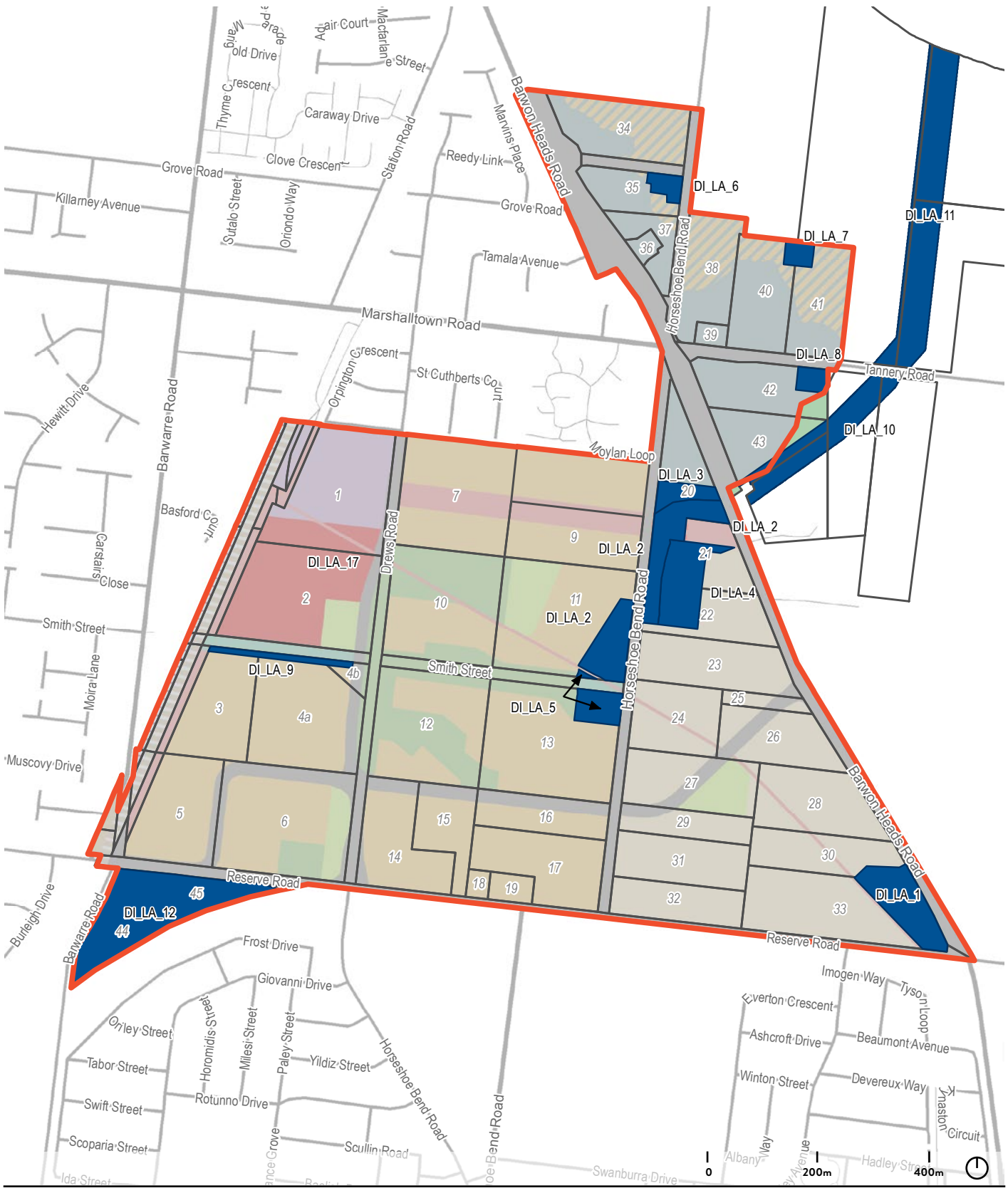
# P5. DRAINAGE PROJECTS



## KEY

- Main Catchment
- Proposed Main Drain – Indicative Location
- Waterway Drainage Line
- Drainage Item
- 13 Property Number

# P6. DRAINAGE LAND ITEMS



- KEY**
- Main Catchment
  - Drainage Land Item
  - 13 Property Number

### 3.2.3 OPEN SPACE PROJECTS

The inclusion of local parks in the Development Contributions Plan (DCP) is required to ensure that their development and maintenance align with the broader needs of the catchment area and the fragmented landholdings within the precinct. This is a fair and equitable development outcome within the Marshall Precinct.

Typically, only local park playground equipment has been included in previous Development Contribution Plans in the Armstrong Creek Growth Area. However, in this case, the DCP for the Marshall Precinct will also encompass the landscape embellishment associated with the local park items, due to the broader catchment that the infrastructure services. This means that the funding for landscaping elements within the local parks will be included in the DCP. The planning and preparation works required to implement the local park items will still remain the responsibility of the developers.

Furthermore, the shared user paths that serve the greater access and movement network will be incorporated within the respective local park item where they intersect. This ensures that the shared user paths, which facilitate connectivity and accessibility within the precinct, are appropriately accounted for in the DCP.

The land associated with the local parks will not be included in the DCP, instead the open space land requirements will be included within Schedule 1 of 53.01 of the Greater Geelong Planning Scheme and subject to equalisation within the Marshall PSP.

#### **In summary the open space projects include:**

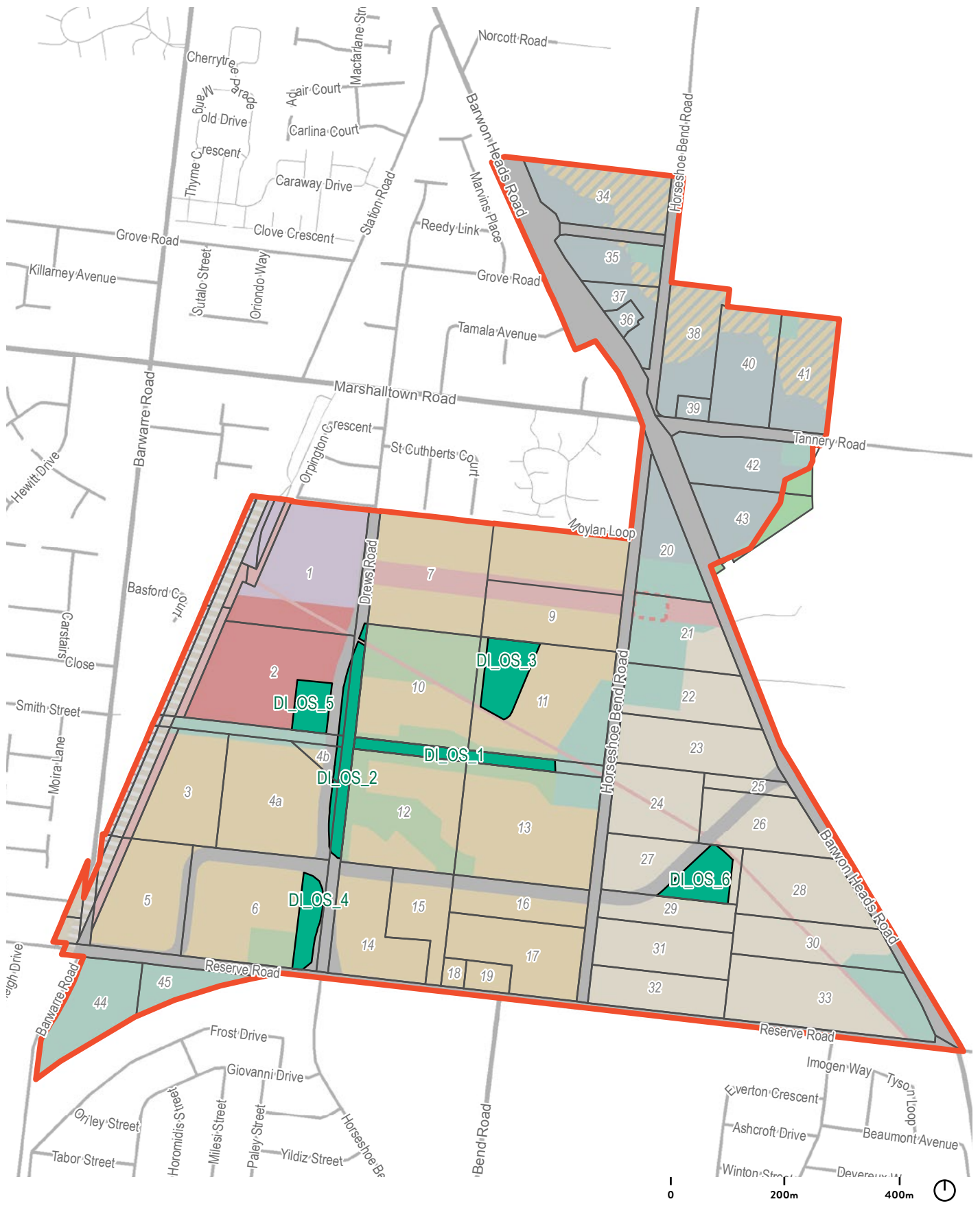
- Playground equipment and landscape embellishment of open space for the four local parks.
- Upgrade Works Required for both the Drews Road and Smith Street Greenways, including the integrated Shared User paths which are located in the relevant Local Park.

The open space projects funded by the DCP are shown on [Plan 7](#) and described in [Table 4](#).

## T4. OPEN SPACE PROJECTS

DCP PROJECT NUMBER/S	PROJECT DESCRIPTION	INDICATIVE PROVISION TRIGGER	STRATEGIC JUSTIFICATION
DI_OS_1	Linear Openspace - Smith Street Greenway	The subdivision and development which requires the upgrades to WW02	Greenways identified in the Precinct Structure Plan to provide linkages between open space, community facilities, residential and commercial areas and protection of native vegetation.
DI_OS_2	Linear Openspace - Drews Road Greenway	The subdivision and development which triggers the ultimate realignment of Drews Road.	Greenways identified in the Precinct Structure Plan to provide linkages between open space, community facilities, residential and commercial areas and protection of native vegetation.
DI_OS_3	Local Park - Northern Park Adjacent to Conservation - Landscaping Embellishment and Playground Equipment and a section of SUP4	The subdivision and or development on the parcel in which contains the local park	Local Park openspace embellishments required to serve the Marshall Residential Catchment as identified within the Marshall PSP
DI_OS_4	Local Park - Southern Park Separated by Conservation - Landscaping embellishment and Playground Equipment	The subdivision and or development on the parcel in which contains the local park	Local Park openspace embellishments required to serve the Marshall Residential Catchment as identified within the Marshall PSP
DI_OS_5	Local Park - Station Precinct Park - Landscaping Embellishment and Playground Equipment	The subdivision and or development on the parcel in which contains the local park	Local Park openspace embellishments required to serve the Marshall Residential Catchment as identified within the Marshall PSP
DI_OS_6	Local Park - East West Connector - Landscaping Embellishment, Playground Equipment and a section of SUP5	The subdivision and or development on the parcel in which contains the local park	Local Park openspace embellishments required to serve the Marshall Precinct residential catchment as identified within the Marshall PSP

# P7. OPEN SPACE PROJECTS



## KEY

 Main Catchment Area

 13 Property Number

 Open Space Item

## 3.2.4 COMMUNITY PROJECTS

The Marshall PSP has identified that there is no requirement for new public community facilities to be developed within the Precinct. However, two existing Armstrong Creek PSP/DCP's include projects that serve a wider catchment that includes the Precinct. These infrastructure items are as follows:

### 1. Armstrong Creek Town Centre Development Contributions Plan (DCP06):

- CI\_CF\_01 – Regional Library: This item refers to the inclusion of a regional library within the community infrastructure. The details of this library, such as its size, services, and location, is outlined in the Armstrong Creek Town Centre Development Contributions Plan.
- In the Armstrong Creek Integrated Infrastructure Delivery Plan of 2009, the library is identified as a regional infrastructure item intended to serve the entire Armstrong Creek Growth Area. As the inclusion of the library in the ACTC PSP occurred after the implementation of other precincts within Armstrong Creek, except for Marshall, no additional community infrastructure contribution has been made to date, apart from the ACTC DCP's Community Infrastructure Levy for a financial 'cash contribution'.
- To address this, the Marshall DCP will incorporate a community infrastructure levy to generate a cash contribution towards the DI\_CF\_01 (Regional Library). The contribution from Marshall has been determined through comparing the forecasted dwelling count for Marshall (1,555) by the forecasted dwelling count for the entire Armstrong Creek Growth Area (23,054) to which the Library is servicing. As of consequence the Marshall DCP will include the Library as a Community Infrastructure Item and will be apportioned 6.74% of the updated Construction cost of the Library.

### 2. Horseshoe Bend Development Contributions Plan (DCPO5):

- CI\_OS\_1 – Regional Active Open Space – Community Pavilion - North: This item pertains to the provision of an open space pavilion in the Regional Active Openspace, specifically in the northern region of the Horseshoe Bend Precinct. The allocation for this pavilion is split, with 80% designated for Horseshoe Bend and 20% for the Marshall Precinct.
- The Marshall DCP will include a Community Infrastructure Levy associated with 20% of the community pavilion item.

Both the regional library and the open space pavilion will be considered as Community Infrastructure Items within the Development Contributions Plan (DCP) for the Marshall Precinct.

## T5. COMMUNITY INFRASTRUCTURE

<b>DCP PROJECT NUMBER/S</b>	<b>PROJECT DESCRIPTION</b>	<b>INDICATIVE PROVISION TRIGGER</b>	<b>STRATEGIC JUSTIFICATION</b>
CI_CF_01	Library - construction (contribution) - Contribution to the construction of the regional library in the Armstrong Creek Town Centre	Refer to ACTC DCP CI_CF_01 Provision Trigger . Currently under construction.	Contribution to community facilities required to serve the new resident population in line with the Armstrong Creek Integrated Infrastructure Delivery Plan 2009, the K2 Planning Report 2012, CoGG Social Infrastructure Plan 2020-2023 and Library Infrastructure Development Plan, 2019 (GRLC).
CI_OS_01	Regional Active Open Space (northern) - Community Pavillion	Please refer to HSB DCP DI_OS_01 Provision Trigger.	Horseshoe Bend PSP Regional Openspace infrastructure in which services the Marshall Precinct

## 3.3 PROJECT TIMING

Each item in the DCP has an assumed indicative provision trigger specified in [Tables 2–5](#). The timing of the provision and the items in the DCP are consistent with information available at the time the DCP was prepared.

City of Greater Geelong is the development agency as well as the collecting agency, and will monitor and assess the required timing for individual items and have regard to its capital works program.

### **The collecting agency may consider alternatives to the priority delivery of works or land where:**

- Infrastructure is to be constructed / provided by development proponents as works or land in kind, as agreed by the collecting agency
- Network priorities require the delivery of works or land to facilitate broader road network connections
- Community needs determine the delivery of works or land for open space and associated facilities.

All items in the DCP will be provided when practicable when sufficient contributions are available, consistent with [Section 4.1](#) and acknowledging the development agency's capacities to provide the balance of funds not recovered by the DCP.

Contributions are to be made by developers at the time of subdivision. If subdivision is not applicable, payments must be made prior to construction of buildings and works (refer to [Section 5.1](#)).

# CALCULATION OF CONTRIBUTIONS

4.1	Calculation of Net Developable Area, Charge Areas and Demand Units	35
4.1.1	Net Developable Area and Land Budget	35
4.1.2	Demand Units and Charge Areas	38
4.2	Calculation of Contributions	40
4.2.1	Calculation of Project Costs	40
4.2.2	Land Value	40
4.2.3	DCP and PSP Preparation	41
4.2.4	Net Developable Area	41



# 4.1 CALCULATION OF NET DEVELOPABLE AREA, CHARGE AREAS AND DEMAND UNITS

The following section sets out how the net developable area (NDA) is calculated outlines the development projections anticipated in the precinct along with establishing the Demand Units and Charge Areas associated with the precinct.

## 4.1.1 NET DEVELOPABLE AREA AND LAND BUDGET

In the DCP, all development infrastructure contributions are payable on the net developable area of land on any given development site. Calculations of NDA for each individual property is outlined in the property-specific land budget included at [Appendix A](#).

For the purposes of the DCP the NDA is defined as the total amount of land within the precinct that is made available for development. It is the total precinct area minus open space and encumbered land. NDA includes any land for lots, housing and employment buildings and all local streets.

[Table 6](#) identifies a summary land budget of the Marshall Precinct, which identifies a total of 70.827 Net developable hectares. [Table 7](#) identifies that the NDA is divided into three separate distinct areas based on future land use, being the Station Precinct, Commercial and Bulky Goods Precinct and the Residential area associated with the Marshall Precinct.

The 'per Net Developable Hectare' contributions will not and must not be amended to respond to minor changes to land budgets that may result from the subdivision process. In other words, the DCP is permanently linked to the calculation of Net Developable Area set out in the detailed land budget in [Table 6](#).

## T6. NDA BREAKDOWN

DESCRIPTION	HECTARES	% OF NDA	DWELLINGS
Residential (Medium and Standard)	56.440	79.73%	1330
Station Precinct	4.489	6.34%	224
Commercial/Bulky Goods	9.864	13.93%	0
<b>TOTAL</b>	<b>70.793</b>	<b>100%</b>	<b>1555</b>

## T7. SUMMARY LAND USE BUDGET

DESCRIPTION	HECTARES	% TOTAL SITE
<b>TOTAL PRECINCT AREA</b>	<b>122.960</b>	<b>100%</b>
<b>TRANSPORT</b>		
Arterial Road	5.949	4.84%
Arterial Road Intersection (Barwon Heads Road)	0.311	0.25%
Arterial Road Intersection (Bellarine Link)	0.576	0.47%
Arterial Road Intersection (HSB/Drews/Reserve Road)	0.251	0.20%
Connector Road Intersection	0.619	0.50%
Drews Rd Connector Road	1.390	1.13%
E/W Connector road	2.707	2.20%
Existing Rail Reserve	1.780	1.45%
Part of Transport or Barwon Water	0.691	0.56%
Retained Existing Road Reserve	5.322	4.33%
Transport Hub	3.205	2.61%
<b>SUBTOTAL TRANSPORT</b>	<b>22.803</b>	<b>18.54%</b>
<b>UNCREDITED OPENSACE</b>		
Barwon Water Easements Adjacent to Rail Reserve	1.173	0.95%
Conservation	7.538	6.13%
Flood Overlay Land	4.876	3.97%
High Voltage Electricity Utility Easement	2.275	1.85%
Main Outfall Sewer Utility Easement	0.807	0.66%
Drainage (Within Existing Road Reserve)	0.699	0.57%
Drainage (Within High Voltage Electricity Utility Easement)	0.246	0.20%
Waterway, Wetland And Drainage Reserve	8.847	7.20%
<b>SUBTOTAL UNCREDITED OPENSACE</b>	<b>26.461</b>	<b>21.52%</b>
<b>CREDITED OPENSACE</b>		
Local Parks	2.903	2.36%
<b>SUBTOTAL CREDITED OPENSACE</b>	<b>2.903</b>	<b>2.36%</b>
<b>NDA</b>		
<b>TOTAL NDA</b>	<b>70.793</b>	<b>58%</b>

## 4.1.2 DEMAND UNITS AND CHARGE AREAS

### DEMAND UNITS

Within this DCP, each 'net developable hectare' corresponds to one demand unit for the Development Infrastructure Levy, while each dwelling corresponds to one demand unit for the Community Infrastructure Levy. The cumulative number of demand units is displayed in [Table 8](#).

All development (residential, station precinct and commercial) contributes to drainage, transport projects and planning items included in the DCP. Only the residential and station precinct development contributes to open space and community projects included in the DCP.

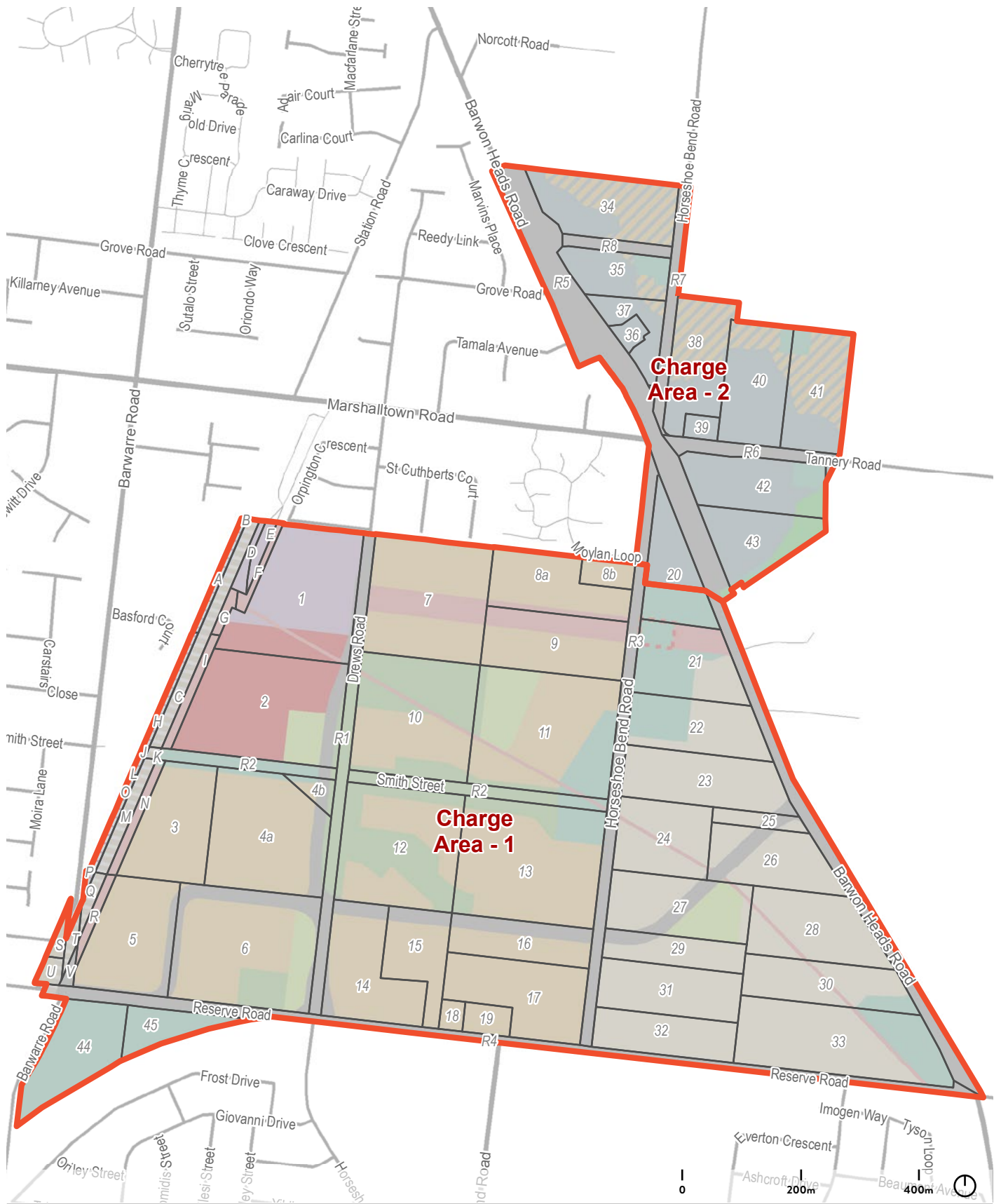
### CHARGE AREAS

The Main Catchment Area has been divided into two charge areas. These charge areas have been established to account for the different land use classifications, geographical locations and the varying need and nexus associated with the relevant Infrastructure items included in the DCP. [Table 7](#) displays the number of demand units (Net Developable Area and residential dwellings) within each charge area, while [Plan 8](#) indicates the respective location of each charge area.

## T8. DEMAND UNITS

CHARGE AREA	DESCRIPTION	DEVELOPMENT INFRASTRUCTURE LEVY (HA)	COMMUNITY INFRASTRUCTURE LEVY (DWELLINGS)
Charge Area 1	Residential Land and Station Precinct	60.929	1555
Charge Area 2	Commercial/Bulky Goods	9.864	0
<b>TOTAL</b>		<b>70.793</b>	<b>1555</b>

# P8. LOCATION OF CHARGE AREAS



## KEY

13 Property Number

## 4.2 CALCULATION OF CONTRIBUTIONS

### 4.2.1 CALCULATION OF PROJECT COSTS

Each infrastructure project has been assigned a land and/or construction cost as required, as listed in [Table 9](#) (Development Infrastructure) and [Table 10](#) (Community Infrastructure). Note that the land items and construction items have been listed separately within [Table 8](#).

**The costs are expressed in 2023 dollars and will be adjusted annually in accordance with the method specified in [Section 5.2](#). The Development Infrastructure costs associated with the construction items to be relied on have been undertaken:**

- Development Infrastructure Projects (Intersection, Drainage and Open space) costs have been determined by Spiire (refer to [Table 8](#) and [9](#) and [Appendix B](#) for cost sheets).
- Community Infrastructure projects have been taken from the respective original DCP they are contained within and indexed to 2023 values.

### 4.2.2 LAND VALUE

The area of land to be acquired for each DCP project on each property was identified from the property specific land budget prepared for the PSP. A description of the precinct land area was provided to a registered valuer who then prepared a valuation on a 'before and after method'. The valuation of the relevant property takes into consideration any underlying land encumbrances and existing conditions that may impact the land value. These encumbrances and existing conditions have been considered to assist in determining the overall value of the property and subsequently the value of the individual land items.

### 4.2.3 DCP AND PSP PREPARATION

In addition to the items described above, the costs incurred by Council in preparing the DCP and PSP have also been included as a project.

### 4.2.4 NET DEVELOPABLE AREA

In this DCP, the Development Infrastructure Levy is payable based on the NDA of land on any given development site as derived from the property specific land budget included in the [Appendix A](#). The NDA is expressed in Hectares.

The 'Net Developable Area per Hectare' contributions will not and must not be amended to respond to minor changes in land budgets that may result from the subdivision process. In other words, the DCP is permanently linked to the calculation of Net Developable Area set out in [Table 9](#).

The property specific land budget included in the PSP is to be used to determine the number of Net Developable Hectares on individual properties. Calculations of NDA for each individual property is outlined in [Appendix A](#).

## T9. CALCULATION OF COSTS – DEVELOPMENT INFRASTRUCTURE LEVY (DIL)

### DRAINAGE

DCP ID	DCP TYPE	PROJECT DESCRIPTION	CONSTRUCTION/ LAND COST	EXTERNAL AP- PORTIONMENT	COST MCA	CHARGE AREAS	DEMAND UNITS	LEVY PER HA	CHARGE AREA 1 LEVY/HA (STATION PRE- CINCT AND RESIDENTIAL)	CHARGE AREA 2 LEVY/HA (COMMER- CIAL)
DI_DR_01	Development	Construction of a sediment pond, wetland and retarding basin (WLRB02).	\$2,490,785.49	0.00%	\$2,490,785.49	All	70.793	\$35,184.06	\$35,184.06	\$35,184.06
DI_DR_01a	Development	Construction of a piped Drainage Main in Between WLRB02 and WW06	\$1,679,906.90	0.00%	\$1,679,906.90	All	70.793	\$23,729.84	\$23,729.84	\$23,729.84
DI_DR_02	Development	Construction of a Waterway from north of SB01 to Barwon Heads Road (WW03).	\$4,211,563.86	0.00%	\$4,211,563.86	All	70.793	\$59,491.25	\$59,491.25	\$59,491.25
DI_DR_03	Development	Construction of Sediment Basin and Siphon and associated infrastructure (SB01) and portion of SUP04 surrounding the Basin	\$1,839,687.08	0.00%	\$1,839,687.08	All	70.793	\$25,986.85	\$25,986.85	\$25,986.85
DI_DR_04	Development	Construction of the Sediment Basin, Waterway and associated infrastructure (SB02) and the portion of SUP02 surrounding the drainage basin.	\$1,066,268.95	0.00%	\$1,066,268.95	All	70.793	\$15,061.79	\$15,061.79	\$15,061.79
DI_DR_05	Development	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB01)	\$785,881.86	0.00%	\$785,881.86	All	70.793	\$11,101.12	\$11,101.12	\$11,101.12
DI_DR_06	Development	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB02)	\$753,961.52	0.00%	\$753,961.52	All	70.793	\$10,650.23	\$10,650.23	\$10,650.23
DI_DR_07	Development	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB03)	\$788,519.44	0.00%	\$752,194.84	All	70.793	\$10,625.27	\$10,625.27	\$10,625.27
DI_DR_08	Development	Construction of Waterway inbetween the Rail Corridor and Drews Road (WW01), including the construction of SUP8	\$1,315,105.56	0.00%	\$1,315,105.56	All	70.793	\$18,576.77	\$18,576.77	\$18,576.77
DI_DR_09	Development	Construction of Waterway abutting the Smith Street Greenway, including bypass drain (WW_02).	\$1,634,060.47	0.00%	\$1,634,060.47	All	70.793	\$23,082.23	\$23,082.23	\$23,082.23
DI_DR_10	Development	Construction of Waterway in between Barwon Heads Road and Tannery Road (WW04)	\$1,836,702.40	29.25%	\$1,299,466.95	All	70.793	\$18,355.87	\$18,355.87	\$18,355.87
DI_DR_11	Development	Construction of the waterway in between Tannery Road and The Barwon River (WW05)	\$3,606,863.56	29.25%	\$2,551,855.97	All	70.793	\$36,046.73	\$36,046.73	\$36,046.73
DI_DR_12	Development	Construction of a Retarding Basin and associated infrastructure (WLRB03), including shared paths SUP_06 (portion) and SUP07	\$6,094,444.51	0.00%	\$6,094,444.51	All	70.793	\$86,088.24	\$86,088.24	\$86,088.24
DI_DR_12a	Development	Construction of the piped drainage main in between WLRB03 and SB01	\$2,584,660.27	0.00%	\$2,584,660.27	All	70.793	\$36,510.11	\$36,510.11	\$36,510.11
DI_DR_13	Development	Construction of a Wetland and Sediment basin (SBWL01), including the construction of a portion of SUP02 (3m)	\$1,986,941.55	0.00%	\$1,986,941.55	All	70.793	\$28,066.92	\$28,066.92	\$28,066.92
DI_DR_14	Development	Construction of the waterway outfall within the Sparrowvale wetlands (WW06)	\$2,429,555.22	85.00%	\$364,433.28	All	70.793	\$5,147.87	\$5,147.87	\$5,147.87
<b>SUB-TOTAL DRAINAGE</b>			<b>\$35,104,908.64</b>		<b>\$31,411,219.06</b>		<b>70.793</b>	<b>\$443,705.16</b>	<b>\$443,705.16</b>	<b>\$443,705.16</b>

## T9. CALCULATION OF COSTS – DEVELOPMENT INFRASTRUCTURE LEVY (DIL)

### TRANSPORT

DCP ID	DCP TYPE	PROJECT DESCRIPTION	CONSTRUCTION/ LAND COST	EXTERNAL AP- PORTIONMENT	COST MCA	CHARGE AREAS	DEMAND UNITS	LEVY PER HA	CHARGE AREA 1 LEVY/HA (STATION PRE- CINCT AND RESIDENTIAL)	CHARGE AREA 2 LEVY/HA (COMMER- CIAL)
DI_IT_01	Development	Construction associated with the Drews Road/Horseshoe Road and Reserve Road 4 way signalised intersection.	\$5,474,476.03	30.00%	\$1,642,342.81	All	70.793	\$23,199.23	\$23,199.23	\$23,199.23
DI_IT_02	Development	Construction associated with the East-West Connector/ Barwon Heads Road/NEIP 4 way signalised intersection	\$5,837,791.52	50.00%	\$2,918,895.76	All	70.793	\$41,231.42	\$41,231.42	\$41,231.42
<b>SUB-TOTAL INTERSECTIONS</b>			<b>\$11,312,267.55</b>		<b>\$4,561,238.57</b>			<b>\$64,430.64</b>	<b>\$64,430.64</b>	<b>\$64,430.64</b>

### OPENSACE

DCP ID	DCP TYPE	PROJECT DESCRIPTION	CONSTRUCTION/ LAND COST	EXTERNAL AP- PORTIONMENT	COST MCA	CHARGE AREAS	DEMAND UNITS	LEVY PER HA	CHARGE AREA 1 LEVY/HA (STATION PRE- CINCT AND RESIDENTIAL)	CHARGE AREA 2 LEVY/HA (COMMER- CIAL)
DI_OS_1	Development	Linear Openspace - Smith Street Greenway	\$361,771.60	0.00%	\$361,771.60	Charge Area 1 only	60.929	\$5,937.59	\$5,937.59	\$0.00
DI_OS_2	Development	Linear Openspace - Drews Road Greenway	\$799,738.75	0.00%	\$799,738.75	Charge Area 1 only	60.929	\$13,125.75	\$13,125.75	\$0.00
DI_OS_3	Development	Local Park - Northern Park Adjacent to Conservation - Landscaping Embellishment and Playground Equipment and a section of SUP4	\$2,174,190.00	0.00%	\$2,174,190.00	Charge Area 1 only	60.929	\$35,683.99	\$35,683.99	\$0.00
DI_OS_4	Development	Local Park - Southern Park Separated by Conservation - Landscaping embellishment and Playground Equipment	\$1,467,515.00	0.00%	\$1,467,515.00	Charge Area 1 only	60.929	\$24,085.66	\$24,085.66	\$0.00
DI_OS_5	Development	Local Park - Station Precinct Park - Landscaping Embellishment and Playground Equipment	\$1,267,277.00	0.00%	\$1,267,277.00	Charge Area 1 only	60.929	\$20,799.24	\$20,799.24	\$0.00
DI_OS_6	Development	Local Park - East West Connector - Landscaping Embellishment, Playground Equipment and a section of SUP5	\$1,902,399.00	0.00%	\$1,902,399.00	Charge Area 1 only	60.929	\$31,223.21	\$31,223.21	\$0.00
<b>SUB-TOTAL OPENSACE</b>			<b>\$7,972,891.35</b>		<b>\$7,972,891.35</b>			<b>\$130,855.44</b>	<b>\$130,855.44</b>	<b>\$0.00</b>

## T9. CALCULATION OF COSTS – DEVELOPMENT INFRASTRUCTURE LEVY (DIL)

### LAND

DCP ID	DCP TYPE	PROJECT DESCRIPTION	CONSTRUCTION/ LAND COST	EXTERNAL AP- PORTIONMENT	COST MCA	CHARGE AREAS	DEMAND UNITS	LEVY PER HA	CHARGE AREA 1 LEVY/HA (STATION PRE- CINCT AND RESIDENTIAL)	CHARGE AREA 2 LEVY/HA (COMMER- CIAL)
DI_LA_1	Development	Land required (1.190ha) for the Construction of WLRB02 - property 30 (0.208ha) and property 33 (0.982ha)	\$1,893,000.00	0.00%	\$1,893,000.00	All	70.793	\$26,739.93	\$26,739.93	\$26,739.93
DI_LA_2	Development	Land required (1.879 ha) for the construction of WW03 - property 11 (0.823 ha), property 20 (0.373 ha), property 21 (0.479 ha), property 22 (0.204 ha)	\$2,993,307.00	0.00%	\$2,993,307.00	All	70.793	\$42,282.53	\$42,282.53	\$42,282.53
DI_LA_3	Development	Land required (0.370 ha) for the construction of SB02 - property 20 (0.370 ha)	\$740,000.00	0.00%	\$740,000.00	All	70.793	\$10,453.01	\$10,453.01	\$10,453.01
DI_LA_4	Development	Land required (1.171 ha) for the construction of SBWL01 - property 21 (0.634 ha), property 22 (0.537 ha)	\$1,653,813.00	0.00%	\$1,653,813.00	All	70.793	\$23,361.25	\$23,361.25	\$23,361.25
DI_LA_5	Development	Land required (0.501 ha) for the construction of SB01 - property 11 (0.026 ha), property 13 (0.475 ha)	\$782,435.00	0.00%	\$782,435.00	All	70.793	\$11,052.43	\$11,052.43	\$11,052.43
DI_LA_6	Development	Land required (0.243 ha) for the construction of SBRB01 - property 35 (0.253)	\$148,000.00	0.00%	\$148,000.00	All	70.793	\$2,090.60	\$2,090.60	\$2,090.60
DI_LA_7	Development	Land required (.231 ha) for the construction of SBRB02 - property 40 (0.096 ha), property 41 (0.135ha)	\$272,000.00	0.00%	\$272,000.00	All	70.793	\$3,842.19	\$3,842.19	\$3,842.19
DI_LA_8	Development	Land required (0.237 ha) for the construction of SBRB03 - property 42 (0.237 ha)	\$349,000.00	0.00%	\$349,000.00	All	70.793	\$4,929.87	\$4,929.87	\$4,929.87
DI_LA_9	Development	Land required (0.258 ha) for the construction of WW01 - property 3 (0.090 ha), property 4a (0.116 ha), property 4b (0.052 ha)	\$535,436.00	0.00%	\$535,436.00	All	70.793	\$7,563.40	\$7,563.40	\$7,563.40
DI_LA_10	Development	Land required (1.476 ha) for the construction of WW04 - Property 43 (0.288 ha), 401-419 Barwon Heads Road, Charlemont (0.502 ha), 30 Tannery Road, Charlemont (0.595 Ha), 46 Tannery Road, Charlemont (0.091 Ha)	\$892,158.00	29.25%	\$626,741.00	All	70.793	\$8,853.15	\$8,853.15	\$8,853.15
DI_LA_11	Development	Land required (3.253 ha) for the construction of WW05 - 31-39 tannery Road, Charlemont (0.392 ha), 41-49 Tannery Road, Charlemont (1.472 Ha), 81-89 Tannery Road, Charlemont (1.389 Ha)	\$675,715.00	29.25%	\$474,689.79	All	70.793	\$6,705.32	\$6,705.32	\$6,705.32
DI_LA_12	Development	Land required (2.931 ha) for the construction of WLRB03 - property 44 (2.054ha), property 45 (.877 ha)	\$6,099,213.00	0.00%	\$6,099,213.00	All	70.793	\$86,155.59	\$86,155.59	\$86,155.59
DI_LA_13	Development	Land required (2.03 ha) for the construction of DI_IT_01 and the Bellarine Link Intersection - Property 45 (0.014 ha), Property 6 (0.078 ha), Property 14 (0.111 ha)	\$339,558.00	0.00%	\$339,558.00	All	70.793	\$4,796.49	\$4,796.49	\$4,796.49
DI_LA_14	Development	Land required (0.311 ha) for the construction of DI_IT_02 - property 23 (0.071ha), property 25 (0.121 ha), property 26 (0.119 ha)	\$614,474.00	0.00%	\$614,474.00	All	70.793	\$8,679.87	\$8,679.87	\$8,679.87
DI_LA_15	Development	Land required (0.430 ha) for the Reserve Road and E/W Connector Road Roundabout - property 4a (0.079 ha), property 6 (0.245 ha), property 12 (0.007 ha), property 14 (0.109 ha)	\$691,294.00	0.00%	\$691,294.00	All	70.793	\$9,765.01	\$9,765.01	\$9,765.01
DI_LA_16	Development	Land required (2.655 ha) for the East/West Connector Road - property 5 (0.275ha), property 6 (0.628 ha), property 14 (0.130 ha), property 15 (0.266 ha), property 16 (0.607 ha), property 24 (0.004ha), property 26 (0.204 ha), property 27 (0.347 ha), property 29 (0.194 ha)	\$4,410,499.00	0.00%	\$4,410,499.00	All	70.793	\$62,301.34	\$62,301.34	\$62,301.34
DI_LA_17	Development	Land required (0.839 ha) for the Drews Road Widening/Realignment - property 1 (0.110ha), property 2 (0.361ha), property 6 (0.005 ha), property 14 (0.009 ha), property 4a (0.248 ha), Property 4b (0.106 ha)	\$1,602,526.00	0.00%	\$1,602,526.00	All	70.793	\$22,636.79	\$22,636.79	\$22,636.79
<b>SUB-TOTAL LAND</b>			<b>\$24,692,428.00</b>		<b>\$24,225,985.78</b>			<b>\$342,208.77</b>	<b>\$342,208.77</b>	<b>\$342,208.77</b>

### PLANNING COSTS

DI_PL_01	Development	Planning Costs associated with the preparation of the Marshall PSP and DCP	\$600,578.80	0.00%	\$600,578.80	All	70.793	\$8,483.59	\$8,483.59	\$8,483.59
<b>TOTAL</b>			<b>\$79,683,074.34</b>		<b>\$68,771,913.56</b>			<b>\$989,683.61</b>	<b>\$989,683.61</b>	<b>\$858,828.16</b>

## T10. CALCULATION OF COSTS – COMMUNITY INFRASTRUCTURE LEVY (CIL)

### COMMUNITY INFRASTRUCTURE ITEM

DCP ID	DCP TYPE	PROJECT DESCRIPTION	CONSTRUCTION COST	EXTERNAL APPORTIONMENT	COST MCA	CHARGE AREAS	DEMAND UNITS	LEVY PER DWELLING	CHARGE AREA A LEVY/ DWELLING (STATION PRECINCT AND RESIDENTIAL)	CHARGE AREA B LEVY/HA (COMMERCIAL)	23/24 CAPPED CIL LEVY/ DWELLING
CI_OS_01	Community	Regional Active Open Space (northern) - Community Pavillion	\$3,500,404.84	80.00%	\$700,080.97	Charge Area 1 only	1,555	\$450.21	\$450.21	\$0.00	\$419.82
CI_CF_01	Community	Library - construction (contribution) - Contribution to the construction of the regional library in the Armstrong Creek Town Centre	\$21,865,000.00	93.26%	\$1,473,701.00	Charge Area 2 only	1,555	\$947.72	\$947.72	\$0.00	\$926.18
<b>TOTAL</b>					<b>\$2,173,781.97</b>			<b>\$1,397.93</b>	<b>\$1,397.93</b>		<b>\$1,346.00</b>

# ADMINISTRATION

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## 5.0 ADMINISTRATION

This section sets out how the DCP will be administered and covers the timing of payment, provision of works and land in kind and how funds generated by the DCP will be managed in terms of reporting, indexation and review periods.

The development infrastructure levy applies to subdivision and/or development of land.

The City of Greater Geelong will be both the **collecting agency** and the **development agency** for the purposes of the DCP.

## 5.1 PAYMENT OF CONTRIBUTIONS & PAYMENT TIMING

### 5.1.1 COMMUNITY INFRASTRUCTURE LEVY (CIL)

The Community Infrastructure Levy must be paid in accordance with section 460 of the Planning & Environment Act (1987). Nevertheless, developers/landowners are strongly encouraged to settle the Community Infrastructure Levy prior to receiving a Statement of Compliance. This proactive approach simplifies the collection of development contributions, reduces administrative complexities for CoGG, and expedites the provision of community infrastructure.

## 5.1.2 DEVELOPMENT INFRASTRUCTURE LEVY (DIL)

### The DIL will be payable to and collected by the collecting agency, for the:

- Subdivision of land; or
- Development of land which requires a planning permit; or
- Development of land which does not require a planning permit, as set out in this DCP

### FOR SUBDIVISION OF LAND

A development infrastructure levy must be paid to the collecting agency for the land within the following specified time, namely after certification of the relevant plan of subdivision but not more than 21 days prior to the issue of a Statement of Compliance in respect of that plan or at such other time as agreed with the collecting agency in an implementation agreement under Section 173 of the Act.

Where the subdivision is to be developed in stages, the infrastructure levy for the stage to be developed only may be paid to the collecting agency within 21 days prior to the issue of a Statement of Compliance in respect of that stage provided that a Schedule of Development Contributions is submitted with each stage of the plan of subdivision. This Schedule of Development Contributions must show the amount of the development contributions payable for each stage and value of the contributions in respect of prior stages to the satisfaction of the collecting agency or at such other time as agreed with the collecting agency in an implementation agreement under section 173 of the Act.

If the collecting agency agrees to works and/or provision of land in lieu of the payment of the infrastructure levy, the landowner must enter into an agreement under Section 173 of the Act in respect of the proposed works and/or provision of land in kind to set out the specific arrangements agreed with the collecting agency.

### FOR DEVELOPMENT OF LAND WHERE NO SUBDIVISION IS PROPOSED

Provided the development infrastructure levy has not already been paid on the land to be developed, the development infrastructure levy must be paid to the collecting agency in accordance with the provisions of the approved DCP for each demand unit (net developable hectare) proposed to be developed prior to the commencement of any buildings and works for the development i.e. development includes (but is not limited to) buildings, car park, access ways, landscaping and ancillary components). The collecting agency may require that development infrastructure levy contributions be made at either the planning permit or building permit stage.

If the collecting agency agrees to works and/or provision of land in lieu of the payment of the infrastructure levy, the landowner must enter into an agreement under Section 173 of the Act in respect of the proposed works and/or land to be provided in kind to set out the specific arrangements agreed with the collecting agency.

### WHERE NO PLANNING PERMIT IS REQUIRED

**The following requirement applies where no planning permit is required. The land may only be used and developed subject to the following requirements being met:**

- Unless some other arrangement has been agreed to by the collecting agency in an agreement under Section 173 of the Act, prior to the commencement of any development, a development infrastructure levy for the land must be paid to the collecting agency in accordance with the provisions of the DCP.
- If the collecting agency agrees to works and/or provision of land in lieu of the payment of the infrastructure levy, the landowner must enter into an agreement under Section 173 of the Act in respect of the proposed works or provision of land which is proposed to be provided in kind to set out the specific arrangements agreed with the collecting agency.

### 5.1.3 LAND AND WORKS-IN-KIND

#### The collecting agency may permit development proponents to undertake works and/or land in lieu of cash payments, providing that:

- The works or land constitute projects funded by the DCP
- The collecting agency agrees that the timing of the works would be consistent with priorities in the DCP
- The development proponent complies with appropriate tendering, documentation, supervision and related provisions as required by the responsible authority
- Works must be provided to a standard that generally accords with the DCP, unless an alternative is agreed by the collecting agency and the development agency
- Detailed design must be approved by the collecting agency and the development agency and must generally accord with the standards outlined in the DCP unless an alternative is agreed by the collecting agency and the development agency
- The construction of works must be completed to the satisfaction of the collecting agency and the development agency
- There should be no negative financial impact on the DCP to the satisfaction of the collecting agency

In particular, the works will only be accepted in lieu of a financial contribution required by the DCP to the extent that they constitute part or all of the design of the infrastructure item and reduce the cost to complete that design, to the satisfaction of the collecting agency. Temporary works will not be accepted as works in kind and any sacrificial works will not be credited.

#### Where the collecting agency agrees that works are to be provided by a development proponent in lieu of cash contribution (subject to the arrangements specified above):

- The credit for the works provided shall equal the final cost of the works as identified in the DCP, taking into account the impact of indexation
- The value of works provided in accordance with the principle outlined above will be offset against the development contributions liable to be paid by the development proponent
- No further financial contributions will be required until the agreed value of any credits are used
- Where a developer is in credit against their contribution liability, this credit should be indexed annually in accordance with the DCP indexation mechanism

### 5.1.4 CREDIT FOR OVER-PROVISION

Where the collecting agency agrees that a development proponent can deliver an infrastructure item (either works and/or land), the situation may arise where the developer makes a contribution with a value that exceeds that required by the DCP (the Over-Contribution).

In such a case the developer may be entitled to a credits equal to the value of the Over-Contribution to be applied against the obligation to pay the development infrastructure levy for future stages of development or for the development of other properties within the DCP. Where, a developer does not own or control any other properties within the DCP, or in such other circumstances as agreed by the collecting agency in writing, the developer may seek an agreement for the collecting agency to provide

cash reimbursement where an over-contribution has been made. The application of credit and/or cash reimbursement (including the time for payment of any cash reimbursement) will be set out in the relevant agreement under Section 173 of the Act.

The details of credits and reimbursements for construction shall equal the cost of the works identified in the DCP, as indexed in accordance with [Section 5.2](#). The value of credits and reimbursements for the transfer of land will need to be at the values that are outlined in the DCP, as revalued and indexed in accordance with in [Section 5.2](#).

## 5.1.5 FUNDS ADMINISTRATION

The administration of the contributions made under the DCP will be transparent and development contributions charges will be held until required for provision of the items in that class. Details of funds received and expenditures will be held by the collecting agency in accordance with the provisions of the Local Government Act 1989 and the Act.

### **The administration of contributions made under the DCP will be transparent and demonstrate the:**

- Amount and timing of funds collected
- Source of the funds collected
- Amount and timing of expenditure on specific projects
- Project on which the expenditure was made
- Details of works in kind arrangements for project provision

The collecting agency will provide for regular monitoring, reporting and review of the monies received and expended in accordance with the DCP.

Development contributions funds will be held into a dedicated reserve account, in compliance with the regulations and the Minister's Direction under the Act. The entirety of these funds, held within the account, will be exclusively utilized for the provision of infrastructure as specified in this DCP.

Should the Council make a decision to not pursue any of the infrastructure projects outlined in this DCP, or any funds not be expended prior to the end of this DCP, the City of Greater Geelong will adhere to the requirements outlined in section 46(Q)(4) of the Act.

## 5.2 CONSTRUCTION & LAND VALUE COSTS INDEXATION

Capital costs of all infrastructure items, including land, are in 2023 dollars and will be adjusted by the collecting agency annually for inflation.

**In relation to the costs associated with infrastructure items other than land, the cost must be adjusted according to the following method:**

- Drainage items, Roads, intersections and bridges – indexed in line with the Australian Bureau of Statistics’ Producer Prices Indexes, Road and Bridge Construction Index, Victoria.
- All other infrastructure items – indexed in line with the Australian Bureau of Statistics’ Producer Price Indexes, Non-Residential Building Construction Index, Victoria.

Estimates of land value will be revised annually by a registered valuer based on the same valuation principles. Revisions may occur more frequently if market conditions warrant.

The revised infrastructure costs must be calculated as at 1 July in each year.

The collecting agency will publish the amended contributions on the collecting agency’s website within 14 days of the adjustments being made.

The CIL cap (currently \$1,346 per dwelling for the 2023-24 financial year) is indexed annually on July 1 by the Minister for Planning and is published on the department website. Council reserves the right to increase the CIL in this DCP to allow for cost escalation in accordance with the indexation method in this DCP up to any new CIL cap. The higher levy will be collected from the date the new CIL cap is introduced.

## 5.3 REVIEW PERIOD

The DCP commenced on the date when it was first incorporated into the Greater Geelong Planning Scheme. The DCP adopts a long-term outlook for future development in Creamery Road.

**The DCP should be reviewed and if necessary updated every 5 years (or more frequently if required). The 5 yearly review is anticipated to include:**

- Updates to any aspect of the plan as required;
- Review of projects required, as well as their costs and scope (as relevant) and indicative provision trigger;

- Review of estimated net developable area (this will also be required in the PSP is subject to a substantive amendment); and
- Review of land values for land to be purchased through the plan.

Any review will need to have regard to any arrangements (e.g. section 173 agreements under the Act) for the implementation of the DCP.

## **5.4 ADJUSTMENT TO THE SCOPE OF PROJECTS**

The infrastructure projects in the DCP have been costed to a sufficient level of detail, however all of the projects will require a detailed design process prior to construction.

As part of detailed design, the Council, or a development proponent with the consent of the Development Agency may amend or modify some aspects of projects, so long as they are still generally in accordance with the PSP and any direction regarding the scope outlined in the DCP.

In these cases there must be no negative impact on the DCP. The developer must pay the additional costs associated with the provision of the infrastructure item over and above the standard required by the DCP.

## **5.5 COLLECTING AGENCY (AGENCY RESPONSIBLE FOR COLLECTING INFRASTRUCTURE LEVY)**

City of Greater Geelong is the collecting agency pursuant to section 46K(1)(fa) of the Act which means that it is the public authority to which all levies are payable. As the collecting agency, council is responsible for the administration of the DCP and also its enforcement pursuant to section 46QC of the Act.

## **5.6 DEVELOPMENT AGENCY (AGENCY RESPONSIBLE FOR WORKS)**

City of Greater Geelong is the development agency and is responsible for the provision of the designated infrastructure projects which are funded under the DCP and the timing of all works.



# IMPLEMENTATION STRATEGY

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## 6.0 IMPLEMENTATION STRATEGY

This section provides further details regarding how the collecting agency intends to implement the DCP. In particular, this section clearly identifies the rationale for the implementation strategy and details the various measures that have been adopted to reduce the risk posed by the DCP to all parties.

### 6.1 RATIONALE FOR THE IMPLEMENTATION STRATEGY

This implementation strategy has been included to provide certainty to both the collecting agency and development proponents. The implementation strategy recognises the complexities associated with infrastructure provision and funding and seeks to minimise risk to the collecting agency, development agency, development proponent and future community.

**This implementation strategy has been formulated by:**

- Assessing the PSP
- Having regard to the development context
- Assessing the need for finance requirements including upfront financing and pooling of funds
- Agreeing the land value and indexing it appropriately (where possible)
- Identifying preferred implementation mechanisms to achieve the above outcomes and reducing the risk associated with the DCP to ensure that it will be delivered as intended.

## 6.2 IMPLEMENTATION MECHANISM

Under section 46P of the Act, the collecting agency may accept (with the consent of the development agency where the collecting agency is not also the development agency) the provision of land, works, services or facilities by the applicant in part or full satisfaction of the amount of levy payment. This can be by agreement with the collecting agency before or after the application for the permit is made or before the development is carried out.

To coordinate the provision of infrastructure, Schedule 7 to the Urban Growth Zone in the Greater Geelong Planning Scheme for the PSP requires an application for subdivision to be accompanied by an infrastructure plan to the satisfaction of the responsible authority.

**The public infrastructure plan needs to show the location, type, staging and timing of infrastructure on the land as identified in the PSP or reasonably required as a result of the subdivision of the land and address the following:**

- Stormwater drainage works;
- Road works internal or external to the land consistent with any relevant traffic report or assessment;
- The reserving or encumbrance of land for infrastructure, including for community facilities, sports reserves and open space;
- Any infrastructure works which an applicant proposes to provide in lieu of development contributions in accordance with the DCP;
- Land Budget breakdown with staging sequencing; and
- Any other relevant matter related to the provision of infrastructure reasonably required by the responsible authority.

Through the approval of these agreements, council (acting as the collecting agency) will consider if and what infrastructure should be provided as works in kind under the DCP in accordance with section 46P of the Act. The agreement must include a list of the DCP infrastructure projects that the collecting agency has agreed in writing to allow to be provided as works and/or land in lieu.



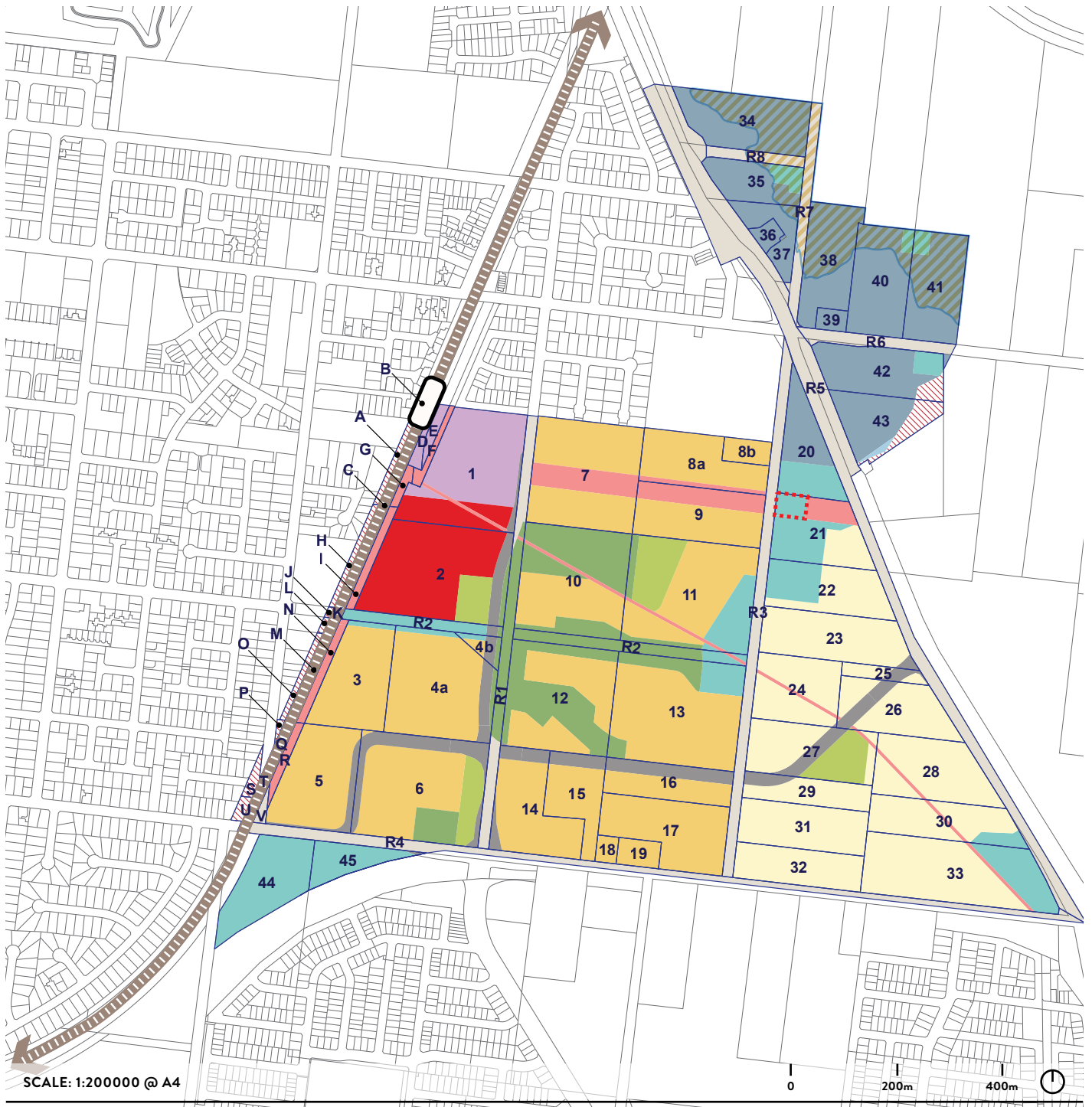
# APPENDICES

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# 7.1 APPENDIX A – PROPERTY PLAN & PROPERTY SPECIFIC LAND BUDGET

## P9. PROPERTY PLAN












### KEY

#### CONTEXT






-  Property Boundary
- #12** Property Identification Number\*

#### EXCLUDED & ENCUMBERED LAND

-  Land excluded from Land Budget (NEIP PSP Area) \*
-  Existing Road Reserve Retained as Road

-  Conservation Reserve
-  Utilities Easements
-  Drainage Reserve
-  Station & Carpark TRZ 1
-  Proposed DCP Connector Road
-  Barwon River Floodplain
-  Drainage area included on Transmission Easement

#### NET DEVELOPABLE LAND

-  Conventional Residential
-  Medium/Higher Density Residential
-  Mixed Use - High Density Residential
-  Commercial/Bulky Goods
-  Credited (Unencumbered) Open Space

\* Refer to property-specific land use budget table for more detailed land use classification

# 7.1 APPENDIX A - PROPERTY PLAN AND PROPERTY SPECIFIC LAND BUDGET

## T11. PROPERTY SPECIFIC LAND USE BUDGET/PLAN

		OUTSIDE PRECINCT	TRANSPORT											UNCREDITED OPEN SPACE							CREDITED OPEN SPACE	NDA				
Property	Total Area	Part of NEIP PSP	Arterial Road	Arterial Road Intersection (Barwon Heads Road)	Arterial Road Intersection (Bellarine Link)	Arterial Road Intersection (HSB / Drews / Reserve Road)	Connector Road Intersection	Drews Rd Connector road	EW Connector road	Existing Rail Reserve	Part of Transport or Barwon Water	Retained Existing Road Reserve	Transport Hub	Barwon Water Easements Adjacent to Rail Reserve	Conservation	Flood Overlay Land	High Voltage Electricity Utility Easement	Main Out-fall Sewer Utility Easement	Drainage (Within Existing Road Reserve)	Drainage (Within High Voltage Electricity Utility Easement)	Water-way, Wet-land And Drainage Reserve	Local Park	Commer-cial Bulky Goods	Medium Density Residential	Standard Residential	Station Precinct
1	4.004	0.000	0.000	0.000	0.000	0.000	0.000	0.110	0.000	0.000	0.000	0.000	2.837	0.000	0.000	0.000	0.000	0.110	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.946
2	4.561	0.000	0.000	0.000	0.000	0.000	0.000	0.361	0.000	0.000	0.000	0.000	0.000	0.000	0.110	0.000	0.000	0.000	0.000	0.000	0.000	0.548	0.000	0.000	0.000	3.543
3	2.390	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.090	0.000	0.000	2.300	0.000	0.000
5	2.729	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.275	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.454	0.000
6	4.767	0.000	0.000	0.000	0.078	0.000	0.235	0.005	0.628	0.000	0.000	0.000	0.000	0.000	0.508	0.000	0.000	0.000	0.000	0.000	0.000	0.583	0.000	0.000	2.729	0.000
7	4.049	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.861	0.000	0.000	0.000	0.000	0.000	0.000	0.000	3.188	0.000
9	2.437	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.810	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.627	0.000
10	4.049	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.304	0.000	0.000	0.131	0.000	0.000	0.000	0.000	0.000	0.000	1.615	0.000
11	4.860	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.310	0.000	0.000	0.130	0.000	0.000	0.849	0.973	0.000	2.597	0.000	0.000
12	4.052	0.000	0.000	0.000	0.000	0.000	0.007	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.030	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.015	0.000
13	4.854	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.579	0.000	0.000	0.000	0.000	0.000	0.475	0.000	0.000	3.801	0.000	0.000
14	2.460	0.000	0.000	0.000	0.111	0.000	0.109	0.009	0.130	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.100	0.000
15	1.508	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.266	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.241	0.000
16	1.623	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.607	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.016	0.000
17	2.550	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.550	0.000
18	0.203	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.203	0.000
19	0.403	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.403	0.000
20	1.748	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.743	0.000	0.000	1.005	0.000	0.000
21	2.099	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.383	0.000	0.000	0.246	0.867	0.000	0.000	0.000	0.602	0.000
22	2.034	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.740	0.000	0.000	0.000	1.294	0.000	0.000
23	2.431	0.000	0.000	0.071	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.360	0.000
24	2.024	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.004	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.106	0.000	0.000	0.000	0.000	0.000	0.000	1.913	0.000
25	0.405	0.000	0.000	0.121	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.284	0.000
26	2.035	0.000	0.000	0.119	0.000	0.000	0.000	0.000	0.204	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.010	0.000	0.000	0.000	0.001	0.000	0.000	1.700	0.000
27	2.425	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.347	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.030	0.000	0.000	0.000	0.798	0.000	0.000	1.250	0.000
28	2.402	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.080	0.000	0.000	0.000	0.000	0.000	2.322	0.000	0.000
29	1.214	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.194	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.020	0.000
30	1.940	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.065	0.000	0.000	0.208	0.000	0.000	0.000	1.668	0.000
31	2.024	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.024	0.000
32	1.642	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.642	0.000
33	4.075	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.110	0.000	0.000	0.982	0.000	0.000	0.000	2.983	0.000
34	2.408	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.612	0.000	0.000	0.000	0.000	0.000	0.000	0.797	0.000	0.000	0.000
35	1.289	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.197	0.000	0.000	0.000	0.000	0.253	0.000	0.840	0.000	0.000	0.000
36	0.230	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.230	0.000	0.000	0.000
37	0.830	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.079	0.000	0.000	0.000	0.000	0.000	0.000	0.751	0.000	0.000	0.000
38	2.006	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.200	0.000	0.000	0.000	0.000	0.000	0.000	0.806	0.000	0.000	0.000
39	0.233	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.233	0.000	0.000	0.000
40	2.205	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.472	0.000	0.000	0.000	0.000	0.096	0.000	1.637	0.000	0.000	0.000
41	2.093	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	1.316	0.000	0.000	0.000	0.135	0.000	0.642	0.000	0.000	0.000	0.000
42	2.089	0.192	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.237	0.000	1.659	0.000	0.000	0.000	0.000
43	1.732	0.411	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.057	0.000	1.264	0.000	0.000	0.000	0.000
44	2.054	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	2.054	0.000	0.000	0.000	0.000	0.000	0.000

# T11. PROPERTY SPECIFIC LAND USE BUDGET/PLAN

		OUTSIDE PRECINCT	TRANSPORT											UNCREDITED OPEN SPACE								CREDITED OPEN SPACE	NDA			
Property	Total Area	Part of NEIP PSP	Arterial Road	Arterial Road Intersection (Barwon Heads Road)	Arterial Road Intersection (Bellarine Link)	Arterial Road Intersection (HSB / Drews / Reserve Road)	Connector Road Intersection	Drews Rd Connector road	EW Connector road	Existing Rail Reserve	Part of Transport or Barwon Water	Retained Existing Road Reserve	Transport Hub	Barwon Water Easements Adjacent to Rail Reserve	Conservation	Flood Overlay Land	High Voltage Electricity Utility Easement	Main Out-fall Sewer Utility Easement	Drainage (Within Existing Road Reserve)	Drainage (Within High Voltage Electricity Utility Easement)	Water-way, Wet-land And Drainage Reserve	Local Park	Commercial Bulky Goods	Medium Density Residential	Standard Residential	Station Precinct
45	0.891	0.000	0.000	0.000	0.000	0.014	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.877	0.000	0.000	0.000	0.000	
4a	3.762	0.000	0.000	0.000	0.000	0.000	0.079	0.248	0.000	0.000	0.000	0.000	0.000	0.000	0.082	0.000	0.000	0.000	0.000	0.000	0.116	0.000	0.000	3.238	0.000	
4b	0.290	0.000	0.000	0.000	0.000	0.000	0.000	0.106	0.000	0.000	0.000	0.000	0.000	0.000	0.054	0.000	0.000	0.000	0.000	0.000	0.052	0.000	0.000	0.078	0.000	
8a	2.033	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.221	0.000	0.000	0.000	0.000	0.000	0.000	1.812	0.000	
8b	0.411	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.411	0.000	
A	0.208	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.208	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
B	0.002	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.002	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
C	0.850	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.850	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
D	0.183	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.183	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
E	0.185	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.185	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
F	0.198	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.198	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
G	0.131	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.131	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
H	0.211	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.211	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
I	0.315	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.315	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
J	0.021	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.021	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
K	0.043	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.043	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
L	0.032	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.032	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
M	0.422	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.422	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
N	0.316	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.300	0.000	0.000	0.000	0.000	0.000	0.000	0.016	0.000	0.000	0.000	0.000	
O	0.179	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.179	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
P	0.038	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.038	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
Q	0.181	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.181	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R	0.229	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.229	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R1	1.664	0.000	0.000	0.000	0.181	0.000	0.189	0.508	0.000	0.000	0.000	0.000	0.000	0.000	0.777	0.000	0.000	0.008	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R2	1.552	0.000	0.000	0.000	0.000	0.000	0.000	0.043	0.000	0.000	0.000	0.000	0.000	0.000	0.784	0.000	0.000	0.027	0.699	0.000	0.000	0.000	0.000	0.000	0.000	
R3	2.044	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.052	0.000	0.000	1.992	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R4	1.954	0.000	0.062	0.000	0.206	0.237	0.000	0.000	0.000	0.037	0.000	1.412	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R5	5.920	0.033	5.887	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R6	0.639	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.639	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R7	0.818	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.818	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
R8	0.377	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.377	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
S	0.019	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.019	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
T	0.144	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.144	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
U	0.085	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.084	0.000	0.001	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
V	0.083	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.083	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	
<b>GRAND TOTAL</b>	<b>123.596</b>	<b>0.636</b>	<b>5.949</b>	<b>0.311</b>	<b>0.576</b>	<b>0.251</b>	<b>0.619</b>	<b>1.390</b>	<b>2.707</b>	<b>1.780</b>	<b>0.691</b>	<b>5.322</b>	<b>3.205</b>	<b>1.173</b>	<b>7.538</b>	<b>4.876</b>	<b>2.275</b>	<b>0.807</b>	<b>0.699</b>	<b>0.246</b>	<b>8.847</b>	<b>2.903</b>	<b>9.864</b>	<b>35.378</b>	<b>21.062</b>	<b>4.489</b>

## 7.2 APPENDIX B – PROJECT SHEETS

Each DCP item to have an individual Project Sheet following finalisation of DCP.

<b>ITEM CODE</b>	DI_DR_01
<b>DESCRIPTION</b>	Construction of a sediment pond, wetland and retarding basin (WLRB02).
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$2,490,785.49
Indicative Provision Trigger	The first subdivision/development within South East Catchment K
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$2,490,785.49
Demand Units	70.793
Levy Amount	\$35,184.06
Costing Source	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_01A</b>
<b>DESCRIPTION</b>	Construction of a piped Drainage Main in Between WLRB02 and WW06
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$1,679,906.90
<b>Indicative Provision Trigger</b>	The first subdivision/development within South East Catchment K, or to the satisfaction of the Responsible Authority
<b>Strategic Justification</b>	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,679,906.90
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$23,729.84
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_02</b>
<b>DESCRIPTION</b>	Construction of a Waterway from north of SB01 to Barwon Heads Road (WW03).
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$4,211,563.86
<b>Indicative Provision Trigger</b>	The first subdivision within Catchment A, B, C, D, E and F, whichever comes first.
<b>Strategic Justification</b>	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$4,211,563.86
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$59,491.25
<b>Costing Source</b>	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_03</b>
<b>DESCRIPTION</b>	Construction of Sediment Basin and Siphon and associated infrastructure (SB01) and portion of SUP04 surrounding the Basin
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$1,839,687.08
Indicative Provision Trigger	The first subdivision within catchment C or D, which ever comes first.
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$1,839,687.08
Demand Units	70.793
Levy Amount	\$25,986.85
Costing Source	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_04</b>
<b>DESCRIPTION</b>	Construction of the Sediment Basin, Waterway and associated infrastructure (SB02) and the portion of SUP02 surrounding the drainage basin.
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$1,066,268.95
Indicative Provision Trigger	The first subdivision within catchment A, F or B, whichever comes first
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$1,066,268.95
Demand Units	70.793
Levy Amount	\$15,061.79
Costing Source	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_05</b>
<b>DESCRIPTION</b>	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB01)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$785,881.86
Indicative Provision Trigger	The first subdivision/development within Catchment G.
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$785,881.86
Demand Units	70.793
Levy Amount	\$11,101.12
Costing Source	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_06</b>
<b>DESCRIPTION</b>	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB02)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$753,961.52
Indicative Provision Trigger	The first subdivision/development within Catchment H.
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$753,961.52
Demand Units	70.793
Levy Amount	\$10,650.23
Costing Source	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_07</b>
<b>DESCRIPTION</b>	Construction of a Sediment Pond and Retarding Basin and associated infrastructure (SBRB03)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$788,519.44
Indicative Provision Trigger	The first subdivision/development within Catchment J.
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$752,194.84
Demand Units	70.793
Levy Amount	\$10,625.27
Costing Source	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_08</b>
<b>DESCRIPTION</b>	Construction of Waterway inbetween the Rail Corridor and Drews Road (WW01), including the construction of SUP8
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$1,315,105.56
Indicative Provision Trigger	The first subdivision within Catchment C or D, whichever comes first.
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$1,315,105.56
Demand Units	70.793
Levy Amount	\$18,576.77
Costing Source	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_09</b>
<b>DESCRIPTION</b>	Construction of Waterway abutting the Smith Street Greenway, including bi pass drain (WW_02).
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$1,634,060.47
Indicative Provision Trigger	The first subdivision within catchment C or D, whichever comes first.
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$1,634,060.47
Demand Units	70.793
Levy Amount	\$23,082.23
Costing Source	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_10</b>
<b>DESCRIPTION</b>	Construction of Waterway in between Barwon Heads Road and Tannery Road (WW04)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$1,836,702.40
Indicative Provision Trigger	Upon 50% of the Marshall Catchment Developing
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	29%
Project Cost to MCA	\$1,299,466.95
Demand Units	70.793
Levy Amount	\$18,355.87
Costing Source	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_11</b>
<b>DESCRIPTION</b>	Construction of the waterway in between Tannery Road and The Barwon River (WW05)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$3,606,863.56
<b>Indicative Provision Trigger</b>	Upon 50% of the Marshall Catchment Developing
<b>Strategic Justification</b>	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	29%
<b>Project Cost to MCA</b>	\$2,551,855.97
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$36,046.73
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_12</b>
<b>DESCRIPTION</b>	Construction of a Retarding Basin and associated infrastructure (WLRB03), including shared paths SUP_06 (portion) and SUP07
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$6,094,444.51
<b>Indicative Provision Trigger</b>	First subdivision within catchment D or to the satisfaction of the Responsible Authority
<b>Strategic Justification</b>	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$6,094,444.51
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$86,088.24
<b>Costing Source</b>	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_12A</b>
<b>DESCRIPTION</b>	Construction of the piped drainage main in Between WLRB03 and SB01
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$2,584,660.27
<b>Indicative Provision Trigger</b>	First subdivision within catchment D or to the satisfaction of the Responsible Authority
<b>Strategic Justification</b>	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$2,584,660.27
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$36,510.11
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_DR_13</b>
<b>DESCRIPTION</b>	Construction of a Wetland and Sediment basin (SBWL01), including the construction of a portion of SUP02 (3m)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$1,986,941.55
Indicative Provision Trigger	The first subdivision within catchment E
Strategic Justification	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	0%
Project Cost to MCA	\$1,986,941.55
Demand Units	70.793
Levy Amount	\$28,066.92
Costing Source	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_DR_14</b>
<b>DESCRIPTION</b>	Construction of the waterway outfall within the Sparrowvale wetlands (WW06)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$2,429,555.22
<b>Indicative Provision Trigger</b>	The first Subdivision within Catchment K or to the satisfaction of the Responsible Authority.
<b>Strategic Justification</b>	Drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	85%
<b>Project Cost to MCA</b>	\$364,433.28
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$5,147.87
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_IT_01</b>
<b>DESCRIPTION</b>	Construction associated with the Drews Road/Horseshoe Road and Reserve Road 4 way signalised intersection.
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Transport
<b>Project Cost</b>	\$5,474,476.03
<b>Indicative Provision Trigger</b>	#N/A
<b>Strategic Justification</b>	#N/A
<b>External Apportionment</b>	30%
<b>Project Cost to MCA</b>	\$1,642,342.81
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$23,199.23
<b>Costing Source</b>	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_IT_02</b>
<b>DESCRIPTION</b>	Construction associated with the East-West Connector/ Barwon Heads Road/NEIP 4 way signalised intersection
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Transport
<b>Project Cost</b>	\$5,837,791.52
<b>Indicative Provision Trigger</b>	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)
<b>Strategic Justification</b>	Intersection Identified in transport and engineering planning as required for appropriate traffic management as identified within the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
<b>External Apportionment</b>	50%
<b>Project Cost to MCA</b>	\$2,918,895.76
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$41,231.42
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_OS_1</b>
<b>DESCRIPTION</b>	Linear Openspace - Smith Street Greenway
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Openspace
<b>Project Cost</b>	\$361,771.60
<b>Indicative Provision Trigger</b>	The subdivision and development which requires the upgrades to WW02
<b>Strategic Justification</b>	Greenways identified in the Precinct Structure Plan to provide linkages between open space, community facilities, residential and commercial areas and protection of native vegetation.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$361,771.60
<b>Demand Units</b>	60.929
<b>Levy Amount</b>	\$5,937.59
<b>Costing Source</b>	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_OS_2</b>
<b>DESCRIPTION</b>	Linear Openspace - Drews Road Greenway
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Openspace
<b>Project Cost</b>	\$799,738.75
<b>Indicative Provision Trigger</b>	The subdivision and development which triggers the ultimate realignment of Drews Road.
<b>Strategic Justification</b>	Greenways identified in the Precinct Structure Plan to provide linkages between open space, community facilities, residential and commercial areas and protection of native vegetation.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$799,738.75
<b>Demand Units</b>	60.929
<b>Levy Amount</b>	\$13,125.75
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_OS_3</b>
<b>DESCRIPTION</b>	Local Park - Northern Park Adjacent to Conservation - Landscaping Embellishment and Playground Equipment and a section of SUP4
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Openspace
<b>Project Cost</b>	\$2,174,190.00
<b>Indicative Provision Trigger</b>	The subdivision and or development on the parcel in which contains the local park
<b>Strategic Justification</b>	Local Park openspace embellishments required to serve the Marshall Residential Catchment as identified within the Marshall PSP
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$2,174,190.00
<b>Demand Units</b>	60.929
<b>Levy Amount</b>	\$35,683.99
<b>Costing Source</b>	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_OS_4</b>
<b>DESCRIPTION</b>	Local Park - Southern Park Separated by Conservation - Landscaping embellishment and Playground Equipment
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Openspace
<b>Project Cost</b>	\$1,467,515.00
<b>Indicative Provision Trigger</b>	The subdivision and or development on the parcel in which contains the local park
<b>Strategic Justification</b>	Local Park openspace embellishments required to serve the Marshall Residential Catchment as identified within the Marshall PSP
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,467,515.00
<b>Demand Units</b>	60.929
<b>Levy Amount</b>	\$24,085.66
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_OS_5</b>
<b>DESCRIPTION</b>	Local Park - Station Precinct Park - Landscaping Embellishment and Playground Equipment
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Openspace
<b>Project Cost</b>	\$1,267,277.00
<b>Indicative Provision Trigger</b>	The subdivision and or development on the parcel in which contains the local park
<b>Strategic Justification</b>	Local Park openspace embellishments required to serve the Marshall Residential Catchment as identified within the Marshall PSP
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,267,277.00
<b>Demand Units</b>	60.929
<b>Levy Amount</b>	\$20,799.24
<b>Costing Source</b>	Spiire QS Package, 2023

<b>ITEM CODE</b>	<b>DI_OS_6</b>
<b>DESCRIPTION</b>	Local Park - East West Connector - Landscaping Embellishment, Playground Equipment and a section of SUP5
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Openspace
<b>Project Cost</b>	\$1,902,399.00
<b>Indicative Provision Trigger</b>	The subdivision and or development on the parcel in which contains the local park
<b>Strategic Justification</b>	Local Park openspace embellishments required to serve the Marshall Precinct residential catchment as identified within the Marshall PSP
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,902,399.00
<b>Demand Units</b>	60.929
<b>Levy Amount</b>	\$31,223.21
<b>Costing Source</b>	Spiire QS Package, 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	DI_LA_1
<b>DESCRIPTION</b>	Land required (1.190ha) for the Construction of WLRB02 - property 30 (0.208ha) and property 33 (0.982ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$1,893,000.00
<b>Indicative Provision Trigger</b>	#N/A
<b>Strategic Justification</b>	#N/A
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,893,000.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$26,739.93
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_2</b>
<b>DESCRIPTION</b>	Land required (1.879 ha) for the construction of WW03 - property 11 (0.823 ha), property 20 (0.373 ha), property 21 (0.479 ha), property 22 (0.204 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$2,993,307.00
<b>Indicative Provision Trigger</b>	The first subdivision within Catchment A, B, C, D, E and F, whichever comes first.
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$2,993,307.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$42,282.53
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_3</b>
<b>DESCRIPTION</b>	Land required (0.370 ha) for the construction of SB02 - property 20 (0.370 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$740,000.00
<b>Indicative Provision Trigger</b>	The first subdivision within catchment A, F or B, whichever comes first
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$740,000.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$10,453.01
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_4</b>
<b>DESCRIPTION</b>	Land required (1.171 ha) for the construction of SBWL01 - property 21 (0.634 ha), property 22 (0.537 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$1,653,813.00
<b>Indicative Provision Trigger</b>	The first subdivision within catchment E
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,653,813.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$23,361.25
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_5</b>
<b>DESCRIPTION</b>	Land required (0.501 ha) for the construction of SB01 - property 11 (0.026 ha), property 13 (0.475 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$782,435.00
<b>Indicative Provision Trigger</b>	The first subdivision within catchment C or D, which ever comes first.
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$782,435.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$11,052.43
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_6</b>
<b>DESCRIPTION</b>	Land required (0.243 ha) for the construction of SBRB01 - property 35 (0.253)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$148,000.00
<b>Indicative Provision Trigger</b>	The first subdivision/development within Catchment G.
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$148,000.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$2,090.60
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_7</b>
<b>DESCRIPTION</b>	Land required (.231 ha) for the construction of SBRB02 -property 40 (0.096 ha), property 41 (0.135ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$272,000.00
<b>Indicative Provision Trigger</b>	The first subdivision/development within Catchment H.
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$272,000.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$3,842.19
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_8</b>
<b>DESCRIPTION</b>	Land required (0.237 ha) for the construction of SBRB03 - property 42 (0.237 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$349,000.00
<b>Indicative Provision Trigger</b>	The first subdivision/development within Catchment J.
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$349,000.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$4,929.87
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_9</b>
<b>DESCRIPTION</b>	Land required (0.258 ha) for the construction of WW01 - property 3 (0.090 ha), property 4a (0.116 ha), property 4b (0.052 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$535,436.00
<b>Indicative Provision Trigger</b>	The first subdivision within Catchment C or D, whichever comes first.
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$535,436.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$7,563.40
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_10</b>
<b>DESCRIPTION</b>	Land required (1.476 ha) for the construction of WW04 - Property 43 (0.288 ha), 401-419 Barwon Heads Road, Charlemont (0.502 ha), 30 Tannery Road, Charlemont (0.595 Ha), 46 Tannery Road, Charlemont (0.091 Ha)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$892,158.00
Indicative Provision Trigger	Upon 50% of the Marshall Catchment Developing
Strategic Justification	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	29%
Project Cost to MCA	\$626,741.00
Demand Units	70.793
Levy Amount	\$8,853.15
Costing Source	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_11</b>
<b>DESCRIPTION</b>	Land required (3.253 ha) for the construction of WW05 - 31-39 tannery Road, Charlemont (0.392 ha), 41-49 Tannery Road, Charlemont (1.472 Ha), 81-89 Tannery Road, Charlemont (1.389 Ha)
Infrastructure Type	Development
Infrastructure Category	Drainage
Project Cost	\$675,715.00
Indicative Provision Trigger	Upon 50% of the Marshall Catchment Developing
Strategic Justification	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
External Apportionment	29%
Project Cost to MCA	\$474,689.79
Demand Units	70.793
Levy Amount	\$6,705.32
Costing Source	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_12</b>
<b>DESCRIPTION</b>	Land required (2.931 ha) for the construction of WLRB03 - property 44 (2.054ha), property 45 (.877 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Drainage
<b>Project Cost</b>	\$6,099,213.00
<b>Indicative Provision Trigger</b>	First subdivision within catchment D or to the satisfaction of the Responsible Authority
<b>Strategic Justification</b>	Land required for drainage infrastructure as required by the Spiire Stormwater Management Strategy 2022.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$6,099,213.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$86,155.59
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_13</b>
<b>DESCRIPTION</b>	Land required (2.03 ha) for the construction of DI_IT_01 and the Bellarine Link Intersection - Property 45 (0.014 ha), Property 6 (0.078 ha), Property 14 (0.111 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Transport
<b>Project Cost</b>	\$339,558.00
<b>Indicative Provision Trigger</b>	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)
<b>Strategic Justification</b>	Land required for intersections as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$339,558.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$4,796.49
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_14</b>
<b>DESCRIPTION</b>	Land required (0.311 ha) for the construction of DI_IT_02 - property 23 (0.071ha), property 25 (0.121 ha), property 26 (0.119 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Transport
<b>Project Cost</b>	\$614,474.00
<b>Indicative Provision Trigger</b>	The first subdivision requiring the intersections delivery in accordance with the Ratio Transport Impact Assessment, 2023)
<b>Strategic Justification</b>	Land required for intersections as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$614,474.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$8,679.87
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_15</b>
<b>DESCRIPTION</b>	Land required (0.430 ha) for the Reserve Road and E/W Connector Road Roundabout - property 4a (0.079 ha), property 6 (0.245 ha), property 12 (0.007 ha), property 14 (0.109 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Transport
<b>Project Cost</b>	\$691,294.00
<b>Indicative Provision Trigger</b>	The first subdivision requiring the associated road section to be delivery in accordance with the Ratio Transport Impact Assessment, 2023)
<b>Strategic Justification</b>	Land required to facilitate the delivery of the road network within the Marshall Precinct as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$691,294.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$9,765.01
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>DI_LA_16</b>
<b>DESCRIPTION</b>	Land required (2.655 ha) for the East/West Connector Road - property 5 (0.275ha), property 6 (0.628 ha), property 14 (0.130 ha), property 15 (0.266 ha), property 16 (0.607 ha), property 24 (0.004ha), property 26 (0.204 ha), property 27 (0.347 ha), property 29 (0.194 ha)
Infrastructure Type	Development
Infrastructure Category	Transport
Project Cost	\$4,410,499.00
Indicative Provision Trigger	The first subdivision requiring the associated road section to be delivery in accordance with the Ratio Transport Impact Assessment, 2023)
Strategic Justification	Land required to facilitate the delivery of the road network within the Marshall Precinct as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
External Apportionment	0%
Project Cost to MCA	\$4,410,499.00
Demand Units	70.793
Levy Amount	\$62,301.34
Costing Source	CBRE Land Valuation Report, August 2023

## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>DI_LA_17</b>
<b>DESCRIPTION</b>	Land required (0.839 ha) for the Drews Road Widening/Realignment - property 1 (0.110ha), property 2 (0.361ha), property 6 (0.005 ha), property 14 (0.009 ha), property 4a (0.248 ha), Property 4b (0.106 ha)
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Transport
<b>Project Cost</b>	\$1,602,526.00
<b>Indicative Provision Trigger</b>	The first subdivision requiring the associated road section to be delivery in accordance with the Ratio Transport Impact Assessment, 2023)
<b>Strategic Justification</b>	Land required to facilitate the delivery of the road network within the Marshall Precinct as identified in the Marshall Transport Infrastructure Assessment 2019 (Cardno) and updated within Marshall Transport Impact Assessment 2023 (Ratio)
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$1,602,526.00
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$22,636.79
<b>Costing Source</b>	CBRE Land Valuation Report, August 2023

<b>ITEM CODE</b>	<b>CI_OS_01</b>
<b>DESCRIPTION</b>	Regional Active Open Space (northern) - Community Pavillion
<b>Infrastructure Type</b>	Community
<b>Infrastructure Category</b>	Community
<b>Project Cost</b>	\$3,500,404.84
<b>Indicative Provision Trigger</b>	Please refer to HSB DCP DI_OS_01 Provision Trigger.
<b>Strategic Justification</b>	Horseshoe Bend PSP Regional Openspace infrastructure in which services the Marshall Precinct
<b>External Apportionment</b>	80%
<b>Project Cost to MCA</b>	\$700,080.97
<b>Demand Units</b>	1555
<b>Levy Amount</b>	\$450.21
<b>Costing Source</b>	Prowse Quantity Surveyors 1 Sept 2014

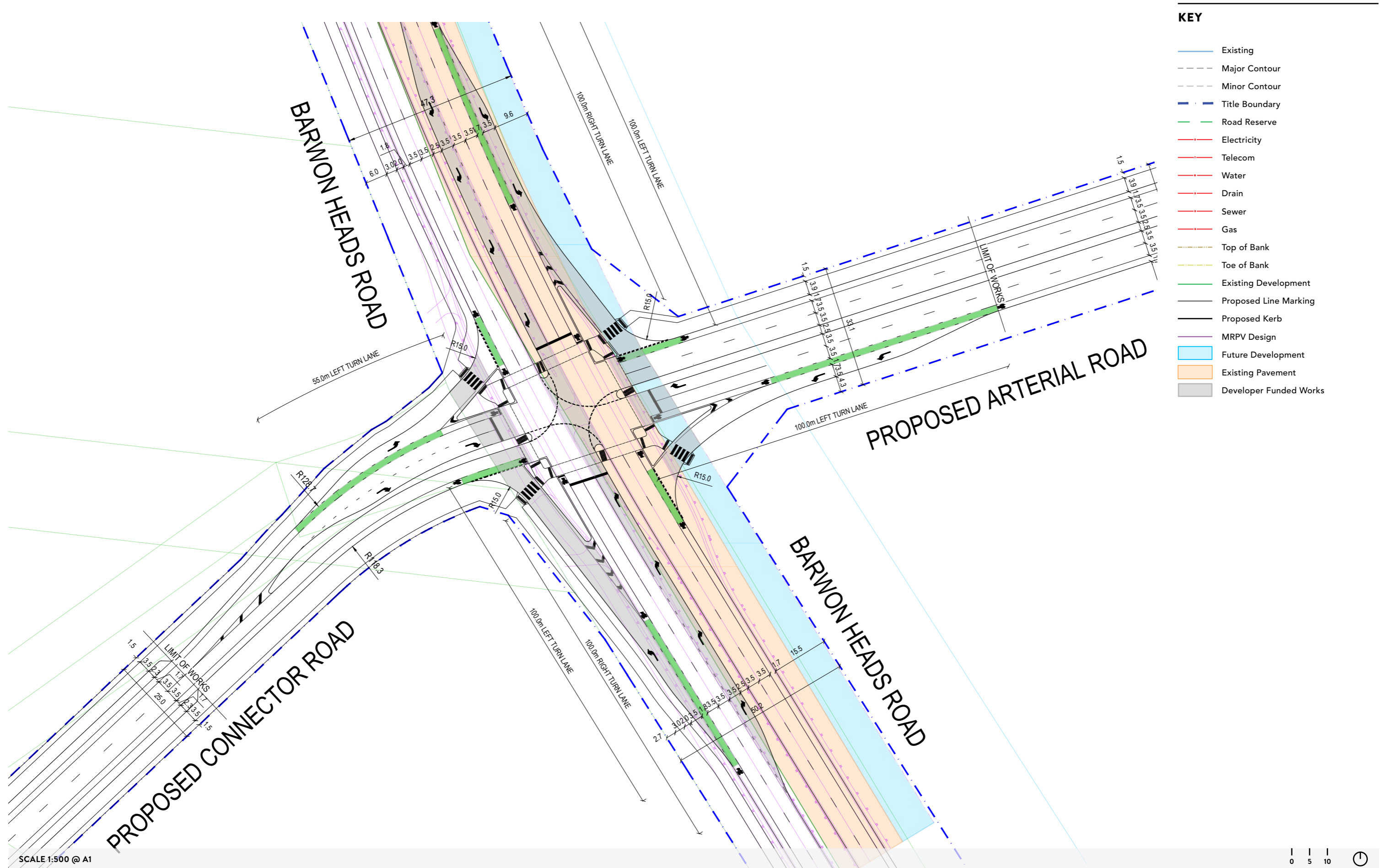
## 7.4 APPENDIX B – PROJECT SHEETS

<b>ITEM CODE</b>	<b>CI_CF_01</b>
<b>DESCRIPTION</b>	Library - construction (contribution) - Contribution to the construction of the regional library in the Armstrong Creek Town Centre
<b>Infrastructure Type</b>	Community
<b>Infrastructure Category</b>	Community
<b>Project Cost</b>	\$21,865,000.00
<b>Indicative Provision Trigger</b>	Refer to ACTC DCP CI_CF_01 Provision Trigger . Currently under construction.
<b>Strategic Justification</b>	Contribution to community facilities required to serve the new resident population in line with the Armstrong Creek Integrated Infrastructure Delivery Plan 2009, the K2 Planning Report 2012, CoGG Social Infrastructure Plan 2020-2023 and Library Infrastructure Development Plan, 2019 (GRLC).
<b>External Apportionment</b>	93%
<b>Project Cost to MCA</b>	\$1,473,701.00
<b>Demand Units</b>	1555
<b>Levy Amount</b>	\$947.72
<b>Costing Source</b>	Zinc Cost Management, June 2023

<b>ITEM CODE</b>	<b>DI_PL_01</b>
<b>DESCRIPTION</b>	Planning Costs associated with the preparation of the Marshall PSP and DCP
<b>Infrastructure Type</b>	Development
<b>Infrastructure Category</b>	Planning Costs
<b>Project Cost</b>	\$600,578.80
<b>Indicative Provision Trigger</b>	Complete
<b>Strategic Justification</b>	Required for the comprehensive planning of the Marshall Precinct.
<b>External Apportionment</b>	0%
<b>Project Cost to MCA</b>	\$600,578.80
<b>Demand Units</b>	70.793
<b>Levy Amount</b>	\$8,483.59
<b>Costing Source</b>	Planning Costings Sheet, 2023

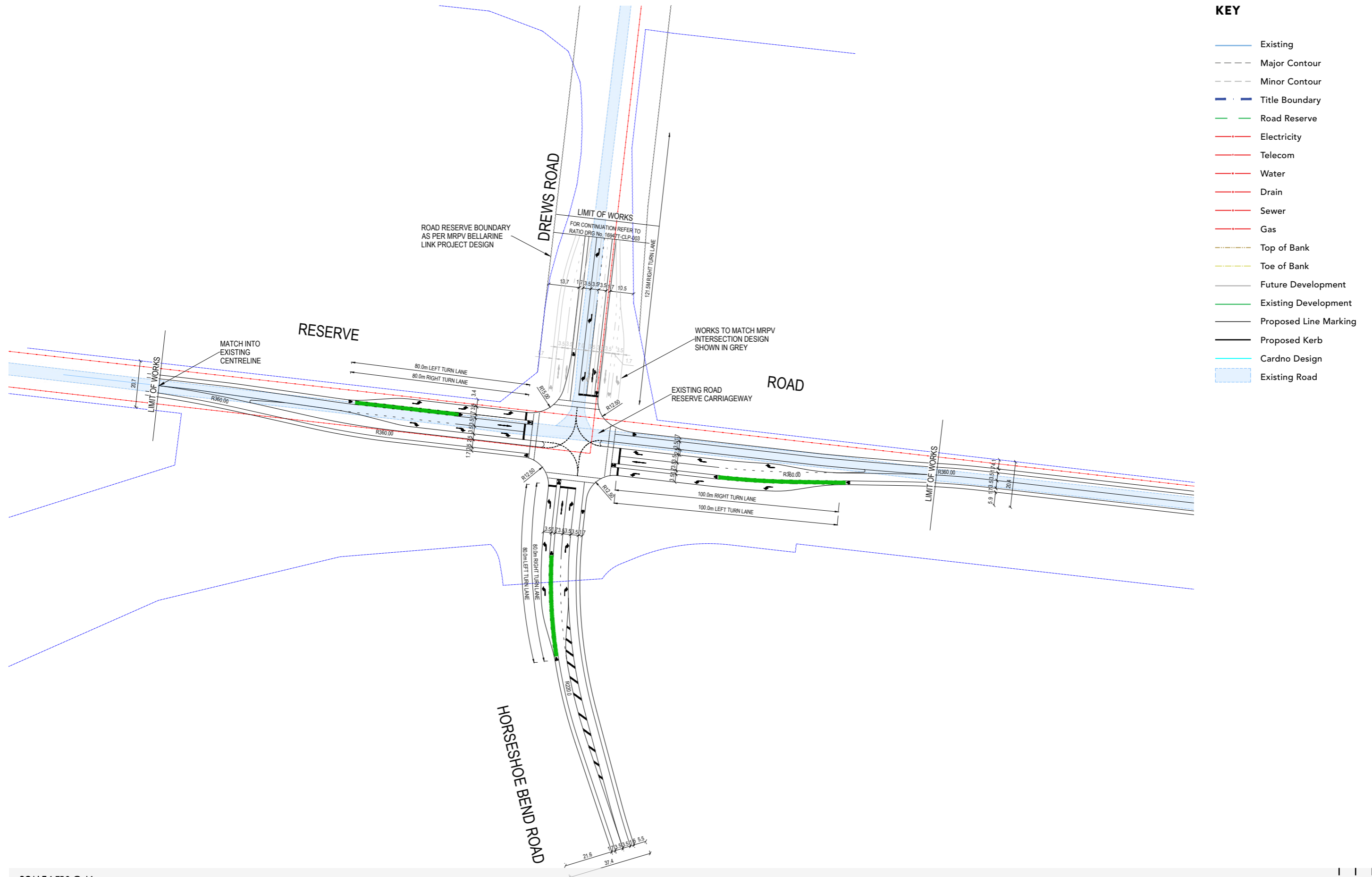
# 7.3 APPENDIX C – INTERSECTION PLANS

## P10. BARWON HEADS RD INTERSECTION PLAN



# 7.3 APPENDIX C – INTERSECTION PLANS

## P11. HORSESHOE BED, DREW, RESERVE RD INTERSECTION PLAN



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**CUSTOMER SERVICE CENTRE**

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Wadawurrung Country

137-149 Mercer Street,

Geelong. 8.00am – 5.00pm

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