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Proposed C432ggee

## **SCHEDULE 50 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY**

Shown on the planning scheme map as **DDO50**

**40-44 LONSDALE STREET, 146 SWANSTON STREET & 51 CARR STREET SOUTH  
GEELONG**

### **1.0**

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#### **Design objectives**

To develop an ecologically sustainable, transit orientated precinct that incorporates best practice environmental management.

To support a mix of high-quality high and medium density built form that has an appropriate interface to adjacent streets and creates a landmark building fronting the station plaza north of the railway line.

To create high-quality landscaped and safe public spaces that ensure a visual connection across the railway line at Bellerine Street, celebrates the beginning of the Bellarine Rail Trail and delivers a legible and coherent street structure.

To improve pedestrian connections within and around the subject site and contribute to improved street environments resulting from the closure and redirection of Carr Street.

To protect the internal residential amenity of new buildings by limiting noise transmission and provide acoustic measures to protect from external noise sources such as train and public spaces.

### **2.0**

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#### **Buildings and works**

The following buildings and works requirements apply to an application to construct a building or construct or carry out works.

##### **General requirements**

Development should be generally in accordance with Map 1 to this schedule.

Support the development of a pedestrian link over the railway line along the Bellerine/Carr Street alignment generally in accordance with Map 3 to this schedule.

##### **Building height**

Development should not exceed the preferred maximum building heights as shown in Table 1 and Map 2 to this schedule.

The preferred maximum building height does not include architectural features, masts and building services including plant rooms, air conditioning, lift overruns, structures associated with roof top gardens, decks and communal outdoor spaces and their ancillary facilities or enclosed stairwells provided that the following criteria are met:

- Not more than 50% of the roof area is occupied by equipment (other than solar panel or greening);
- The equipment is located to minimise additional overshadowing and reduce visual impact;
- The equipment does not exceed the height limit by more than 3.6 metres; and
- The equipment and screening is integrated into the design of the building to the satisfaction of the responsibly authority.

Development should meet the following minimum floor to floor dimensions:

- 4 metres at ground level
- 3.2 - 3.5 metres for residential and non-residential uses in the levels above.

##### **Street wall heights**

Street wall heights should be generally in accordance with the maximum street wall heights specified in Table 1 and Map 2 to this schedule.

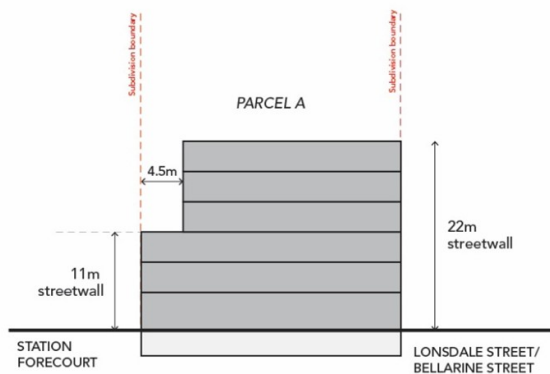
**Setbacks**

Development should be generally in accordance with the preferred setbacks specified in Table 1 and Figures 1 to 5.

**Table 1 to Schedule 50 to Clause 43.02**

Parcel	Preferred Maximum Building Height	Preferred Maximum Street Wall Height - Carr Street	Preferred Maximum Street Wall Height - Lonsdale Street	Preferred Above Street Wall Setbacks	Preferred Other Setbacks
A	22 metres (6 storeys)	11 metres (3 storeys) to Station Forecourt	22 metres (6 storeys)	4.5 metres to Station Forecourt	n/a
B	18 metres - 22 metres (5 - 6 storeys)	11 metres (3 storeys)	11 metres (3 storeys)	4.5 metres to Lonsdale Street 3 metres to Carr Street, laneway and any new pedestrian link	4.5 metres to eastern boundary at ground level for new shared street. 6 metres at ground level to new pedestrian links.
C	11 metres (3 storeys)	n/a	n/a	n/a	4.5 metres to western boundary at ground level for new shared street. 4 metres at ground level to new pedestrian links.

**Figure 1: Parcel A fronting Lonsdale Street, Bellarine Street, and Future Station Forecourt**



**Figure 2: Parcel B fronting Lonsdale Street and Carr Street**

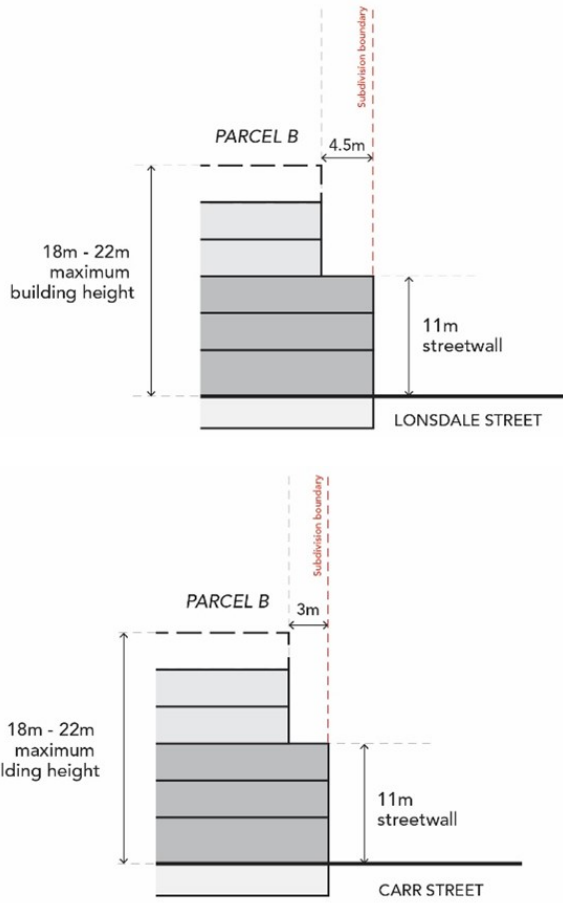


Figure 3: Setback to existing laneway for Parcel C

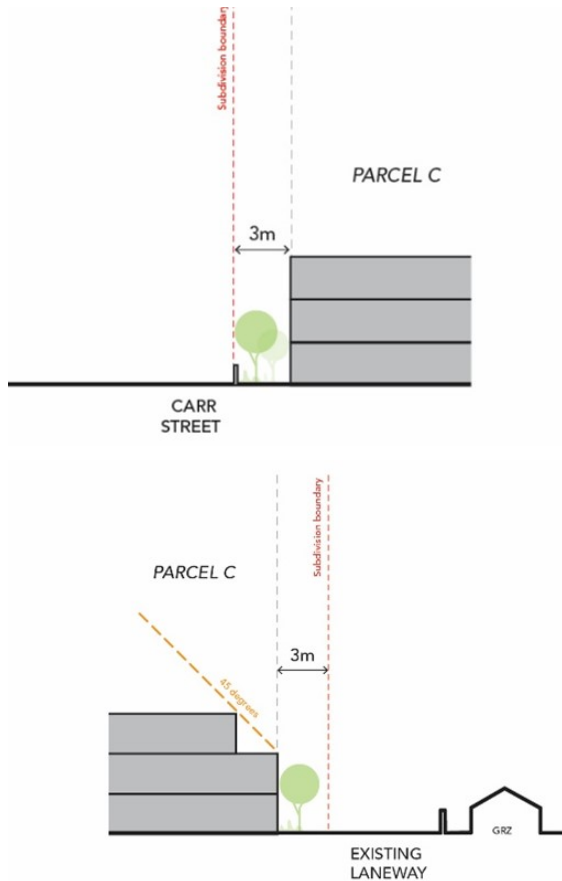
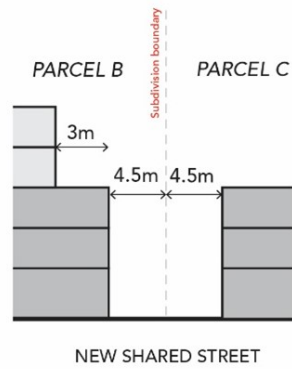
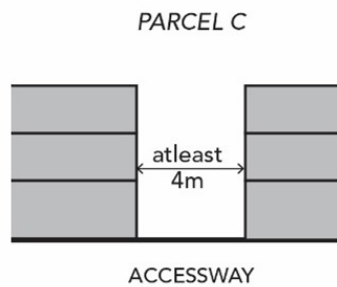
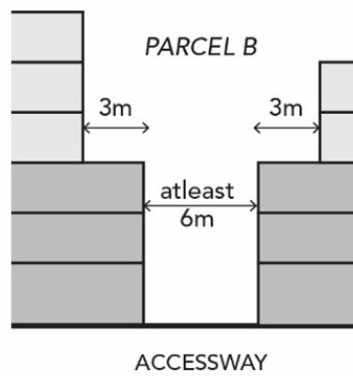


Figure 4: Setback to new shared street



**Figure 5: Setback to new pedestrian links**



**Building Separation**

Development should provide a minimum of 9 metres distance between towers above the maximum street wall height of 11 metres.

**Building Design**

Establish a low scale podium and reduce the visual prominence of the upper levels with setbacks as indicated in Table 1 and Figure 1, 2 and 4 for Parcel A and B.

Ensure that Parcel A presents as a distinctive landmark building of high architectural quality, providing a point of positive reference to South Geelong Station and further enhances the character, function, and appearance of the surrounding area, including the public realm.

Design buildings to address front, oblique and side views.

Mitigate the impact of visual bulk with well-articulated facades that include variation in forms, materials, openings, and recesses and building separation in upper levels.

Create visual interest in upper levels through the provision of balconies, eaves, terraces, and verandas that are visually recessed from adjoining streets and laneways but ensures passive surveillance of the public realm.

Avoid highly reflective glazing in the façade that diminishes the ability of the development to fit within the historic context.

Avoid blank walls visible from the public realm including where they abut a street or laneway. Include an interim façade strategy when blank walls are visually prominent while adjoining properties are being realised.

Introduce midblock pedestrian links connecting Lonsdale Street to Carr Street as indicated in Map 3.

Ensure new pedestrian connections are safe (comply with Crime Prevention Through Environmental Design Guidelines), receive adequate access to sunlight and are open to the sky.

### **Public Realm Interface**

Provide opportunities for active surveillance of the public realm including plazas and where practical laneways and car parks. Avoid concealed alcoves and hidden entrances that affect perceived safety.

Maximise the usability and attractiveness of new public spaces by introducing ground floor uses that ensure interaction with the street and enhance the pedestrian environment.

Provide ground floor tenancy frontages that can accommodate a wide range of commercial and retail uses while maintaining the fine grain character of the area on Parcels A and B.

Avoid large signage and/or non-transparent glazing at ground level.

Provide separate residential and commercial entrances that are clearly legible from the street. Dwellings at the ground level should be provided with individual entrances from the footpath.

Create opportunities for landscaping, particularly along the existing and new pedestrian links, laneways and shared street.

Incorporate façade design and lighting that establishes a sense of safety and security after hours.

Provide active frontages to both sides of pedestrian connections through development where possible.

Ensure direct, attractive, safe, and well-lit pedestrian and cycle access between main streets and South Geelong Station.

### **Overshadowing**

Development should not overshadow the opposite footpath of adjoining streets (excluding Carr Street) between 10:00 am and 3:00 pm on 22 September.

Minimise the impact on solar access from new development to established residential areas, including balconies, terraces and habitable room windows.

### **Wind and weather protection**

Ensure safe wind conditions as specified in Table 2 on public land, publicly accessible areas on private land, private open space and communal open space; and

Achieve comfortable wind conditions as specified in Table 2 on public land and publicly accessible areas on private land.

Avoid the addition of protective screens and other incidental add-ons to offset excessive wind gust levels. Discourage landscaping within public spaces as a means to mitigate wind.

Incorporate continuous weather protection such as awnings, openings and architectural detail that promotes activity and visual interest. Ensure the soffit of awnings are well detailed and attractive when viewed from the street.

Ensure weather protection technique employed does not impinge existing or future street trees.

**Table 2 to Schedule 50 to Clause 43.02**

<b>Wind condition</b>	<b>Requirement</b>
Comfortable wind conditions	Hourly mean wind speed or gust equivalent mean speed (3 second gust wind speed divided by 1.85), from all wind directions combined with probability of exceedance less than 20% of the time, equal to or less than: <ul style="list-style-type: none"> <li>▪ 3 metres per second for sitting areas,</li> <li>▪ 4 metres per second for standing areas,</li> <li>▪ 5 metres per second for walking areas.</li> </ul>
Unsafe wind conditions	Annual maximum 3 second gust wind speed exceeding 20 metres per second with a probability of exceedance of 0.1% considering at least 16 wind directions.

**Acoustic attenuation**

Incorporate acoustic treatments to limit noise transmission from external noise sources such as trains, train station, Kardinia Park & Stadium, entertainment venues and public spaces to a level that is comfortable for residential uses.

Incorporate noise attenuation measures and suppression techniques to ensure noise does not unreasonably affect the amenity of nearby land within a General Residential Zone and Residential Growth Zone.

A permit must contain conditions which give effect to the Acoustic Assessment Report, where the land has been identified as a lot that requires mitigation measures against noise impacts.

**Sustainable Design**

Provide green roofs where viable in accessible and serviceable locations and where visible from the public realm or within the development.

Ensure landscaping includes high-quality finishes, well-designed seating areas, lighting and places for diverse uses.

**Access, Parking and Loading Areas**

Provide an easily identifiable entrance and sense of arrival and safety through placing the primary pedestrian entrances to dwellings and tenancies on the street frontage.

Minimise the impact of vehicle access and car parking on the public realm by:

- Consolidating vehicular entries and locate vehicle access at the rear of buildings generally in accordance with Map 3 (Parcel A via Bellerine Street, and Parcel B and C via future shared street or existing laneway).
- Locating private car parking where it cannot be seen from the public realm.
- Designing basement or carparking garage entrance to have minimal visual impact.

Provide car parking within the basement or conceal it from the public realm if located within the podium by sleeving with active uses.

Support shared car parking arrangements across the site.

Design all above ground car parking areas to support adaptive reuse over time.

Support active transport through the supply of bicycle parking in both commercial and residential development and change facilities for commercial development.

Provide centralised loading areas for commercial uses across the site away from pedestrian priority areas.

Provide a central bin storage area for each development which is not visible from the street.

### 3.0 Subdivision

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A permit to subdivide land must meet the following requirements:

- Supports the partial closure and reconfiguration of Carr Street establishing land parcels generally in accordance with Map 1.

An application to subdivide land into lots each containing an existing dwelling or car parking space is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act.

### 4.0 Signs

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None specified

### 5.0 Application requirements

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The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

An Urban Context Report and Design Response which:

- Responds to the urban context (including built form character of adjacent and nearby buildings, adjacent and nearby heritage places, and equitable outcomes for potential development on adjoining sites);
- Implements recommendations from the other technical reports required under the application requirements of this schedule;
- Responds to the design objectives and buildings and works requirements of this schedule; and
- Provides design excellence through providing sustainable, high quality architecture which articulates and minimises visual bulk, and landscape architecture and urban design enforcing liveability and activating the public realm.

An Environmentally Sustainable Design Management Plan (EMP) prepared by a suitably qualified environmental engineer or equivalent that demonstrates how the development provides for best practice environmentally sustainable design.

A wind report for buildings exceeding a height of 18 metres (5 storeys).

Any application for subdivision or development of land for Accommodation, Education Centre (other than Tertiary institution and Employment training centre) or Hospital, must be accompanied by an acoustic assessment report prepared by a qualified acoustic engineer or other suitably skilled person to the satisfaction of the responsible authority which:

- Applies the following noise objectives:
  - Not greater than 35 dB LAeq,8h when measured within a sleeping area between 10 pm and 6 am.

- Not greater than 40 dB LAeq,16h when measured within a living area between 6 am and 10 pm.
- For areas other than sleeping and living areas, not greater than the median value of the range of recommended designed sound levels of Australian Standard AS/NZ 2107:2016 (Acoustics – Recommended design sound level and reverberation times for building interiors).
- Noise levels should be assessed:
  - Considering the cumulative noise from all sources impacting on the proposal including road traffic noise and industry noise, as well as potential other potential noise sources; and
  - In unfurnished rooms with a finished floor and the windows closed and be based on average external noise levels measured as part of a noise level assessment.
- Addresses noise compatible design for buildings, with siting, orientation, and internal layout, to be considered prior to setting building envelope performance requirements.
- Addresses potential noise character (such as tonality, impulsiveness or intermittency) is addressed wherever relevant, including through the application of adjustments to the internal noise levels that are determined using the procedures to adjust industry noise levels of the Noise Protocol.

A Traffic Impact Assessment Report (TIAR), where relevant.

A Circulation and Movement Plan prepared by a suitably qualified person that demonstrates how the internal road hierarchy, pedestrian and cycling priority areas, and loading and building services achieve a legible and coherent structure improving the pedestrian connectivity and creating safe public spaces.

Plans, elevations, and section drawings for any car parking proposed at or above ground level to show finished floor levels and a statement by a suitably qualified engineer to demonstrate the capacity for this to be adapted to alternative uses.

A Landscape Plan detailing proposed hard and soft landscape elements, plant schedule, plant container details and maintenance and irrigation systems.

Scaled shadow diagrams to show existing and proposed shadows at hourly intervals between 10:00 am and 3:00 pm on 22 September, to demonstrate compliance with overshadowing requirements.

A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development.

Any application for development of land for a dwelling including a dwelling as part of a mixed-use development should provide an Affordable Housing Delivery Strategy to the satisfaction of the responsible authority which sets out:

- How affordable housing is to be delivered in accordance with social and affordable housing policy at Clause 16.01 of the Planning Scheme.
- The method of implementing the strategy, such as by an agreement under Section 173 of the Planning and Environment Act 1987.
- Locations for the affordable housing to be delivered.
- A summary of the range of housing types, densities and sizes.
- Staging requirements and ensuring that affordable housing is provided in a timely manner as development occurs.

## 6.0

### Decision guidelines

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The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

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- Whether the development appropriately responds to the design objectives and requirements including building heights, street wall heights and setbacks in this schedule.
- Whether the development or subdivision is generally in accordance with the maps included in this schedule
- Whether the subdivision of the site supports the partial closure and reconfiguration of Carr Street and ensures equitable development opportunities to all three sites.
- Whether the development employs an appropriate massing strategy that reduces visual bulk.
- Whether the development supports high-quality pedestrian amenity within the public realm, in relation to human scale and overshadowing.
- Whether the buildings adopt and enables a diversity of forms, typologies, and architectural language.
- Whether the application incorporates acoustic treatments to limit the impact from noise from railway operation and activities on Kardinia Park and Stadium.
- Whether the application includes an Affordable Housing Delivery Strategy to the satisfaction of the responsible authority.
- Whether the development incorporates ecologically sustainable design practices.
- Whether the proposal allows for open sightlines and high levels of passive surveillance by users and residents.
- Whether the development avoids:
  - Extensive blank walls along streets.
  - Buildings oriented towards internal spaces rather than streets.
  - Alcoves and recesses that may provide hiding places or that may collect dirt and litter
- Whether the development provides active street frontages including integration of required servicing into the façade away from key pedestrian spaces and public spaces, co-location of service cabinets internal to loading, waste or parking areas where possible.
- Whether the development supports a staged development of the site in a manner which effectively manages car parking demand and access, enhances pedestrian connections and public realm within and around the Station Hub.
- Whether car parking demand can be appropriately managed.
- Whether the development achieves design excellence by providing high quality and innovative architecture, landscape architecture and urban design which demonstrates function, liveability, sustainability and public contribution to buildings and urban spaces.
- Whether the development achieves appropriate levels of residential density and is not an underdevelopment of the site.

### **Variations to preferred requirements**

Where an application proposes to exceed, or vary a preferred requirement under a discretionary control contained within this schedule consider:

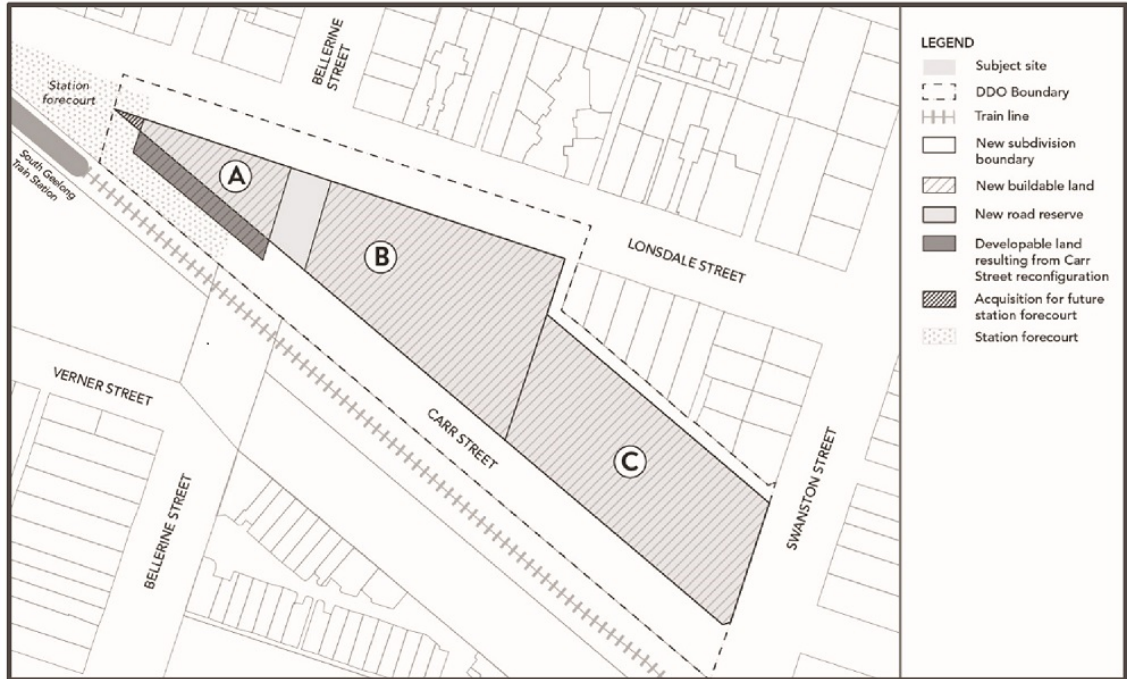
- Whether the design objectives have been met.
- Whether the street wall heights and upper level setbacks of development minimises the impact on the character and appearance of any Heritage Area opposite or adjacent to the site.
- Whether a variation to a requirement avoids or minimises adverse off-site impacts such as visual bulk, overlooking and overshadowing to adjacent residential properties and the public realm.

## **GREATER GEELONG PLANNING SCHEME**

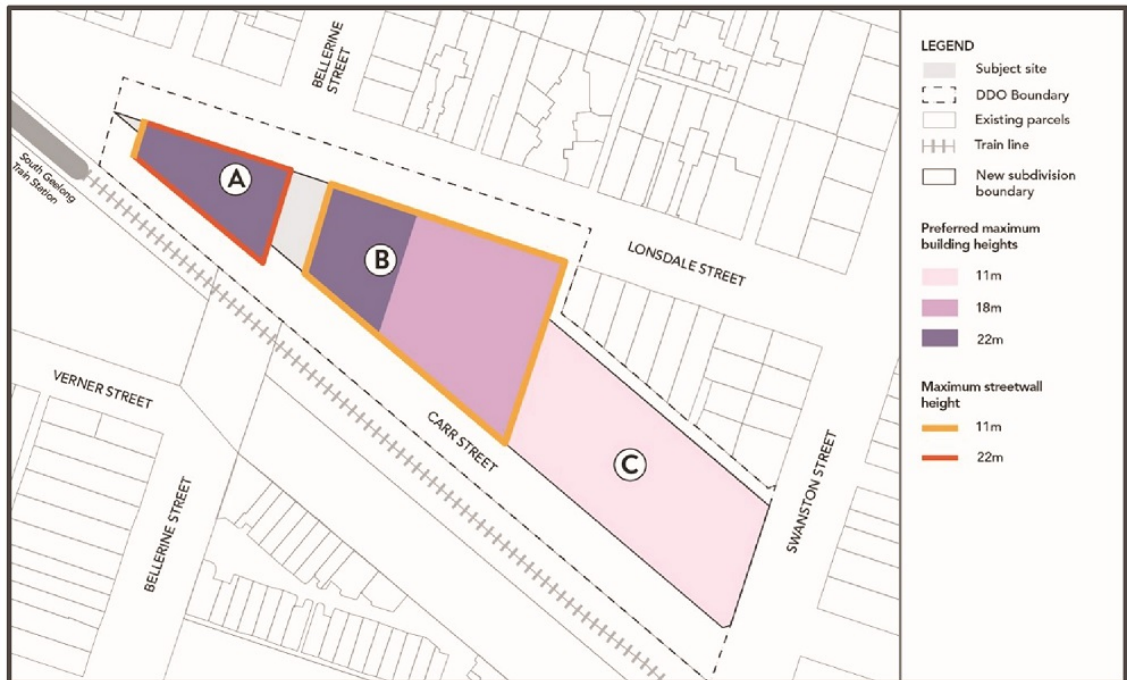
- Whether the proposal can enable a variation to a requirement without material adverse offsite impacts.
- Whether the proposal presents, or substantially facilitates an improved architectural outcome.

GREATER GEELONG PLANNING SCHEME

Map 1 to Schedule 50 to Clause 43.02



Map 2 to Schedule 50 to Clause 43.02



Map 3 to Schedule 50 to Clause 43.02

# GREATER GEELONG PLANNING SCHEME

