

**11.03**  
31/07/2018  
VC148

**PLANNING FOR PLACES**

**11.03-1S**03/02/2022  
VC199**Activity centres****Objective**

To encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

**Strategies**

Build up activity centres as a focus for high-quality development, activity and living by developing a network of activity centres that:

- Comprises a range of centres that differ in size and function.
- Is a focus for business, shopping, working, leisure and community facilities.
- Provides different types of housing, including forms of higher density housing.
- Is connected by transport.
- Maximises choices in services, employment and social interaction.

Support the role and function of each centre in the context of its classification, the policies for housing intensification, and development of the public transport network.

Undertake strategic planning for the use and development of land in and around activity centres.

Give clear direction on preferred locations for investment.

Encourage a diversity of housing types at higher densities in and around activity centres.

Reduce the number of private motorised trips by concentrating activities that generate high numbers of (non-freight) trips in highly accessible activity centres.

Improve access by walking, cycling and public transport to services and facilities.

Support the continued growth and diversification of activity centres to give communities access to a wide range of goods and services, provide local employment and support local economies.

Encourage economic activity and business synergies.

Improve the social, economic and environmental performance and amenity of activity centres.

**Policy documents**

Consider as relevant:

- *Urban Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2017)
- *Apartment Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2021)
- *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021)

**11.03-1L**07/07/2022  
C417ggee**Activity centres in Greater Geelong****Objectives**

To support the role and function of centres in the Geelong Retail Centre Hierarchy.

To support continued diversification of retail centres over time.

**General strategies**

Encourage a mix of retail, office, cafes, higher density housing, education and community facilities to locate within centres, but particularly in Regional (Central Geelong), Sub-regional and Town Centres.

Support accommodation uses above ground level floor space in retail and activity centres subject to provision of parking and access requirements to meet the needs of users.

Direct entertainment and cultural activities to Central Geelong and support local events in other centres.

Encourage the redevelopment of car parking areas for commercial and residential uses where adequate car parking to meet the needs of existing and future uses can be achieved.

Support increased intensity and vertical growth of centres to encourage expansion and to reduce the need to develop new centres.

**Retail hierarchy strategies**

Direct new retail development to existing centres, consistent with the role and function described in the Geelong Retail Centre Hierarchy at Clause 02.03-1.

Support the development of discount department stores in Central Geelong and sub-regional centres over lower order centres.

Encourage development that supports a diversity of retail forms across the hierarchy.

Ensure planned centres in growth area are consistent with the role, function, size and type of centre as set out in the Geelong Retail Centre Hierarchy at Clause 02.03-1.

**Residential Growth Zone land strategies**

Ensure that retail use and development on land within the Residential Growth Zone:

- Has a functional relationship with an existing activity centre.
- Is designed and sited to operate without amenity impacts to neighbouring residents.

**Commercial and industrial land strategies**

Encourage retail use and development in industrial areas only if associated with an industrial use occurring on the land or where it will meet the needs of people employed in the area.

Encourage use and development that will provide for strategic employment land.

Ensure that use or development for a new or expanded supermarket-based centre within the Commercial 2 Zone or Industrial 3 Zone:

- Does not have a significant economic impact on a nearby centre(s) identified within the Geelong Retail Centre Hierarchy at Clause 02.03-1.
- Provides a net community benefit.

**Restricted retail uses strategies**

Direct restricted retail use and development to nominated restricted retail precincts as identified in the Geelong Retail Centre Hierarchy at Clause 02.03-1.

Support the development of smaller scale, higher density restricted retail uses such as homewares within activity centres.

**Policy document**

Consider as relevant:

- *City of Greater Geelong Retail Strategy 2020-2036* (City of Greater Geelong and SGS Economics and Planning, August 2020)

**11.03-2S**04/05/2022  
VC210**Growth areas****Objective**

To locate urban growth close to transport corridors and services and provide efficient and effective infrastructure to create sustainability benefits while protecting primary production, major sources of raw materials and valued environmental areas.

**Strategies**

Concentrate urban expansion into growth areas that are served by high-capacity public transport.

Implement the strategic directions in the Growth Area Framework Plans.

Encourage average overall residential densities in the growth areas of a minimum of 15 dwellings per net developable hectare, and over time, seek an overall increase in residential densities to more than 20 dwellings per net developable hectare.

Deliver timely and adequate provision of public transport and local and regional infrastructure and services, in line with a preferred sequence of land release.

Provide for significant amounts of local employment opportunities and in some areas, provide large scale industrial or other more regional employment generators.

Create a network of mixed-use activity centres that are high quality, well designed and create a sense of place.

Provide a diversity of housing type and distribution.

Retain unique characteristics of established areas impacted by growth.

Protect and manage natural resources and areas of heritage, cultural and environmental significance.

Create well planned, easy to maintain and safe streets and neighbourhoods that reduce opportunities for crime, improve perceptions of safety and increase levels of community participation.

Develop Growth Area Framework Plans that will:

- Include objectives for each growth area.
- Identify the long term pattern of urban growth.
- Identify the location of broad urban development types, for example activity centre, residential, employment, freight centres and mixed use employment.
- Identify the boundaries of individual communities, landscape values and, as appropriate, the need for discrete urban breaks and how land uses in these breaks will be managed.
- Identify transport networks and options for investigation, such as future railway lines and stations, freight activity centres, freeways and arterial roads.
- Identify the location of open space to be retained for recreation, and/or biodiversity protection and/or flood risk reduction purposes guided and directed by regional biodiversity conservation strategies.
- Show significant waterways as opportunities for creating linear trails, along with areas required to be retained for biodiversity protection and/or flood risk reduction purposes.
- Identify appropriate uses for constrained areas, including quarry buffers.

Develop precinct structure plans consistent with the *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021) approved by the Minister for Planning to:

- Establish a sense of place and community.
- Create greater housing choice, diversity and affordable places to live.
- Create highly accessible and vibrant activity centres.

## GREATER GEELONG PLANNING SCHEME

- Provide for local employment and business activity.
- Provide better transport choices.
- Respond to climate change and increase environmental sustainability.
- Deliver accessible, integrated and adaptable community infrastructure.

### **Policy documents**

Consider as relevant:

- Any applicable Growth Area Framework Plans (Department of Sustainability and Environment, 2006)
- *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021)
- *Ministerial Direction No. 12 – Urban Growth Areas*

**11.03-3S**

31/07/2018  
VC148

**Peri-urban areas**

**Objective**

To manage growth in peri-urban areas to protect and enhance their identified valued attributes.

**Strategies**

Identify and protect areas that are strategically important for the environment, biodiversity, landscape, open space, water, agriculture, energy, recreation, tourism, environment, cultural heritage, infrastructure, extractive and other natural resources.

Provide for development in established settlements that have capacity for growth having regard to complex ecosystems, landscapes, agricultural and recreational activities including in Warragul-Drouin, Bacchus Marsh, Torquay-Jan Juc, Gisborne, Kyneton, Wonthaggi, Kilmore, Broadford, Seymour and Ballan and other towns identified by Regional Growth Plans as having potential for growth.

Establish growth boundaries for peri-urban towns to avoid urban sprawl and protect agricultural land and environmental assets.

Enhance the character, identity, attractiveness and amenity of peri-urban towns.

Prevent dispersed settlement and provide for non-urban breaks between urban areas.

Ensure development is linked to the timely and viable provision of physical and social infrastructure.

Improve connections to regional and metropolitan transport services.

**11.03-4S**20/03/2023  
VC229**Coastal settlement****Objective**

To plan for sustainable coastal development.

**Strategies**

Plan and manage coastal population growth and increased visitation so that impacts do not cause unsustainable use of coastal resources.

Support a network of diverse coastal settlements that provide for a broad range of housing types, economic opportunities and services.

Identify a clear settlement boundary around coastal settlements to ensure that growth in coastal areas is planned and coastal values are protected. Where no settlement boundary is identified, the extent of a settlement is defined by the extent of existing urban zoned land and any land identified on a plan in the planning scheme for future urban settlement.

Minimise linear urban sprawl along the coastal edge and ribbon development in rural landscapes.

Protect areas between settlements for non-urban use.

Limit development in identified coastal hazard areas, on ridgelines, primary coastal dune systems, shorelines of estuaries, wetlands and low-lying coastal areas, or where coastal processes may be detrimentally impacted.

Encourage the restructure of old and inappropriate subdivisions to reduce development impacts on the environment.

Ensure a sustainable water supply, stormwater management and sewerage treatment for all development.

Minimise the quantity and enhance the quality of stormwater discharge from new development into the ocean, bays and estuaries.

Prevent the development of new residential canal estates.

**Policy documents**

Consider as relevant:

- *G21 Regional Growth Plan* (Geelong Region Alliance, 2013)
- *Gippsland Regional Growth Plan* (Victorian Government, 2014)
- *Great South Coast Regional Growth Plan* (Victorian Government, 2014)
- *Marine and Coastal Policy* (Department of Environment, Land, Water and Planning, 2020)
- *Marine and Coastal Strategy* (Department of Environment, Land, Water and Planning, 2022)
- *Siting and Design Guidelines for Structures on the Victorian Coast* (Department of Environment, Land, Water and Planning, 2020)

**11.03-5S**30/04/2021  
VC185**Distinctive areas and landscapes****Objective**

To recognise the importance of distinctive areas and landscapes to the people of Victoria and protect and enhance the valued attributes of identified or declared distinctive areas and landscapes.

**Strategies**

Recognise the unique features and special characteristics of these areas and landscapes.

Implement the strategic directions of approved Localised Planning Statements and Statements of Planning Policy.

Integrate policy development, implementation and decision-making for declared areas under Statements of Planning policy.

Recognise the important role these areas play in the state as tourist destinations.

Protect the identified key values and activities of these areas.

Enhance conservation of the environment, including the unique habitats, ecosystems and biodiversity of these areas.

Support use and development where it enhances the valued characteristics of these areas.

Avoid use and development that could undermine the long-term natural or non-urban use of land in these areas.

Protect areas that are important for food production.

**Policy documents**

Consider as relevant:

- *Bellarine Peninsula Localised Planning Statement* (Victorian Government, 2015)
- *Macedon Ranges Statement of Planning Policy* (Victorian Government, 2019)
- *Mornington Peninsula Localised Planning Statement* (Victorian Government, 2014)
- *Yarra Ranges Localised Planning Statement* (Victorian Government, 2017)

**11.03-5R**20/03/2023  
VC229**The Great Ocean Road region****Objective**

To manage the sustainable development of the Great Ocean Road region.

**Strategies**

Protect public land and parks and identified significant landscapes in the Great Ocean Road region.

Ensure development responds to the identified landscape character of the area.

Manage the growth of towns by:

- Respecting the character of coastal towns and promoting best practice design for new development.
- Directing urban growth to strategically identified areas.

Manage the impact of development on catchments and coastal areas.

Manage the impact of development on the environmental and cultural values of the area.

Improve the management of access and transport by:

- Managing the Great Ocean Road for tourism and regional access.
- Enhancing the safety and travelling experience of the Great Ocean Road.
- Improving the safety and operational performance of the inland routes from the Princes Highway to the Great Ocean Road.
- Providing travel choices to and in the region.

Encourage sustainable tourism and resource use by:

- Developing a network of tourism opportunities throughout the region.
- Supporting tourism activities that provide environmental, economic and social benefits.
- Supporting the land use and transport needs of key regional industries including tourism.
- Using natural resources with care.

**Policy documents**

Consider as relevant:

- *Marine and Coastal Policy* (Department of Environment, Land, Water and Planning, 2020)
- *Marine and Coastal Strategy* (Department of Environment, Land, Water and Planning, 2022)
- *The Great Ocean Road Region Landscape Assessment Study* (Department of Sustainability and Environment, 2003)
- *The Great Ocean Road Region - A Land Use and Transport Strategy* (Department of Sustainability and Environment, 2004)

**11.03-6S**

31/07/2018  
VC148

**Regional and local places**

**Objective**

To facilitate integrated place-based planning.

**Strategies**

Integrate relevant planning considerations to provide specific direction for the planning of sites, places, neighbourhoods and towns.

Consider the distinctive characteristics and needs of regional and local places in planning for future land use and development.

**11.03-6L-01 Bellarine Peninsula**

07/07/2022  
C417ggee

**Policy application**

This policy applies to land shown on the following maps forming part of this clause:

- St Leonards Structure Plan Map
- Portarlington Structure Plan Map
- Indented Head Structure Plan Map
- Ocean Grove Structure Plan Map
- Leopold Structure Plan Map
- Barwon Heads Structure Plan Map
- Drysdale/Clifton Springs Structure Plan Map
- Jetty Road Urban Growth Plan Map
- Point Lonsdale Structure Plan Map

**Objectives**

To ensure development responds to the identity and preferred character of the individual township in which it is located.

To provide attractive and sustainable industrial, commercial, retail, agricultural and tourism development in designated locations, to service the wider Bellarine community.

**General strategies**

Support the district towns of Ocean Grove, Drysdale/Clifton Springs and Leopold to fulfil their role as service hubs for the Bellarine Peninsula.

Ensure all other townships provide retail, commercial and community uses and facilities that serve the daily needs of the community.

Retain the land surrounding each town as rural breaks and for its agricultural and scenic values.

**St Leonards strategies**

Support a mix of retail, commercial, community and entertainment uses within the town centre.

Encourage development that respects the coastal landscape setting of St Leonards by:

- Providing reasonable sharing of views of the coast and foreshore.
- Promoting contemporary design that reflects the existing scale, setbacks, spacing, forms and materials of the buildings in the locality.
- Ensuring that development allows for the protection of significant vegetation or planting around buildings and has minimal impact on roadside vegetation.

Support the on-going management, enhancement and sensitive development of the foreshore, Salt Lagoon Wildlife Reserve and Edwards Point State Faunal Reserve.

Protect the scenic qualities of Murradoc Hill and Swan Bay.

Support the development of a focal building, comprising retail and tourist related activities, at the south west corner of Murradoc and Bluff Roads.

Support the development of Growth Areas 1 and 2 identified on the St Leonards Structure Plan map.

Facilitate development of a community facility including an Early Years Learning Centre, preferably at 1345 Murradoc Road.

Integrate the town centre and pier-foreshore area.

### **Portarlington strategies**

Support the on-going management, enhancement and sensitive development of the foreshore, including a place making project for the main activity node, Point Richards Flora and Fauna Reserve.

Encourage development that respects the coastal landscape setting of Portarlington by:

- Providing reasonable sharing of views of the coast and foreshore, where a view has been identified in an overlay.
- Promoting contemporary design that reflects the existing scale, setbacks, spacing, forms and materials of the buildings in the locality.
- Ensuring that development allows for the protection of significant vegetation or planting around buildings and has minimal impact on roadside vegetation.

Support the establishment of a Portarlington Community Hub.

Support the redevelopment of the Country Fire Authority fire station.

Support the integration of the Portarlington town centre and pier-foreshore area.

Support the development of the development opportunity sites including:

- Development of a focal building at 22-34 Newcombe Street, Portarlington.
- Redevelopment of the rear of 40-42 Newcombe Street in a manner that is sympathetic to and maintains the heritage context of the site.
- Redevelopment of land between Newcombe Street, Fenwick Street, Harding Street and Brown Street to improve pedestrian connectivity by creating a two-way rear lane system and additional shop front activation.

Support development in the Portarlington town centre incorporating accommodation uses above ground level retail floor space, where such development meets all parking and access requirements.

Support the use and development of 33-41 Mercer Street, Portarlington for aged care living.

### **Indented Head strategies**

Encourage development that respects the coastal landscape setting of Indented Head, by:

- Providing reasonable sharing of views of the coast and foreshore, where a view has been identified in an overlay.
- Promoting contemporary design that reflects the existing scale, setbacks, spacing, forms and materials of the buildings in the locality.
- Ensuring that development allows for the protection of significant vegetation or planting around buildings and has minimal impact on roadside vegetation.

### **Ocean Grove strategies**

Contain urban development within the defined settlement boundary identified on the Ocean Grove Structure Plan map.

Encourage development that respects the coastal landscape setting of Ocean Grove, by:

- Providing reasonable sharing of views of the coast and foreshore.
- Promoting contemporary design that reflects the existing scale, setbacks, spacing, forms and materials of the buildings in the locality.
- Ensuring that development allows for the protection of significant trees or planting around buildings and has minimal impact on roadside vegetation.

Support the continued development of the north-east growth area as shown on the Structure Plan map.

Encourage a range of accommodation and housing options, including aged care within and adjacent to the Town Centre and other activity centres.

Ensure development avoids detrimental impacts on environmental assets including the coast, Buckley Park Foreshore Reserve, Goandra Estate, Ocean Grove Nature Reserve, Begola Wetlands, Barwon River/Lake Conneware and the Lake Victoria Wetlands.

Support the Town Centre as the primary retail centre for Ocean Grove by providing for a range of retail, business and accommodation uses.

Ensure that the supply of car parking in the Town Centre increases commensurate with parking demands from development.

Preserve The Terrace as the potential long-term arterial route through the Town Centre.

Support the development of a new Neighbourhood Activity Centre, including community uses, in the north-east growth area adjacent to Grubb Road.

Support the development of the restricted retail and industrial precincts within the north-east growth area.

Support the development of a strategic footpath network for the town that provides permeable and safe routes to key destinations and services.

Encourage developments to assist in the establishment of a safe bicycle-pedestrian path network around the town connecting the foreshore, river, nature reserve, and Grubb, Banks and Bonnyvale Roads as shown on the Structure Plan map.

Encourage development to contribute to the improvement of open spaces and key pedestrian links.

Encourage a range of appropriately scaled and located tourism accommodation and activities, including opportunities for revitalisation of existing uses.

Support further development of existing caravan parks and accommodation uses in residential areas, particularly those close to the beach and river, to provide a broader range of accommodation type and mix.

Encourage a range of appropriately scaled tourism related activities on the rural periphery of the town that are complementary to the environmental and rural setting.

Support the duplication of Grubb Road in a manner that preserves significant roadside vegetation, provides an attractive town entry, safe crossing points, pedestrian/cycle paths and undergrounding of powerlines.

Support the provision of community and social infrastructure commensurate with population growth.

### **Leopold strategies**

Support Leopold as a Sub-Regional Retail Activity Centre for the Bellarine Peninsula, that also provides local community, recreational and employment facilities to Leopold's residents.

Retain Leopold as an 'urban island', supporting urban growth contained within the settlement boundary to preserve the surrounding rural hinterland.

Support the development of the Ash Road Growth Area and other areas identified for residential development on the Leopold Structure Plan map.

Support increased housing densities around the Sub-Regional Retail Activity Centre and the neighbourhood shopping centres at Ash Road and Dorothy Street.

Encourage the northerly expansion of the Sub-Regional Retail Activity Centre, ensuring any development integrates with the existing centre and surrounding community facilities, and enhances its appearance and functionality.

Support the local convenience role of the Ash Road and Dorothy Street neighbourhood shopping centres, whilst restricting any future expansion of these centres.

Support the development of Council's Kensington Road Community Hub to provide a wide range of community, health, education and civic services/facilities.

Encourage the creation of an additional local mixed-use centre on the south east corner of Bellarine Highway and Melaluka Road. Any redevelopment of this site could accommodate restaurants, convenience shops, offices and residential development.

Provide public open space in residential areas to cater for the passive and active recreation needs of the community.

Facilitate an improved transport network that includes better traffic movements, pedestrian and cyclist linkages and public transport options.

Protect environmentally sensitive areas including Lake Connewarre and Reedy Lake from detrimental impacts of development.

### **Barwon Heads strategies**

Maintain a compact urban form and avoid outward sprawl by ensuring that urban development does not occur outside of the defined settlement boundary shown on the Barwon Heads Structure Plan map.

Protect the unique character of Barwon Heads as a coastal village located within a sensitive environment and significant landscape setting.

Protect the very low density residential character of the Warrenbeen Court area to ensure that development has minimal impact on the indigenous vegetation and landscape character of the area.

Reinforce the Hitchcock Avenue shopping centre as the focus of retail activity in Barwon Heads.

Restrict new retail development within the existing town centre and discourage the use of land for industry and warehouse uses.

Support the development of Stage 3 of the 13th Beach Resort as a focus for golf that excludes residential development and provides net environmental benefits.

Facilitate the upgrading of the Barwon Heads Village Park and foreshore reserves in accordance with established master plans.

Protect existing street trees and where possible informal landscaping in streets.

Support development of tourist accommodation around the Barwon Heads town centre, including improvement of accommodation diversity.

### **Drysdale/Clifton Springs strategies**

Contain urban development within the defined settlement boundary identified on the Drysdale/Clifton Springs Structure Plan map.

Support the development of the Jetty Road Urban Growth Area and other areas identified for residential development on the Structure Plan map.

Reinforce the Drysdale town centre as the primary retail centre including the development of an additional supermarket on the south side of Murradoc Road.

Provide for the expansion of the Drysdale town centre to the east along Murradoc Road.

Locate future service business or industrial development in the identified precinct along Murradoc Road extending to the Drysdale Bypass.

Design development opposite or close to the Drysdale Bypass to minimise back fencing as viewed from the Bypass.

Co-locate and integrate future education, community and recreation facilities to enhance their accessibility and to maximise joint use wherever possible.

Develop the Council owned Palmerston Street site and the Drysdale Regional Community and Cultural Hub for community and recreation purposes.

Locate development of short-term tourist accommodation at the Curlewis Golf Course at the eastern end of the course close to the Jetty Road Urban Growth Area.

Facilitate additional bicycle and pedestrian opportunities throughout the townships including new footpaths and bicycle lanes.

Support the creation of consolidated parking areas in the town centre.

### **Point Lonsdale strategies**

Support low-scale tourism opportunities on designated land identified on the Point Lonsdale Structure Plan map.

Support development that strengthens the township's coastal village character and landscape setting by requiring an architectural and urban design response that includes:

- Low scale forms.
- Articulation (materials, openings or features such as decks, balconies, wide eaves or canopies).
- Contemporary design.
- Use of materials such as timber, corrugated iron, weatherboard or light coloured render.
- Limited site coverage and provision of front and side setbacks that reflect existing neighbourhood patterns and provide opportunities for maintenance and planting of native coastal and indigenous vegetation.

Ensure that urban street works associated with development contribute to the implementation of the identified primary and secondary pedestrian/cycle routes and are designed to complement the informal coastal streetscape character.

### **Policy documents**

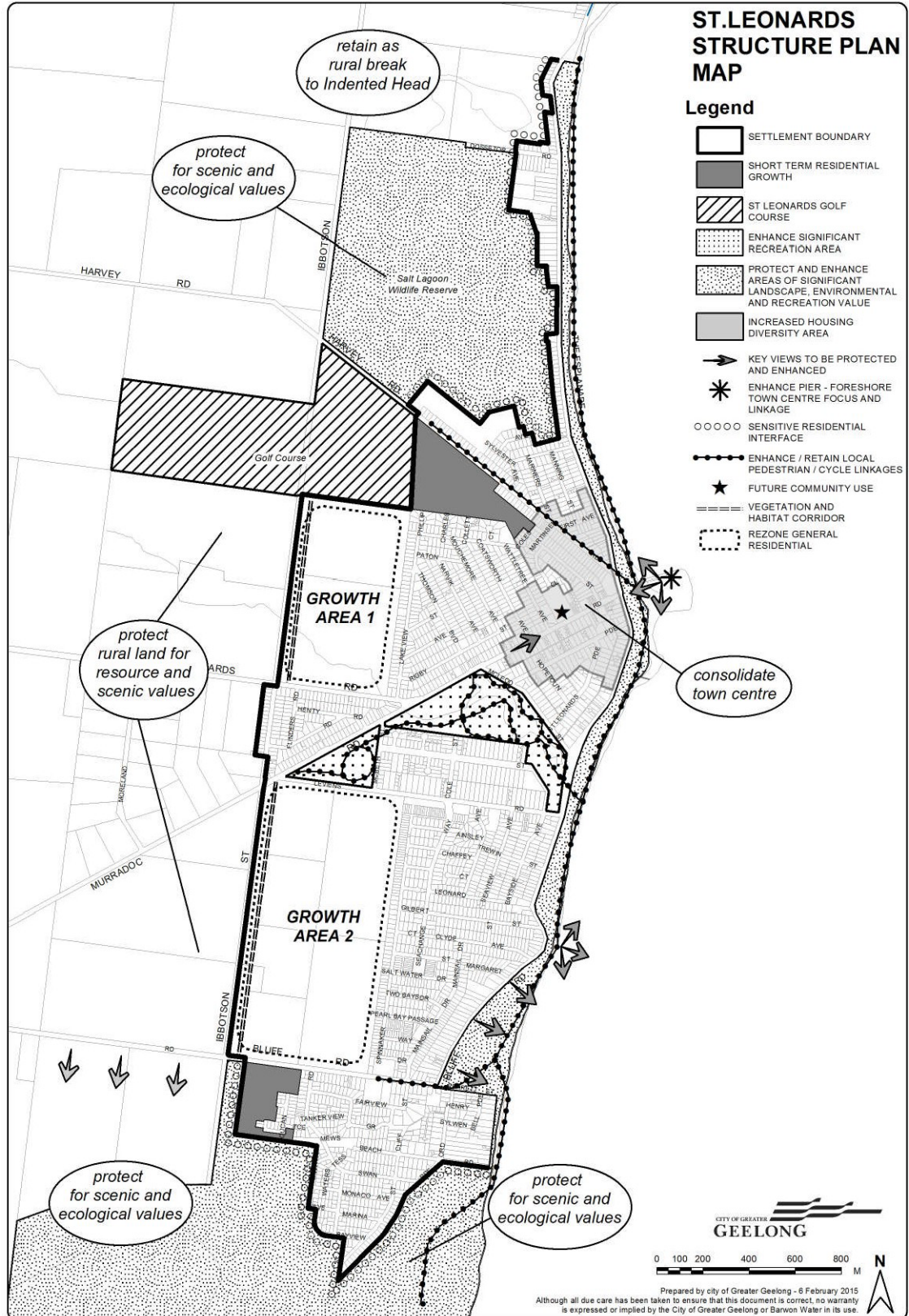
Consider as relevant:

- *Bellarine Peninsula Strategic Plan* (City of Greater Geelong, 2006)
- *Ocean Grove Structure Plan* (City of Greater Geelong, 2015 amended 2016)
- *Ocean Grove Urban Design Framework* (City of Greater Geelong, 2014)
- *Portarlington Structure Plan* (City of Greater Geelong, 2016 amended 2017)
- *Indented Head Structure Plan* (City of Greater Geelong, 2016)
- *St Leonards Structure Plan* (City of Greater Geelong, 2015)
- *Leopold Structure Plan* (City of Greater Geelong, 2011 amended 2013)
- *Leopold Urban Design Framework* (City of Greater Geelong, 2011)
- *Barwon Heads Structure Plan* (City of Greater Geelong, 2010)
- *Drysdale Clifton Springs Structure Plan* (City of Greater Geelong, 2010)
- *Jetty Road Urban Growth Plan* (City of Greater Geelong, 2007 amended 2008)
- *Coastal Spaces Landscape Assessment Study- Municipal reference document* (Planisphere, 2006)
- *Point Lonsdale Structure Plan* (Planisphere, 2009 amended 2011).
- *Drysdale Urban Design Framework* (City of Greater Geelong, 2012)
- *Drysdale Bypass Access Management Strategy* (VicRoads, 2017)
- *City of Greater Geelong Settlement Strategy* (City of Greater Geelong, August 2020)

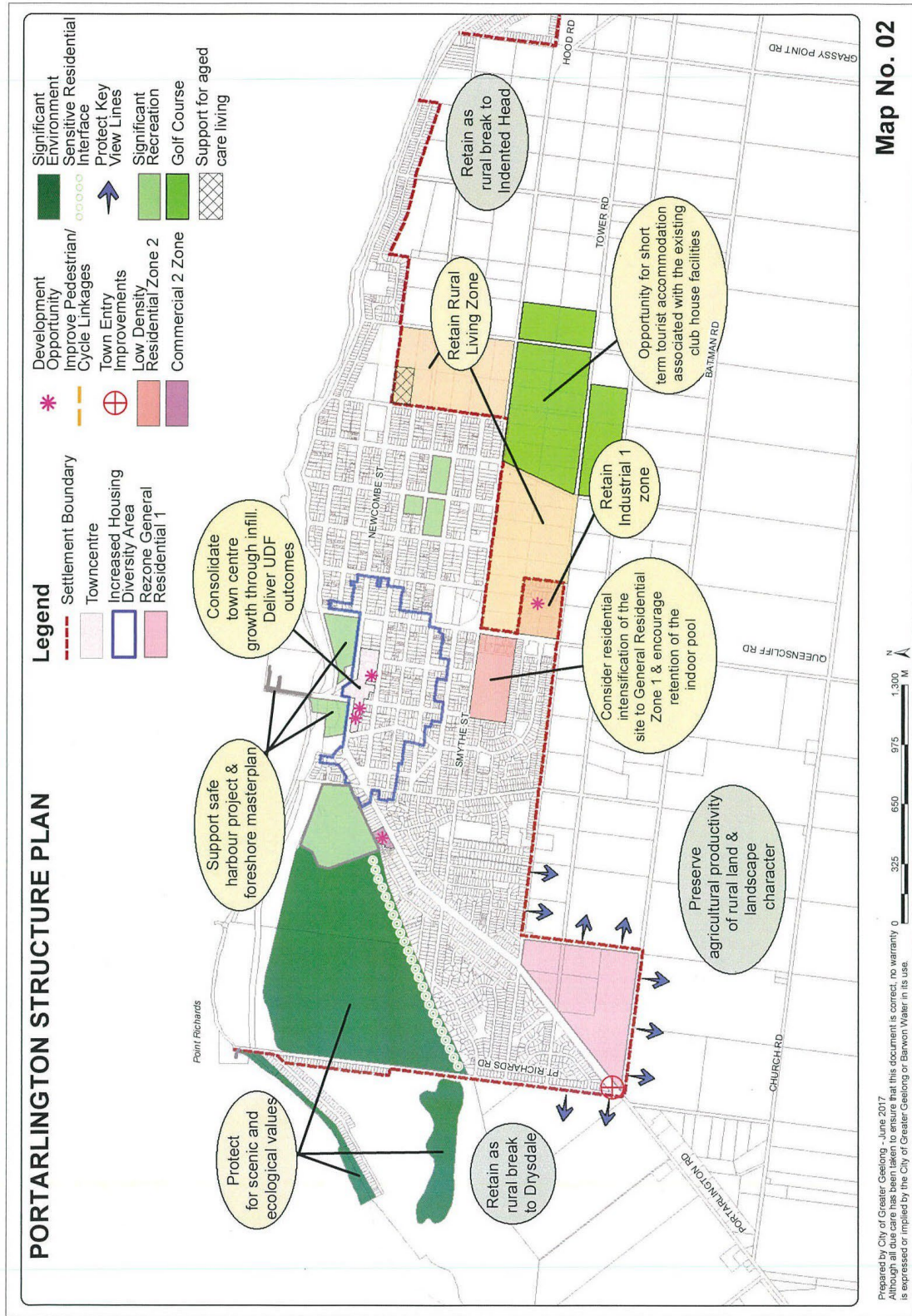
**Expiry**

This local policy will expire three years from the gazettal of Amendment C417ggee.

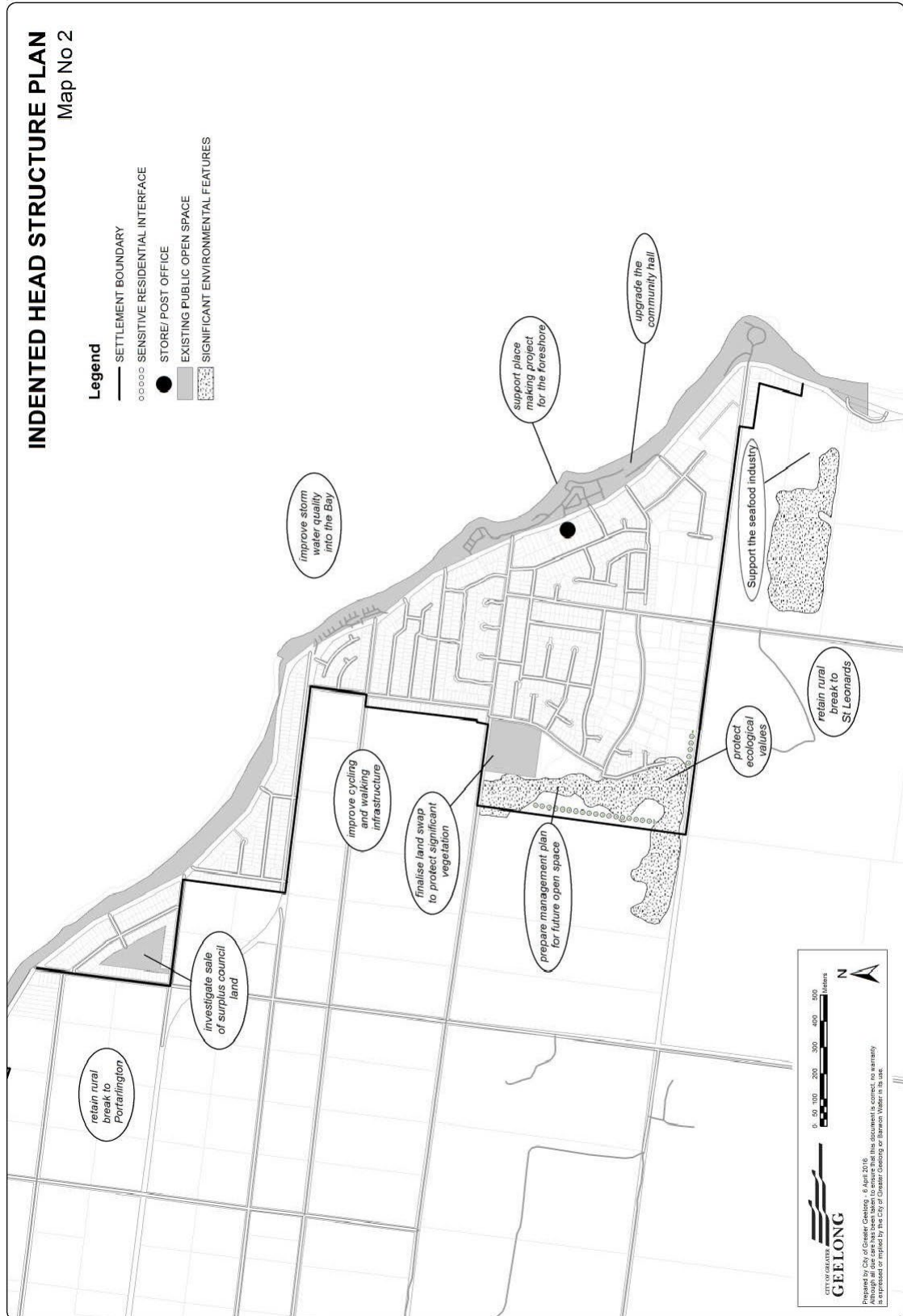
**St Leonards Structure Plan Map**



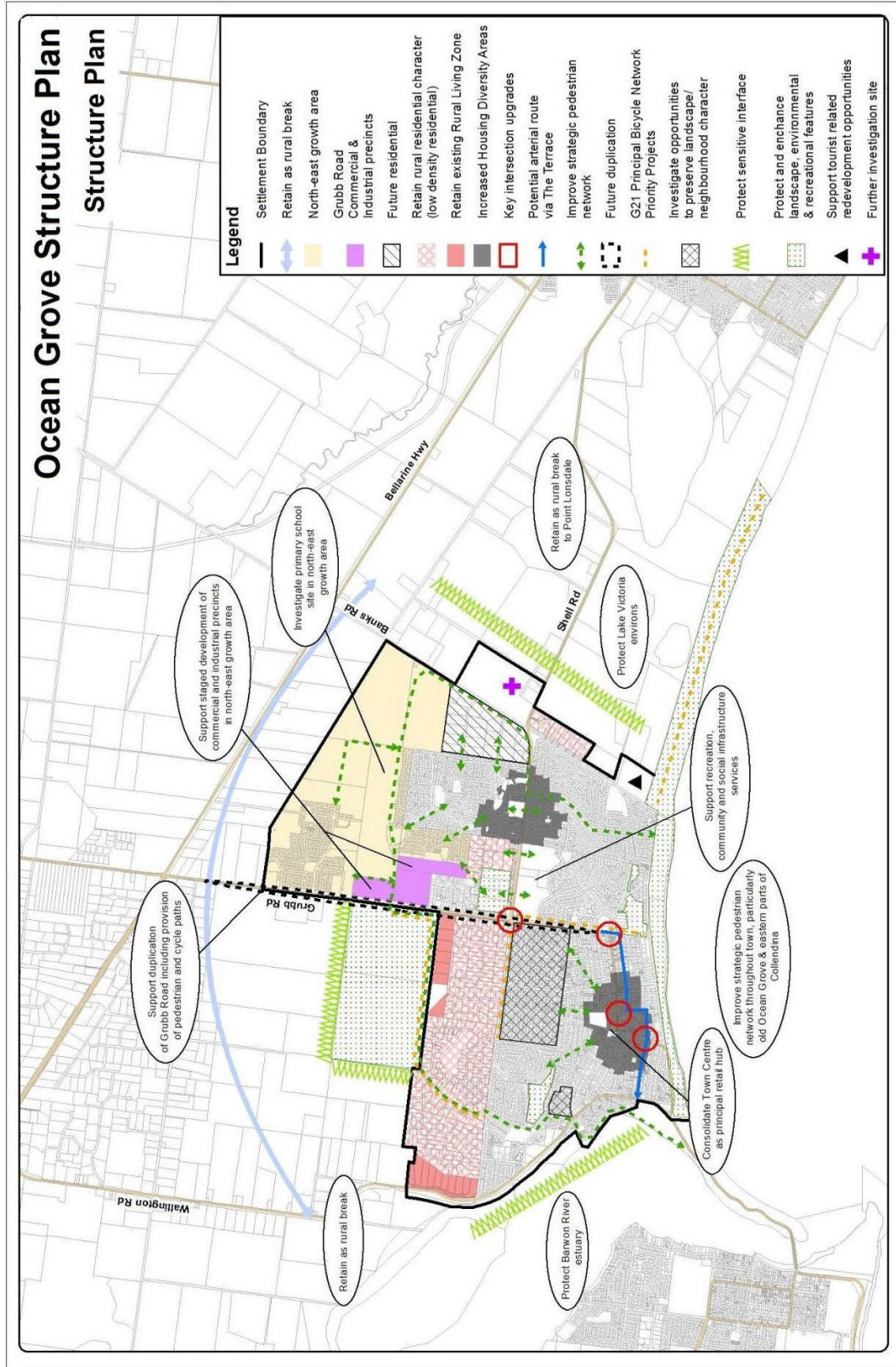
Portarlington Structure Plan Map



Indented Head Structure Plan Map



Ocean Grove Structure Plan Map



Map No.4

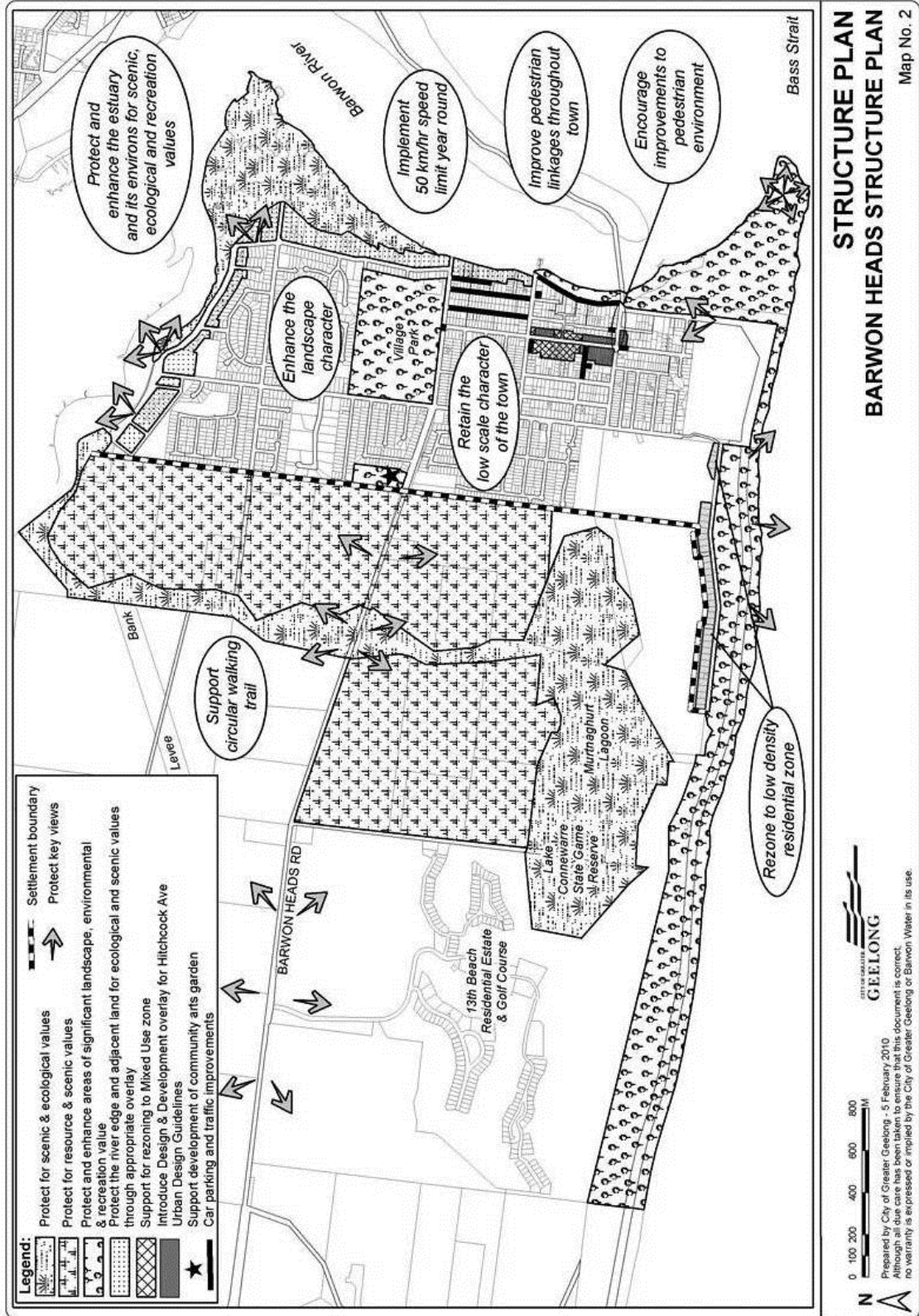


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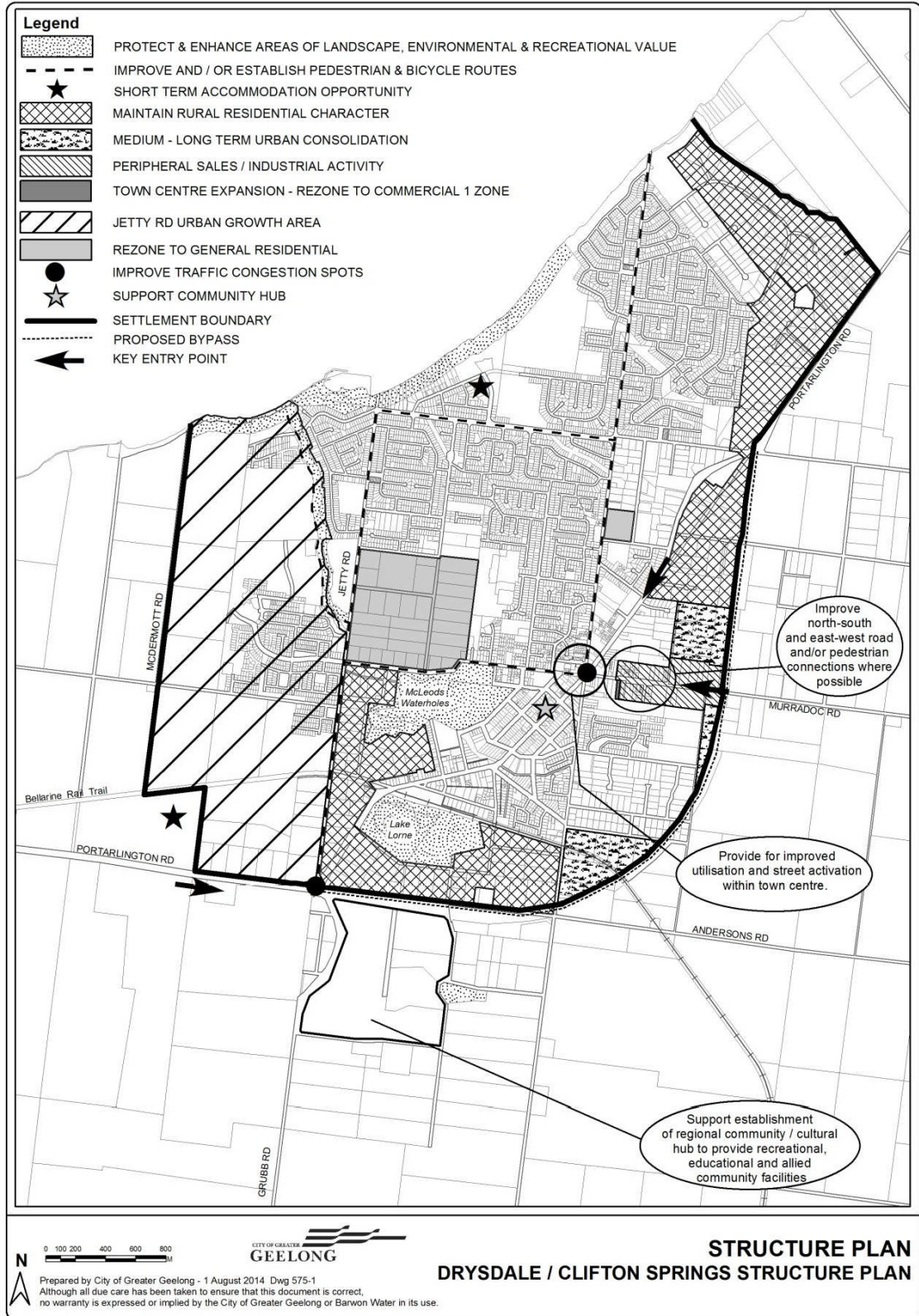
Leopold Structure Plan Map



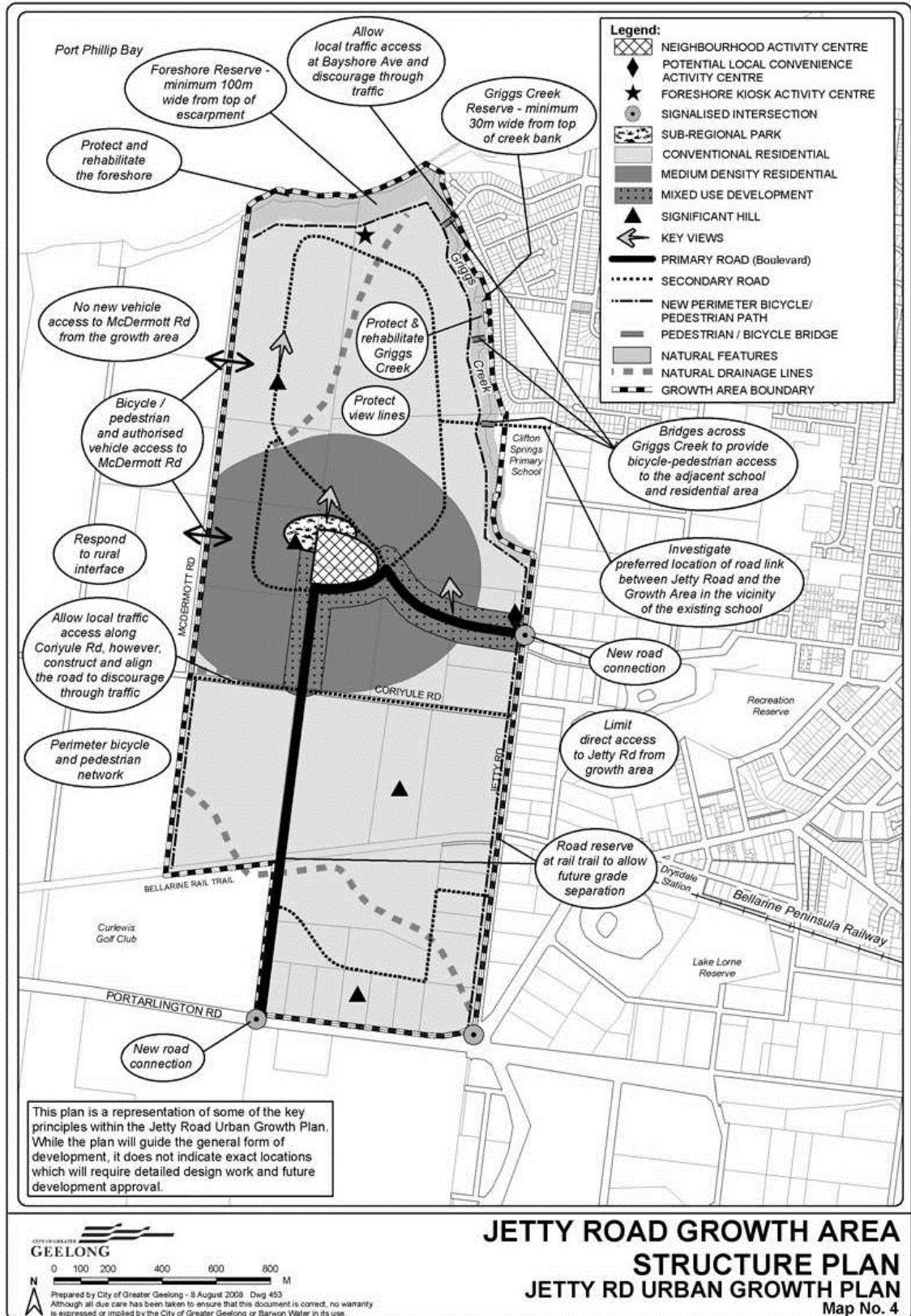
Barwon Heads Structure Plan Map



Drysdale/Clifton Springs Structure Plan Map



Jetty Road Urban Growth Plan Map



Point Lonsdale Structure Plan Map



Township Structure Plan

Legend					
	Existing public open space and golf course		Minimal change residential areas		Encourage revegetation to coastal dune edge
	Coastal dunes and vegetation		Residential growth areas		Significant entry points
	Community infrastructure		Potential residential growth area subject to an amendment and EES		Local shop
	Queenscliffe Coastal Management Plan Area		Rezone for tourist accommodation / activity		Neighbourhood activity centre
	Buckley Park Management Plan Area		Land with environmental constraints		Proposed vegetation protection
	Railway line, possible new station and interchange/parking area		Primary pedestrian & cycle route		Opportunities for appropriately low scaled and designed tourism accommodation
	Point Lonsdale Settlement Boundary		Potential expansion of Golf Course subject to an amendment and EES		

**11.03-6L-02 Corio Norlane**07/07/2022  
C417ggee**Policy application**

This policy applies to land identified in the Corio Norlane Framework Plan Map.

**Objective**

To facilitate the urban renewal of the Corio Norlane area.

**Strategies**

Encourage increased housing diversity areas around the Corio Sub-Regional Activity Centre, Bell Post Shopping Centre, North Shore Station and local shopping centres.

Encourage the redevelopment of school sites subject to closure for residential development (where not required for a community use in need of a larger site).

Support the ongoing improvement and expansion of the Corio Sub-Regional Activity Centre, and the development of retail, offices, cafes/restaurants and health services around the centre.

Improve the role, function and presentation of retail activity centres through measures such as supporting a diverse mix of uses and the use of public art to enhance their presentation and create a sense of place.

Encourage the development of the Waterworld Precinct as a multi-purpose community precinct that includes dedicated arts and culture spaces within a “landmark” building.

Facilitate the redevelopment and rezoning of land no longer required for commercial use in local shopping centres for housing and other compatible uses.

Limit intensification of residential development within the North Shore residential area close to heavy industrial land uses.

Support the development of health and support services, including potential expansion of facilities within the Corio ‘heart’ area.

Improve the amenity and safety of local reserves and parks and consider for trade poorly configured parks that do not contribute to the permeability of neighbourhoods.

Encourage the upgrade of ageing drainage infrastructure to better manage impacts resulting from stormwater flooding.

Facilitate the provision of a safe and attractive active transport network to access destinations in Corio and Norlane.

**Policy document**

Consider as relevant:

- *Corio Norlane Structure Plan* (City of Greater Geelong and Department of Planning and Community Development, 2012)

**Expiry**

This clause expires three years from the gazettal of Amendment C417ggee.

Corio Norlane Framework Plan Map



LEGEND

Existing Features

- Study area boundary
- Major road network
- Railway line/ station
- Industrial zoned land
- Major employment sites
- Public open space/ recreation
- Local passive open space reserves
- Shell Refinery Land Use Buffer
- Schools
- Medical Facilities

Proposed Initiatives

- Retain redundant school site for education/community uses
- Redevelop redundant school sites for residential/community uses
- Upgrade local shops
- Develop mixed use precinct around Corio Activity Centre
- Implement masterplans for active open space
- Include as Increased Housing Diversity Area
- Boulevard treatment of Bacchus Marsh Road
- Preferred primary heavy vehicle freight route
- Preferred secondary heavy vehicle freight route
- Priority road infrastructure upgrades

- Develop community precincts/hubs
- Improve Corio Activity Centre as focus for community activity
- Develop landmark arts and culture building on Waterworld site
- Enhance physical and habitat links to Corio Bay
- Manage sensitive residential interface
- Improve lighting, amenity and surveillance at railway stations

City of Greater Geelong Final Version - July 2012

0 500m 1km

**FRAMEWORK PLAN**  
CORIO NORLANE STRUCTURE PLAN

**11.03-6L-03 Moolap-Point Henry**07/07/2022  
C417ggee**Policy application**

This policy applies to land within the Moolap Coastal Strategic Framework Plan Map.

**Objectives**

To facilitate the transformation of Moolap-Point Henry into a safe, sustainable, integrated, connected, accessible and attractive place to live, work and visit.

To encourage development that integrates with and provides an interface to adjoining precincts and surrounding areas, including the settlement break between eastern Geelong and the Bellarine Peninsula.

To encourage land use and development that maintains or enhances environmental values and responds to environmental risks.

**Strategies**

Support residential and tourism use and development in the Point Henry Precinct, including medium-high density residential development, tourist accommodation and facilities, and complementary commercial and community facilities, water access and boating infrastructure.

Designate the Wetlands and Former Saltworks Precinct for conservation purposes and public open space, with complementary infrastructure and tourism, research and culture/heritage facilities.

Support residential use and development at a range of densities in the Moolap East Precinct, with complementary community, retail and commercial facilities, open space and transport links following the relocation of Dow Chemical and site remediation.

Support industrial activities and commercial businesses in the Southern Precinct to provide local services and employment.

Site and design buildings, infrastructure and coastal facilities to minimise impacts on the environmental, landscape, cultural heritage and biodiversity values of the wetlands, coastal environs and Moolapio grasslands.

Site and design land use and development to respond to potential risks from environmental hazards and climate change, including flooding, acid sulfate soils, sea level rise, coastal erosion and retreat, ground contamination and groundwater salinity.

Provide landscaping and interface treatments to Portarlington Road and Clifton Avenue that are sympathetic to the rural landscapes and green break between Moolap and Leopold.

Provide a movement network that provides for convenient and safe access and is integrated with the existing network and surrounding areas.

Plan for drainage and stormwater management to take a whole of catchment approach that maintains water quality and includes water sensitive urban design.

Encourage environmentally sustainable and water sensitive design that responds to the land's context, natural features and constraints.

Include landscaping and interface treatments that avoid adverse impacts between industry and existing and future residential uses and conservation areas.

Encourage urban and building design that responds to the coastal setting and establishes a unique character.

Facilitate the necessary clean-up, rehabilitation and remediation of former industrial uses and landfill sites.

Support interim land use and development, consistent with the zoning of the land, where the ultimate land use and development outcomes for the land are not prejudiced.

Support the continuation of Dow Chemical in the Moolap East Precinct until this industry chooses to relocate and the precinct transitions to residential.

**Policy guidelines**

Consider as relevant:

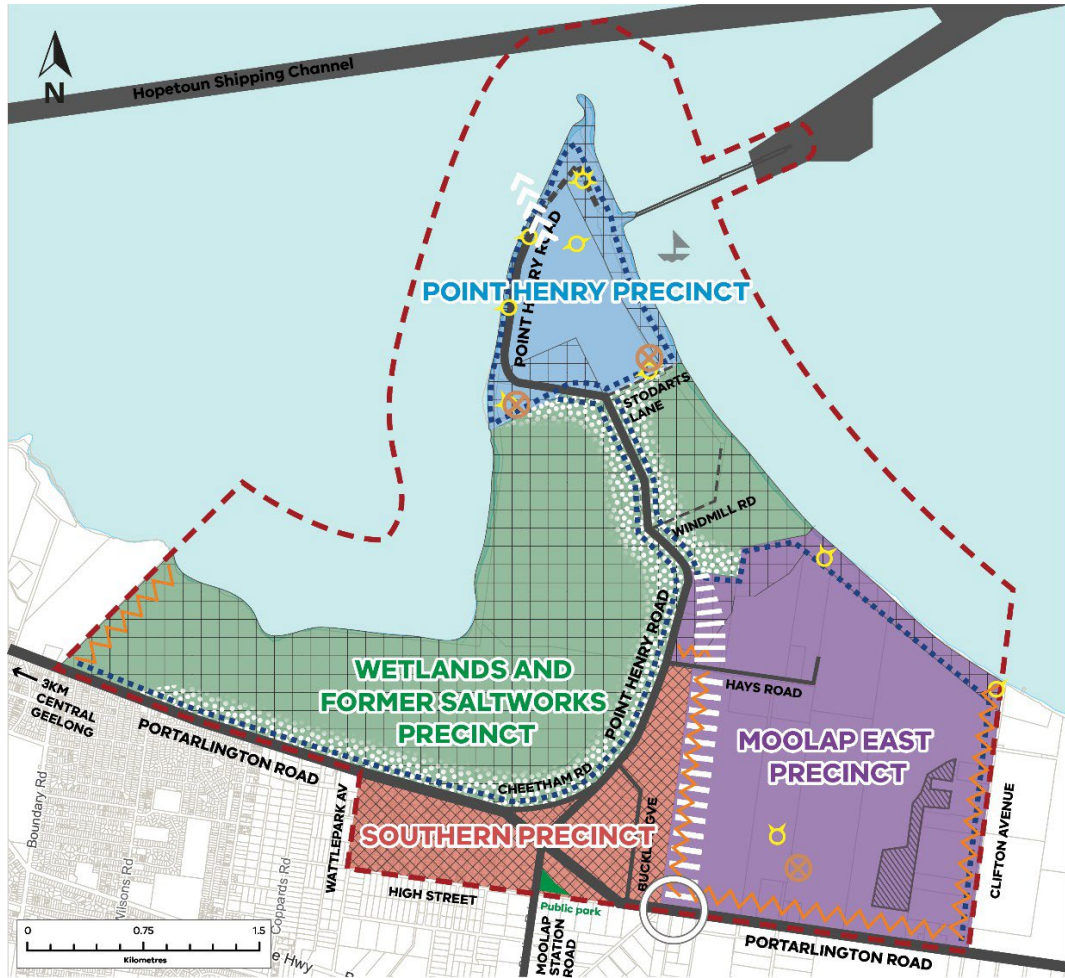
- Whether interim land use and development proposals would prejudice the ultimate transition to future land use, having regard to:
  - The expected timing of the transition to future land use.
  - The potential adverse impacts on the environment including contamination.
  - Potential land use conflicts, adverse amenity impacts or other off-site impacts.
  - The need for provision and ongoing maintenance of new infrastructure.
  - The extent, lifecycle and implications of significant investment in infrastructure, services or buildings.
  - Whether any conditions are required to limit the extent, nature and expiry of proposals that may prejudice the future land use directions.
- Whether the use or development is consistent with the provisions of the *Mines (Aluminium Agreement) Act 1961*, where applicable.

**Policy document**

Consider as relevant:

- *Moolap Coastal Strategic Framework Plan* (Department of Environment, Land, Water and Planning, 2019)

Moolap Coastal Strategic Framework Plan Map



**11.03-6L-04 West Fyans - Fyans Street**

07/07/2023  
C458ggee

**Policy application**

This policy applies to the land identified in the West Fyans – Fyans Street Precinct Structure Plan Area Map.

**General strategy**

Ensure that development enhances the visual relationship with public open space areas, including the Barwon River.

**Open space precinct 1 strategies**

Support community and recreation use and development.

Support tourist-related use and development in identified locations as shown on the West Fyans – Fyans Street Precinct Structure Plan Area Map.

Design and site development to enhance the open space qualities of the area including the Barwon River Corridor.

**Business park precinct 2 strategy**

Encourage uses that contribute to the development of the business park including manufacturing, office and active recreation uses.

**Residential interface precinct 3A strategies**

Direct commercial uses to West Fyans Street including office and retail development that could include housing at upper levels.

In the Residential and Mixed Use Zones support the development of housing, including medium density housing, in a manner which responds to the existing housing stock’s modest scale, built form and setbacks.

Within Industrial 1 zoned areas support a range of activities complementary to the long-term mixed-use vision for the area including office, service industry and leisure and recreation activities.

Discourage industrial uses that do not meet Clause 53.10 buffer requirements.

**Mixed use precinct 3B strategies**

Within the Commercial Node support a mix of accommodation and commercial activity including retail, food and drink premises, office, community and recreation uses.

In the West Fyans Street area, west of Pakington Street encourage a mix of office and retail uses and opportunities for upper level housing.

In the West Fyans Street area, east of Pakington Street encourage restricted retail and other commercial uses.

Encourage medium to high density residential development in the remainder of the precinct with opportunities for small-scale non-residential uses such as arts and crafts, leisure and recreation, home-based offices and medical offices.

Design development to provide for or improve pedestrian and bicycle linkages to the Rutland Street extension connecting to Pakington Street.

**Mixed use precinct 4 strategies**

Within the Commercial Node support a mix of accommodation and commercial activity including retail, food and drink premises, office, community and recreation uses.

Limit non-residential uses outside the Industrial 1 Zone and the Commercial Node to areas with convenient access to and from the river environment.

Encourage medium to high density residential development in the remainder of the precinct.

Within Industrial 1 zoned areas support a range of activities complementary to the long-term mixed-use vision for the area including office, service industry, leisure and recreation activities.

Discourage industrial uses that do not meet Clause 53.10 buffer requirements.

Promote the adaptive reuse of heritage buildings including the former heritage woollen mills.

Design development to provide for or improve pedestrian and bicycle linkages to the river environment and the Rutland Street extension connecting to Pakington Street.

**Business precinct 5 strategy**

Maintain this precinct as the focus for restricted retail premises.

**Industrial precinct 6 strategies**

Discourage the use of land for offensive industries and restricted retail activity.

Encourage the use of land for warehousing, service and non-offensive industries and office uses.

**Mixed use - high density residential precinct 7 strategies**

Direct restricted retail uses to Moorabool Street and larger commercial uses to Moorabool and Fyans Streets at ground level and encourage housing at upper levels.

Encourage medium to high density residential uses south of Little Fyans Street with higher density housing adjacent to Barwon Terrace.

Support a range of office, warehousing, non-offensive industrial and service industry uses north of Little Fyans Street.

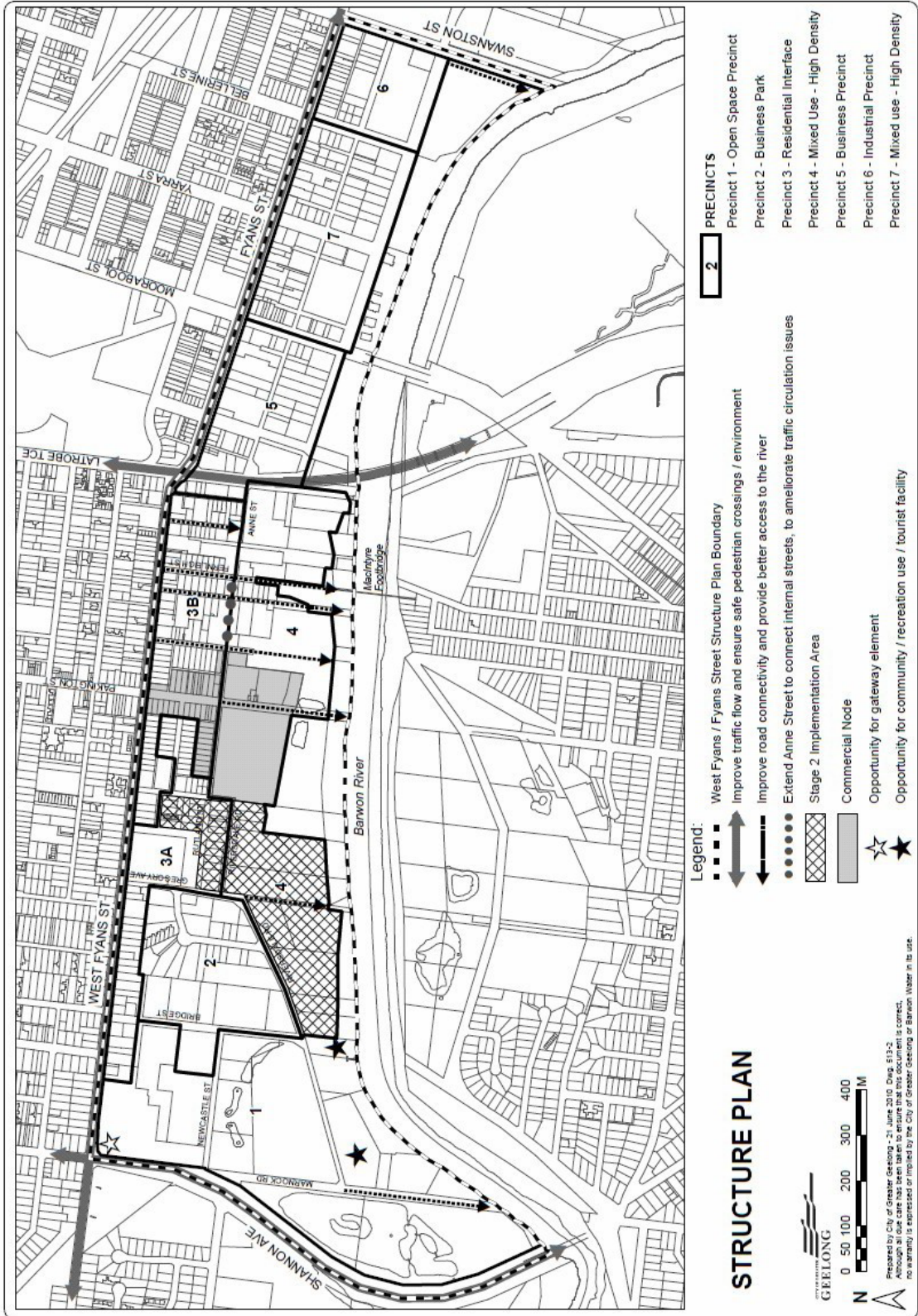
Support commercial uses including food and drink premises, offices, arts and crafts and leisure and recreation activities at ground and first floor levels adjacent to Barwon Terrace and Yarra Street.

**Policy document**

Consider as relevant:

- *West Fyans –Fyans Street Precinct Structure Plan* (City of Greater Geelong, 2009)

West Fyans – Fyans Street Precinct Structure Plan Area Map



11.03-6L-05 South Geelong

Proposed C432ggee

Policy application

This policy applies to the land identified in South Geelong Urban Design Framework Area and Key Development Areas Map.

Objectives

- To promote diverse, high-quality, sustainable urban environments whilst respecting identified neighbourhood character and heritage values.
- To create green streets and public spaces with safe and accessible environments for pedestrians and cyclists and water sensitive urban design.
- To minimise the visual impact of car parking.

### Strategies

- Improve circulation and way finding features for cyclists and pedestrians.
- Ensure all development, streets and car parking incorporate significant canopy tree planting, landscaping and water-sensitive urban design to increase tree numbers in public and private realms.
- Support the reuse/re purpose of heritage buildings, particularly corner buildings within the General Residential Zone for commercial uses.

### Key Development Areas – Moorabool Street and Station Hub

#### Objective

To encourage medium and higher density development with a diverse mix of housing (including affordable housing) and ground level retail, commercial and community uses that service the needs of the local community and visitors.

#### Moorabool Street Strategies

##### Public Domain

Ensure entrances and paths to public spaces are designed to cater for all people including those with mobility difficulties.

##### Street Network and Access

~~Discourage private vehicle use by designing~~Design streets for slow circulation speeds, ~~giving priority to other modes at intersections~~ and ~~constraining provisions~~support the reduction of private vehicle parking.

Support the development of safe pedestrian and cyclists' crossings on Moorabool Street, particularly at Lonsdale and Foster Street intersections.

Support the widening of Lamb Place to allow for rear loading vehicle access to development sites.

##### Land Use

Support residential uses and limit the number of buildings used solely for commercial purposes, except for properties adjacent to the railway line.

Support active uses adjoining street corners, public plazas and around heritage buildings.

##### Building Form and Layout

Support consolidation of lots to maximise development potential, ensuring equitable development opportunities on adjacent lots.

Ensure a transition of building height and an appropriate interface to residential properties to the east of Moorabool Street.

Support a reduction in, or shared car parking space for ~~residential~~ development.

#### Station Hub Strategies

##### General

Support the redevelopment and transition of the South Geelong Station Hub into a transit orientated community hub.

Encourage shared car parking through the provision of a multi deck car park or an alternative car parking arrangement.

### Land Use

Support a mix of commercial and community land uses including retail, small offices, childcare, supermarket, food and drink premises on the ground floor near the station, which service the local area while continuing to meet the needs of station users.

Support high density residential development.

Support the development of a public plaza adjacent to the station.

### Built Form and Layout

Support centre median car parking along Lonsdale and Bellerine Streets adjacent to the Station Hub.

Support a wider footpath and verge opposite 40-44 Lonsdale Street to increase public realm, street planting and to help reduce visual bulk from development.

Minimise the impact from overshadowing on residential properties from the multi deck car park.

Support building heights of 3 to 5 storeys on the south side of the South Geelong Station.

Encourage the development of a main street through the southern side of the station.

### Movement networks

~~Provide directional signage to clearly mark the start and finish of the Bellerine Rail Trail.~~

~~Support an upgrade to the Bellerine/Lonsdale Street intersection to improve pedestrian and vehicle safety.~~

Support improved pedestrian and bicycle connections through and to the station hub.

Support the development of a pedestrian bridge over the railway line, connecting Bellerine Street ensuring access at either side of the station hub.

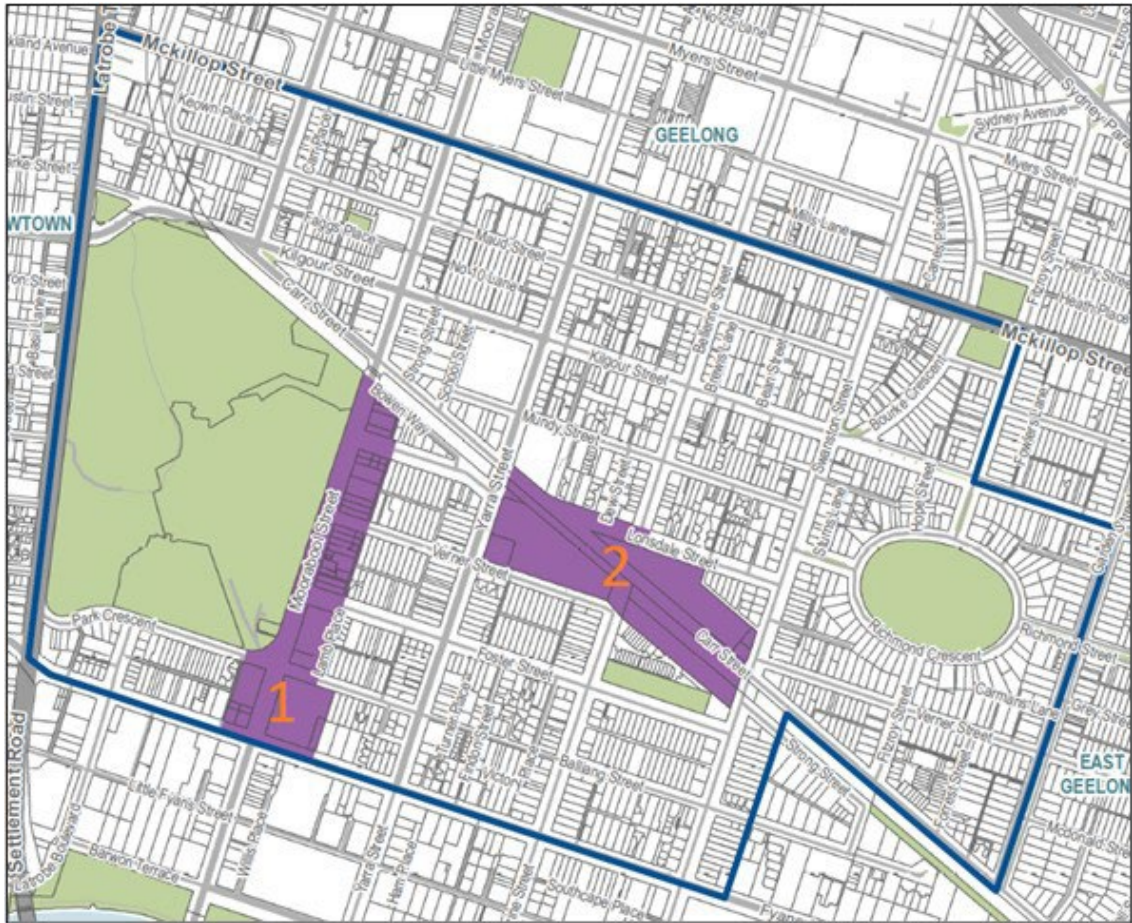
~~Support the provision of a bus stop accommodating additional bus routes improving access to the South Geelong Station Hub and surrounding suburbs.~~

### Policy Document

Consider as relevant:

- *South Geelong Urban Design Framework (City of Greater Geelong, 2022)*

South Geelong Urban Design Framework Area and Key Development Areas Map



Key Development Areas

1. Moorabool Street
2. Station Hub

--/--/---  
Proposed C432ggee

## SCHEDULE 4 TO CLAUSE 32.07 RESIDENTIAL GROWTH ZONE

Shown on the planning scheme map as **RGZ4**.

### SOUTH GEELONG - SUBSTANTIAL CHANGE AREA

#### 1.0 Design objectives

--/--/---  
Proposed C432ggee

To provide diverse housing that responds to the surrounding built form and character.

To encourage lot consolidation.

To minimise loss of canopy trees.

#### 2.0 Requirements of Clause 54 and Clause 55

--/--/---  
Proposed C432ggee

	Standard	Requirement
Minimum street setback	A3 and B6	<del>In accordance with Standard A3 or B6 or 3m, which ever is the lesser</del> Setback a minimum of 6m to Latrobe Terrace and 3m to all other streets.
Site coverage	A5 and B8	Maximum site coverage 70 per cent
Permeability	A6 and B9	None specified
Landscaping	B13	The development should provide 1 canopy tree in the front setback.
Side and rear setbacks	A10 and B17	None specified
Walls on boundaries	A11 and B18	None specified
Private open space	A17	None specified
	B28	None specified
Front fence height	A20 and B32	None specified

#### 3.0

--/--/---  
Proposed C432ggee

#### Maximum building height requirement for a dwelling or residential building

None specified.

#### 4.0 Application requirements

--/--/---  
Proposed C432ggee

None specified.

#### 5.0 Decision guidelines

--/--/---  
Proposed C432ggee

None specified.



## GREATER GEELONG PLANNING SCHEME

CoGG Day 1 Version 28/10/24

### SCHEDULE 50 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO50**

**40-44 LONSDALE STREET, 146 SWANSTON STREET & 51 CARR STREET SOUTH  
GEELONG**

#### 1.0 Design objectives

Proposed C432ggee

To develop an ecologically sustainable, transit orientated precinct that incorporates best practice environmental management.

To support a mix of high-quality high and medium density built form that has an appropriate interface to adjacent streets and creates a landmark building fronting the station plaza north of the railway line.

To create high-quality landscaped and safe public spaces that ensure a visual connection across the railway line at Bellerine Street, celebrates the beginning of the Bellarine Rail Trail and delivers a legible and coherent street structure.

To improve pedestrian connections within and around the subject site and contribute to improved street environments resulting from the closure and redirection of Carr Street.

To protect the internal residential amenity of new buildings by limiting noise transmission and provide acoustic measures to protect from external noise sources such as train and public spaces.

#### 2.0 Buildings and works

Proposed C432ggee

The following buildings and works requirements apply to an application to construct a building or construct or carry out works.

##### General requirements

Development should be generally in accordance with Map 1 to this schedule.

Support the development of a [second](#) pedestrian link over the railway line along the Bellerine/Carr Street alignment generally in accordance with Map 3 to this schedule.

##### Building height

Development should not exceed the preferred maximum building heights as shown in Table 1 and Map 2 to this schedule.

The preferred maximum building height does not include architectural features, masts and building services including plant rooms, air conditioning, lift overruns, structures associated with roof top gardens, decks and communal outdoor spaces and their ancillary facilities or enclosed stairwells provided that the following criteria are met:

- Not more than 50% of the roof area is occupied by equipment (other than solar panel or greening);
- The equipment is located to minimise additional overshadowing and reduce visual impact;
- The equipment does not exceed the [preferred maximum](#) height limit by more than 3.6 metres; and

The equipment and screening is integrated into the design of the building to the satisfaction of the responsibly authority.

~~Development should meet the following minimum floor to floor dimensions:~~

- ~~4 metres at ground level~~
- ~~2.2 – 3.5 metres for residential and non-residential uses in the levels above.~~

##### Street wall heights

Street wall heights should be generally in accordance with the maximum street wall heights specified

**GREATER GEELONG PLANNING SCHEME**

in Table 1  
and Map 2  
to this  
schedule.

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GREATER GEELONG PLANNING SCHEME

Setbacks

Development should be generally in accordance with the preferred setbacks specified in Table 1 and Figures 1 to 5.

Table 1 to Schedule 50 to Clause 43.02

Parcel	Preferred Maximum Building Height	Preferred Maximum Street Wall Height - Carr Street	Preferred Maximum Street Wall Height - Lonsdale Street	Preferred Above Street Wall Setbacks	Preferred Other Setbacks
A	22 metres (6 storeys)	11 metres (3 storeys) to Station Forecourt	22 metres (6 storeys)	4.5 metres to Station Forecourt	n/a
B	<del>18 metres</del> 22 metres (5-6 storeys)	11 metres (3 storeys)	11 metres (3 storeys)	4.5 metres to Lonsdale Street 3 metres to Carr Street, laneway and any new pedestrian link	4.5 metres to eastern boundary at ground level for new shared street.  6 metres at ground level to new pedestrian links.
C	<del>13.5</del> 14 metres (3-4 storeys)	n/a	n/a	n/a	4.5 metres to western boundary at ground level for new shared street.  4 metres at ground level to new pedestrian links.

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Figure 1: Parcel A fronting Lonsdale Street, Bellarine Street, and Future Station Forecourt

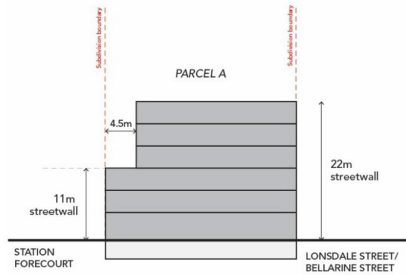
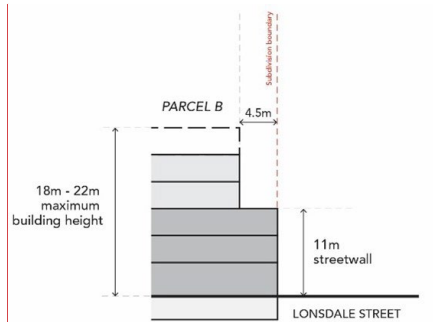
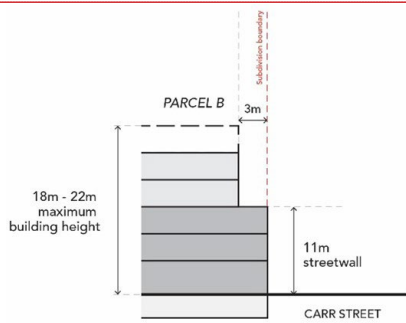


Figure 2: Parcel B fronting Lonsdale Street and Carr Street

GREATER GEELONG PLANNING SCHEME

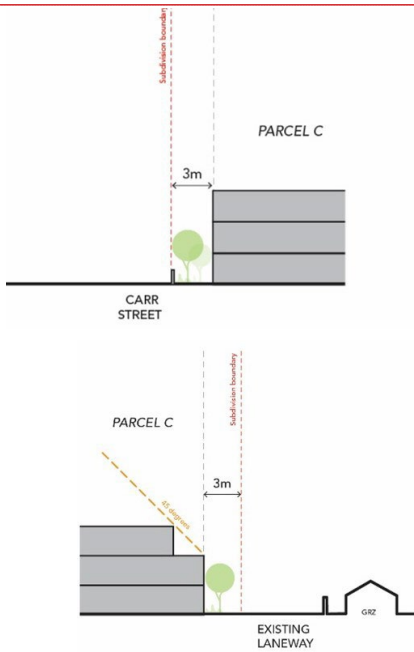


**Commented [MQ1]:** Figure 2 revise to show a 22m building height



**Commented [MQ2]:** Amend to show 22m building height and to read Carr street, laneway and any new ped link.

Figure 3: Setback to existing laneway for Parcel C

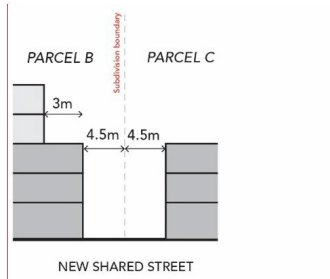


**Commented [MQ3]:** Amend to show 4 storeys

**Commented [MQ4]:** Amend to show 4 storeys and no above street wall setback

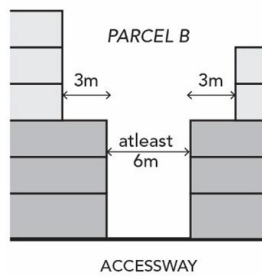
Figure 4: Setback to new shared street

**GREATER GEELONG PLANNING SCHEME**



**Figure 5: Setback to new pedestrian links**

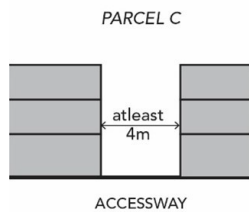
**Commented [MQ5]:** Amend to show Parcel C with 4 storeys



**Commented [MQ6]:** Amend to show both buildings with 6 storeys. And change accessway to pedestrian link

**Commented [MQ7]:** Amend Figure 5 Parcel B and C to read pedestrian link instead of access way.

**Commented [MQ8]:** Amend to show buildings with 4 storeys and change accessway to pedestrian link



**Building Separation**

Development should provide a minimum of 9 metres distance between towers above the preferred maximum street wall height of 11 metres.

**Building Design**

~~Establish a low scale podium and reduce the visual prominence of the upper levels with setbacks as indicated in Table 1 and Figure 1, 2 and 4 for Parcel A and B.~~

Ensure that Parcel A presents as a distinctive landmark building of high architectural quality, providing a point of positive reference to South Geelong Station and further enhances the character, function, and appearance of the surrounding area, including the public realm.

Design buildings to address front, oblique and side views.

## GREATER GEELONG PLANNING SCHEME

Mitigate the impact of visual bulk with well-articulated facades that include variation in forms, materials, openings, and recesses and building separation in upper levels.

Create visual interest in upper levels through the provision of balconies, eaves, terraces, and verandas that ~~are visually recessed from adjoining streets and laneways but~~ ensures passive surveillance of the public realm.

Avoid highly reflective glazing in the façade that diminishes the ability of the development to fit within the historic context.

Avoid blank walls visible from the public realm including where they abut a street or laneway. Include an interim façade strategy when blank walls are visually prominent while adjoining properties are being realised.

Introduce midblock pedestrian links connecting Lonsdale Street to Carr Street generally in accordance with as indicated in Map 3.

Ensure new pedestrian connections are safe (comply with Crime Prevention Through Environmental Design Guidelines), receive adequate access to sunlight and are open to the sky.

Development should meet the following minimum floor to floor dimensions:

- 4 metres at ground level
- 3.2 - 3.5 metres for residential and non-residential uses in the levels above.

### Public Realm Interface

Provide opportunities for active surveillance of the public realm including plazas and where practical laneways and car parks. Avoid concealed alcoves and hidden entrances that affect perceived safety.

Maximise the usability and attractiveness of new public spaces by introducing ground floor uses that ensure interaction with the street and enhance the pedestrian environment.

Provide ground floor tenancy frontages that can accommodate a wide range of commercial and retail uses while maintaining the fine grain character of the area on Parcels A and B.

Avoid large signage and/or non-transparent glazing at ground level.

Provide separate residential and commercial entrances that are clearly legible from the street. Dwellings at the ground level should be provided with individual entrances from the footpath.

Create opportunities for landscaping, particularly along the existing and new pedestrian links, laneways and shared street.

Incorporate façade design and lighting that establishes a sense of safety and security after hours.

Provide active frontages to both sides of pedestrian connections through development where possible.

Ensure direct, attractive, safe, and well-lit pedestrian and cycle access between main streets and South Geelong Station.

### Overshadowing

Development should not overshadow the opposite footpath of adjoining streets (excluding Carr Street) between 10:00 am and 3:00 pm on 22 September.

Minimise the impact on solar access from new development to established residential areas, including balconies, terraces and habitable room windows.

### Wind and weather protection **for buildings 18m or taller**

Ensure safe wind conditions as specified in Table 2 on public land, publicly accessible areas on private land, private open space and communal open space; and

Achieve comfortable wind conditions as specified in Table 2 on public land and publicly accessible areas on private land.

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**GREATER GEELONG PLANNING SCHEME**

Avoid the addition of protective screens and other incidental add-ons to offset excessive wind gust levels. Discourage landscaping within public spaces as a means to mitigate wind.

## GREATER GEELONG PLANNING SCHEME

Incorporate continuous weather protection such as awnings, openings and architectural detail that promotes activity and visual interest. Ensure the soffit of awnings are well detailed and attractive when viewed from the street.

Ensure weather protection technique employed does not impinge existing or future street trees.

**Table 2 to Schedule 50 to Clause 43.02**

Wind condition	Requirement
Comfortable <u>Safe</u> wind conditions	Hourly mean wind speed or gust equivalent mean speed (3 second gust wind speed divided by 1.85), from all wind directions combined with probability of exceedance less than 20% of the time, equal to or less than: <ul style="list-style-type: none"> <li>▪ 3 metres per second for sitting areas,</li> <li>▪ 4 metres per second for standing areas,</li> <li>▪ 5 metres per second for walking areas.</li> </ul>
Unsafe wind conditions	Annual maximum 3 second gust wind speed exceeding 20 metres per second with a probability of exceedance of 0.1% considering at least 16 wind directions.

### Acoustic attenuation

Incorporate acoustic treatments to limit noise transmission from external noise sources such as trains, train station, Kardinia Park & Stadium, entertainment venues and public spaces to a level that is comfortable for residential uses.

Incorporate noise attenuation measures and suppression techniques to ensure noise does not unreasonably affect the amenity of nearby land within a General Residential Zone and Residential Growth Zone.

A permit must contain conditions which give effect to the Acoustic Assessment Report, where the land has been identified as a lot that requires mitigation measures against noise impacts.

### Sustainable Design

Provide green roofs where viable in accessible and serviceable locations and where visible from the public realm or within the development.

Ensure landscaping includes high-quality finishes, well-designed seating areas, lighting and places for diverse uses.

### Access, Parking and Loading Areas

Provide an easily identifiable entrance and sense of arrival and safety through placing the primary pedestrian entrances to dwellings and tenancies on the street frontage.

Minimise the impact of vehicle access and car parking on the public realm by:

- Incorporating streetscapes and building interfaces that encourage movement by means other than private car.
  - Consolidating vehicular entries and locate vehicle access at the rear of buildings generally in accordance with Map 3: (Parcel A via Bellerine Street, and Parcel B and C via future shared street or existing laneway).
- Locating private car parking where it cannot be seen from the public realm.
- Designing basement or carparking garage entrance to have minimal visual impact.

Provide car parking within the basement, within a consolidated carpark suitable for repurposing or conceal it from the public realm if located within the podium by sleeving with active uses.

**GREATER GEELONG PLANNING SCHEME**

Support shared car parking arrangements across the site.

## GREATER GEELONG PLANNING SCHEME

Design all above ground car parking areas to support adaptive reuse over time.

Support active transport through the supply of bicycle parking in both commercial and residential development and change facilities for commercial development.

Provide centralised loading areas for commercial uses across the site away from pedestrian priority areas.

Provide a central bin storage area for each development which is not visible from the street.

### 3.0 Subdivision

Proposed C432ggee

A permit to subdivide land must meet the following requirements:

- Supports the partial closure and reconfiguration of Carr Street establishing land parcels generally in accordance with Map 1.

An application to subdivide land into lots each containing an existing dwelling or car parking space is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act.

### 4.0 Signs

Proposed C432ggee

None specified

### 5.0 Application requirements

Proposed C432ggee

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

An Urban Context Report and Design Response which:

- Responds to the urban context (including built form character of adjacent and nearby buildings, adjacent and nearby heritage places, and equitable outcomes for potential development on adjoining sites);
- Implements recommendations from the other technical reports required under the application requirements of this schedule;
- Responds to the design objectives and buildings and works requirements of this schedule; and
- Provides design excellence through providing sustainable, high quality architecture which articulates and minimises visual bulk, and landscape architecture and urban design enforcing liveability and activating the public realm.

An Environmentally Sustainable Design Management Plan (EMP) prepared by a suitably qualified environmental engineer or equivalent that demonstrates how the development provides for best practice environmentally sustainable design.

A wind report for buildings exceeding a height of 18 metres (5 storeys).

Any application for subdivision or development of land for Accommodation, Education Centre (other than Tertiary institution and Employment training centre) or Hospital, must be accompanied by an acoustic assessment report prepared by a qualified acoustic engineer or other suitably skilled person to the satisfaction of the responsible authority which:

- Applies the following noise objectives:
  - Not greater than 35 dB LAeq,8h when measured within a sleeping area between 10 pm and 6 am.

## GREATER GEELONG PLANNING SCHEME

- Not greater than 40 dB LAeq,16h when measured within a living area between 6 am and 10 pm.
- For areas other than sleeping and living areas, not greater than the median value of the range of recommended designed sound levels of Australian Standard AS/NZ 2107:2016 (Acoustics – Recommended design sound level and reverberation times for building interiors).
- Noise levels should be assessed:
  - Considering the cumulative noise from all sources impacting on the proposal including road traffic noise and industry noise, as well as potential other potential noise sources; and
  - In unfurnished rooms with a finished floor and the windows closed and be based on average external noise levels measured as part of a noise level assessment.
- Addresses noise compatible design for buildings, with siting, orientation, and internal layout, to be considered prior to setting building envelope performance requirements.
- Addresses potential noise character (such as tonality, impulsiveness or intermittency) is addressed wherever relevant, including through the application of adjustments to the internal noise levels that are determined using the procedures to adjust industry noise levels of the Noise Protocol.

A Traffic Impact Assessment Report (TIAR), where relevant.

A Circulation and Movement Plan prepared by a suitably qualified person that demonstrates how the internal road hierarchy, pedestrian and cycling priority areas, and loading and building services achieve a legible and coherent structure improving the pedestrian connectivity and creating safe public spaces.

Plans, elevations, and section drawings for any car parking proposed at or above ground level to show finished floor levels and a statement by a suitably qualified engineer to demonstrate the capacity for this to be adapted to alternative uses.

A Landscape Plan detailing proposed hard and soft landscape elements, plant schedule, plant container details and maintenance and irrigation systems.

Scaled shadow diagrams to show existing and proposed shadows at hourly intervals between 10:00 am and 3:00 pm on 22 September, to demonstrate compliance with overshadowing requirements.

A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development.

Any application for development of land for a dwelling including a dwelling as part of a mixed-use development should provide an Affordable Housing Delivery Strategy to the satisfaction of the responsible authority which sets out:

- How affordable housing is to be delivered in accordance with social and affordable housing policy at Clause 16.01 of the Planning Scheme.
- The method of implementing the strategy, such as by an agreement under Section 173 of the Planning and Environment Act 1987.
- Locations for the affordable housing to be delivered.
- A summary of the range of housing types, densities and sizes.
- Staging requirements and ensuring that affordable housing is provided in a timely manner as development occurs.

## 6.0

Proposed C432ggee

### Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

## GREATER GEELONG PLANNING SCHEME

- Whether the development appropriately responds to the design objectives and requirements ~~including building heights, street wall heights and setbacks~~ in this schedule.
- Whether the development or subdivision is generally in accordance with the maps included in this schedule
- Whether the subdivision of the site supports the partial closure and reconfiguration of Carr Street and ensures equitable development opportunities to all three sites.
- Whether the development employs an appropriate massing strategy that reduces visual bulk.
- Whether the development supports high-quality pedestrian amenity within the public realm, in relation to human scale and overshadowing.
- Whether the buildings adopt and enables a diversity of forms, typologies, and architectural language.
- Whether the application incorporates acoustic treatments to limit the impact from noise from railway operation and future commercial activities.
- Whether the application includes an Affordable Housing Delivery Strategy to the satisfaction of the responsible authority.
- Whether the development incorporates ecologically sustainable design practices.
- Whether the proposal allows for open sightlines and high levels of passive surveillance by users and residents.
- Whether the development avoids:
  - Extensive blank walls along streets.
  - Buildings oriented towards internal spaces rather than streets.
  - Alcoves and recesses that may provide hiding places or that may collect dirt and litter
- Whether the development provides active street frontages including integration of required servicing into the façade away from key pedestrian spaces and public spaces, co-location of service cabinets internal to loading, waste or parking areas where possible.
- Whether the development supports a staged development of the site in a manner which effectively manages car parking demand and access, enhances pedestrian connections and public realm within and around the Station Hub.
- Whether car parking demand can be appropriately managed.
- Whether the development achieves design excellence by providing high quality and innovative architecture, landscape architecture and urban design which demonstrates function, liveability, sustainability and public contribution to buildings and urban spaces.
- Whether the development achieves appropriate levels of residential density and is not an underdevelopment of the site.

### Variations to preferred requirements

Where an application proposes to exceed, or vary a preferred requirement under a discretionary control contained within this schedule consider:

- Whether the design objectives have been met.
- Whether the street wall heights and upper level setbacks of development minimises the impact on the character and appearance of any Heritage Area opposite or adjacent to the site.
- Whether a variation to a requirement avoids or minimises adverse off-site impacts such as visual bulk, overlooking and overshadowing to adjacent residential properties and the public realm.

**GREATER GEELONG PLANNING SCHEME**

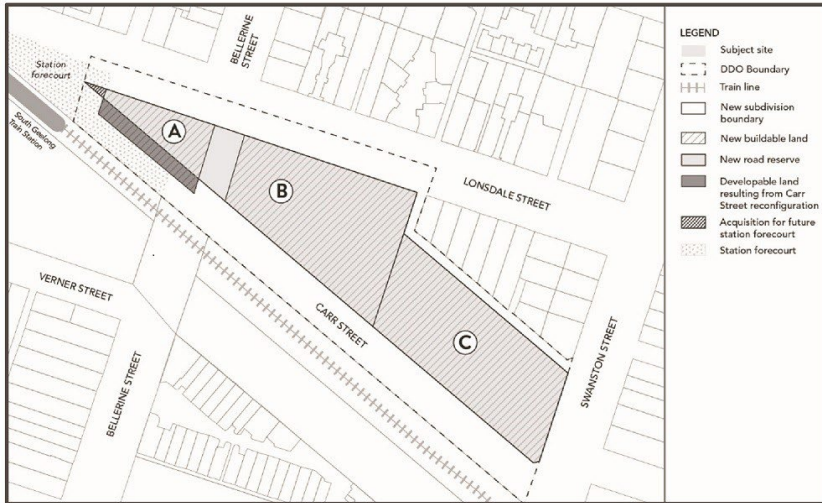
- Whether the proposal can enable a variation to a requirement without material adverse offsite impacts.
- Whether the proposal presents, or substantially facilitates an improved architectural outcome.
- Whether the proposal results in, or substantially facilitates, the delivery of appropriately secured public benefits including:
  - Provision of pedestrian links or public open space in excess of any minimum requirement in this Scheme

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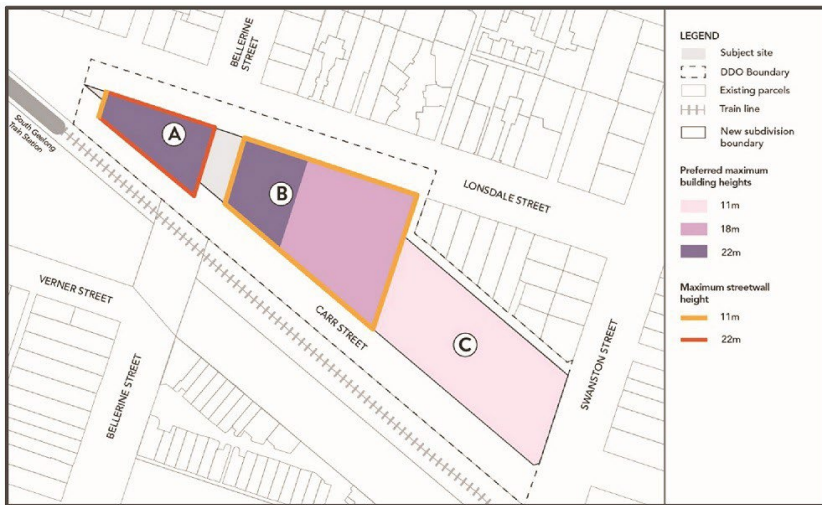
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GREATER GEELONG PLANNING SCHEME

Map 1 to Schedule 50 to Clause 43.02

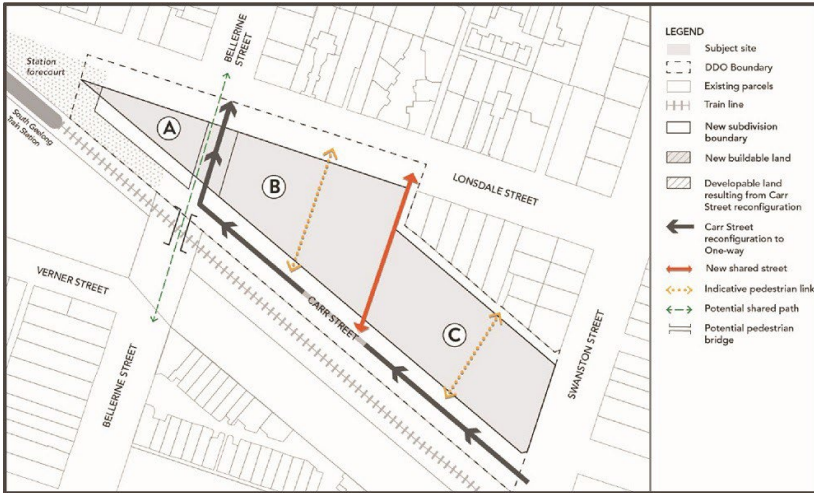


Map 2 to Schedule 50 to Clause 43.02



Map 3 to Schedule 50 to Clause 43.02

GREATER GEELONG PLANNING SCHEME



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Proposed C432ggee

## SCHEDULE 54 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO54**

### SOUTH GEELONG - MOORABOOL STREET KEY REDEVELOPMENT AREA

#### 1.0 Design objectives

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Proposed C432ggee

To promote contemporary design and built form that demonstrates design excellence, enables a diverse mix of housing and commercial uses, and contributes to the distinctive boulevard character and greening of Moorabool Street.

To support lot consolidation to achieve [preferred](#) maximum building heights, desired scale and setbacks that also deliver high levels of internal amenity.

To ensure that development provides equitable development rights to adjoining sites and allows reasonable access to sunlight, daylight, outlook, and privacy to habitable spaces.

To ensure that development supports accessibility and ease of walking, cycling, and use of public transport, to, from and around the Moorabool Street area.

To protect the internal residential amenity of new buildings by limiting noise transmission and provide acoustic measures to protect from external noise sources such as trains, commercial activities and public spaces.

#### 2.0 Buildings and works

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Proposed C432ggee

The following buildings and works requirements apply to an application to construct a building or construct or carry out works.

##### [Exemption](#)

[A permit is not required for buildings and works for a single storey structure or where the proposed buildings and works to do not increase the existing floor area by more than 100 square metres.](#)

##### **General Requirement**

Consolidate sites where appropriate to maximise development potential while balancing equitable development opportunities on adjacent lots.

##### **Building height**

Development should not exceed the preferred maximum building heights as shown in Map 1 to this schedule.

The preferred maximum building height does not include architectural features, masts and building services including plant rooms, air conditioning, lift overruns, structures associated with roof top gardens, decks and communal outdoor spaces and their ancillary facilities or enclosed stairwells provided that the following criteria are met:

- Not more than 50% of the roof area is occupied by equipment (other than solar panel or greening).
- The equipment is located to minimise additional overshadowing and reduce visual impact.
- The equipment does not exceed the [preferred maximum](#) height limit by more than 3.6 metres; and
- The equipment and screening are integrated into the design of the building to the satisfaction of the responsibly authority.

Development should meet the following minimum floor to floor dimensions:

- 4 metres at ground level
- 3.2 - 3.5 metres for residential and non-residential uses in the levels above.

**Interfaces**

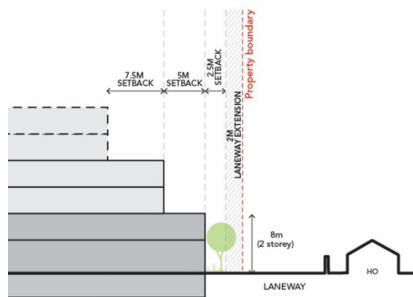
Development should not exceed the [preferred](#) maximum street wall heights specified in Map 1 and Table 1 to this schedule

Development should provide a [preferred](#) 2 metre setback to boundaries adjoining laneways [as indicated in Figure 1 and 2](#) and should be generally in accordance with the preferred setbacks specified in Table 1 and Figures 1 to 46.

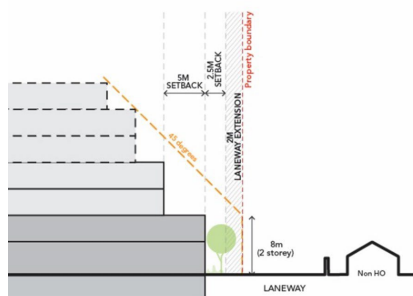
**Table 1 to Schedule 54 of Clause 43.02**

Interface Type	<a href="#">Preferred</a> <del>M</del> maximum street wall / podium height	<a href="#">Preferred</a> setback above street wall / podium
Interface Type A	None.	None.
Interface Type B1	<a href="#">118</a> metres ( <a href="#">32</a> storeys)	4 metres to Moorabool Street
Interface Type B2	<a href="#">118</a> metres ( <a href="#">32</a> storeys)	3 metres to Lonsdale Street
Interface Type C1	11 metres (3 storeys)	4 metres to Moorabool Street, Verner Street, Foster Street. Park Crescent
Interface Type C2	11 metres (3 storeys)	3 metres to Moorabool Street, Baliang Street, Fyans Street
<a href="#">Interface Type L1 and L2</a>	<a href="#">11</a> metres (3 storeys)	<a href="#">4.5</a> metres to Lambs Place

**Figure 1: Laneway Interface Type L1 – To properties within Heritage Overlay**



**Figure 2: Laneway Interface Type L2 – To properties outside of Heritage Overlay**



**Figure 31: Interface to future pedestrian links**

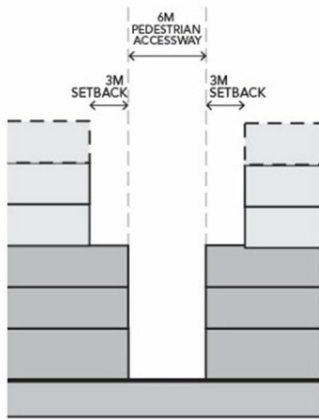


Figure 42: Interface to Sons of Temperance Hall

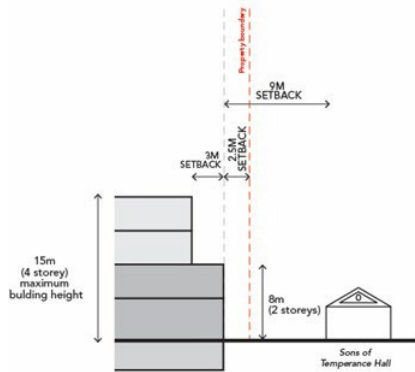


Figure 53: Interface to South Geelong Uniting Church (Moorabool Street)

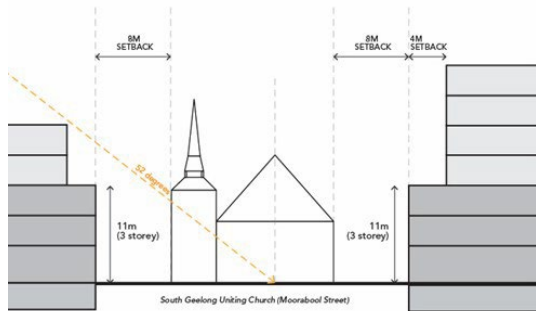
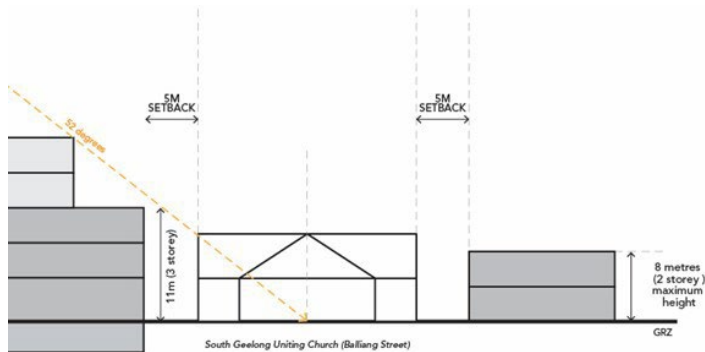


Figure 64: Interface to South Geelong Uniting Church (Balliang Street)



Building Setbacks and Separation

All buildings should generally be built to side boundaries.

~~Development above the maximum street wall height of 11 metres or 3 storeys should be set back a minimum of 4.5 metres from boundaries unless a different setback is listed in Table 1.~~

Development should provide a minimum of 9 metres distance between towers above the [preferred](#) maximum street wall height of 11 metres or 3 storeys.

### **Building Design**

Incorporate low scale podium and distinctive upper levels through variation in form, materiality, recesses, and openings with emphasis on ground floor detailing that achieves human scale with an active street experience.

Ensure that building design and setbacks at the podium level provide separation that assists to mitigate visual bulk and excessive screening to avoid overlooking.

Design buildings to address front, oblique and side views.

Achieve high quality design outcomes on corner sites that address both street frontages with door openings, windows and interesting soffit lighting and treatment at street level.

Create visual interest in upper levels through the provision of balconies, eaves, terraces, and verandas that ~~are visually recessed from adjoining streets and laneways but~~ ensures passive surveillance of the public realm.

Discourage primary aspect to side boundaries for residential dwellings.

Avoid extensive expanses of unbroken solid or reflective blank glass or blank walls visible to the public realm including where they abut a street or laneway. Include an interim façade strategy when blank walls are visually prominent while adjoining properties are being realised.

~~Consolidate rear setbacks to avoid 'wedding cake' profiles.~~

Create midblock pedestrian links connecting Moorabool Street to Lamb Place [generally](#) as indicated in Map 1.

Ensure new pedestrian links are safe (comply with Crime Prevention Through Environmental Design Guidelines), receive adequate access to sunlight and are open to the sky as indicated in Figure [31](#).

Ensure devices and privacy screens do not substantially reduce the potential for day lighting or outlook for residents.

### **Public Realm Interface**

Ensure ground floor activity, façade and internal layout is designed to facilitate visual and physical access between the building and adjoining public realm, enhancing the pedestrian environment.

Retain and plant street trees along Moorabool Street to create a boulevard effect.

Avoid concealed alcoves, recessed nooks and hidden entrances that affect perceived safety.

Avoid large signage and/or non-transparent glazing at ground level.

Provide separate residential and commercial entrances that are clearly legible from the street. Dwellings at the ground level should be provided with individual entrances from the footpath.

Provide a high level of passive surveillance from upper levels of building ~~to future adjacent to~~ open space ~~including, rear laneways, public open space~~ and forecourts ~~and space~~ surrounding the former South Geelong Uniting Church including the former Methodist Hall at 451-471 Moorabool Street and 4-6 Balliang Street and the Sons of Temperance Hall at 441 Moorabool Street.

Incorporate façade design and lighting that establishes a sense of safety and security after hours including along main streets, side streets and laneways.

Provide active frontages along pedestrian connections where possible.

### **Overshadowing**

~~Avoid~~ Minimise any overshadowing to the secluded private open space of dwellings to the rear of the Moorabool Street Key ~~Red~~ Development Area between 10 am and 2 pm on 22 September.

~~Avoid additional shadows over public open space between 10 am and 3 pm at the winter solstice.~~

Minimise additional overshadowing at the spring equinox to existing and future forecourts of the Former South Geelong Uniting Church including the former Methodist Hall at 451-471 Moorabool Street and 4-6 Balliang Street and Sons of Temperance Hall at 441 Moorabool Street as indicated in Figure 53 and 64 and should be designed to maximise solar access at the spring equinox to the new open space required through setbacks around these buildings.

~~Minimise the impact on solar access from new development to residential areas to the east of the site, communal and secluded open spaces within the development, including balconies, terraces and habitable room windows.~~

**Wind and weather protection**

Ensure safe wind conditions as specified in Table 2 on public land, publicly accessible areas on private land, private open space and communal open space.

Achieve comfortable wind conditions as specified in Table 2 in public land and publicly accessible areas on private land.

Avoid the addition of protective screens and other incidental add-ons to offset excessive wind gust levels.

Discourage the use of landscaping within public spaces to mitigate wind.

**Table 2 to Schedule 54 to Clause 43.02**

Wind condition	Requirement
<del>Comfortable</del> <u>Safe</u> wind conditions	Hourly mean wind speed or gust equivalent mean speed (3 second gust wind speed divided by 1.85), from all wind directions combined with probability of exceedance less than 20% of the time, equal to or less than: <ul style="list-style-type: none"> <li>▪ 3 metres per second for sitting areas,</li> <li>▪ 4 metres per second for standing areas,</li> <li>▪ 5 metres per second for walking areas.</li> </ul>
Unsafe wind conditions	Annual maximum 3 second gust wind speed exceeding 20 metres per second with a probability of exceedance of 0.1% considering at least 16 wind directions.

Incorporate along Moorabool Street continuous weather protection such as awnings, openings and architectural detail that promotes activity and visual interest. Ensure the soffit of awnings are well detailed and attractive when viewed from the street.

Ensure weather protection technique employed does not impinge upon existing or future street trees.

**Acoustic attenuation**

Incorporate acoustic treatments to limit noise transmission from external noise sources such as trains, South Geelong train station, Kardinia Park and Stadium, entertainment venues and public spaces to a level that is comfortable for residential uses.

Incorporate noise attenuation measures and suppression techniques to ensure noise does not unreasonably affect the amenity of nearby land within a residential zone.

A permit must contain conditions which give effect to the Acoustic Assessment Report, where the land has been identified as a lot that requires mitigation measures against noise impacts.

### Access, Parking and Loading Areas

Provide an easily identifiable entrance, sense of arrival and safety through placing the primary pedestrian entrances to dwellings and tenancies on the street frontage.

Minimise the impact of vehicle access and car parking on the public realm by:

- Providing vehicle access to side streets or rear laneways.
- Locating car parking where it cannot be seen from the public realm.
- Designing vehicular entrances to have minimal visual impact.
- Providing car parking within the basement or conceal it from the public realm if located within the podium by [sleeving-encouraging](#) active use [frontages](#).

Avoid vehicular crossovers to Moorabool Street and Lonsdale Street.

Remove redundant crossovers from Moorabool Street

~~Ensure the design of rear setbacks are as indicated in Figure 1 and 2 to facilitate the expansion of Lamb Place for vehicular access.~~

Design all above ground car parking areas to support adaptive reuse over time.

Support reduced or shared car parking amenities for [residential](#) development where possible.

Locate bin enclosures and other storage at the rear of buildings and screened from public view to ensure a tidy presentation of streets and laneways.

### Sustainable Design

Integrate landscape design with building form and layout.

Provide green roofs in accessible, serviceable, and visible parts of the roof. Incorporate vertical gardens where possible.

### Fencing

Discourage fencing on Moorabool Street [and Lambs Place](#).

Ensure a side or rear boundary fence does not exceed 1800 mm in height above ground level at any point and follows any change in level along the property boundary.

Discourage gates that open directly over a footpath or road.

### Exemption from notice and review

An application to construct a building or construct or carry out works is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act. This exemption does not apply to land within 30 metres of land (not a road) which is in a residential zone, land used for a hospital or an education centre or land in a Public Acquisition Overlay to be acquired for a hospital or an education centre.

## 3.0

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### Subdivision

#### Exemption from notice and review

An application to subdivide land is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act. This exemption does not apply to land within 30 metres of land (not a road) which is in a residential zone, land used for a hospital or an education centre or land in a Public Acquisition Overlay to be acquired for a hospital or an education centre.

## 4.0

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Proposed C432ggee

### Signs

None specified

**5.0**

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Proposed C432ggee

**Application requirements**

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

An Urban Context Report and Design Response which:

- Responds to the urban context (including built form character of adjacent and nearby buildings, adjacent and nearby heritage places, and equitable outcomes for potential development on adjoining sites).
- Implement recommendations from the other technical reports required under the application requirements of this schedule.
- Responds to the design objectives and buildings and works requirements of this schedule.
- Provides design excellence through providing sustainable, high quality architecture which articulates and minimises visual bulk, and landscape architecture and urban design enforcing liveability and activating the public realm.

An Environmentally Sustainable Design Management Plan (EMP) prepared by a suitably qualified environmental engineer or equivalent that demonstrates how the development provides for best practice environmentally sustainable design.

A Wind Report for buildings exceeding a height of 18 metres (5 storeys).

Any application for subdivision or development of land for Accommodation, Education Centre (other than Tertiary institution and Employment training centre) or Hospital, must be accompanied by an acoustic assessment report prepared by a qualified acoustic engineer or other suitably skilled person to the satisfaction of the responsible authority which:

- Applies the following noise objectives:
  - Not greater than 35 dB LAeq,8h when measured within a sleeping area between 10pm and 6am.
  - Not greater than 40 dB LAeq,16h when measured within a living area between 6am and 10pm.
  - For areas other than sleeping and living areas, not greater than the median value of the range of recommended designed sound levels of Australian Standard AS/NZ 2107:2016 (Acoustics – Recommended design sound level and reverberation times for building interiors).
- Noise levels should be assessed:
  - Considering the cumulative noise from all sources impacting on the proposal including road traffic noise and industry noise, as well as potential other potential noise sources; and
  - In unfurnished rooms with a finished floor and the windows closed and be based on average external noise levels measured as part of a noise level assessment.
- Addresses noise compatible design for buildings, with siting, orientation, and internal layout, to be considered prior to setting building envelope performance requirements.

A Traffic Impact Assessment Report (TIAR), where relevant.

Plans, elevations, and section drawings for any car parking proposed at or above ground level to show finished floor levels and a statement by a suitably qualified engineer to demonstrate the capacity for this to be adapted to alternative uses.

A Landscape Plan detailing proposed hard and soft landscape elements, plant schedule, plant container details and maintenance and irrigation systems.

Scaled shadow diagrams to show existing and proposed shadows at hourly intervals between 10:00 am and 3:00 pm on 22 September, to demonstrate compliance with overshadowing requirements.

Streetscape elevations showing the existing streetscape, and how the proposed development sits within the streetscape. A three-dimensional perspective which shows the proposed development within the streetscape in the context of adjacent development.

Any application for development of land for a dwelling including a dwelling as part of a mixed-use development should provide an Affordable Housing Delivery Strategy to the satisfaction of the responsible authority which sets out:

- How affordable housing is to be delivered in accordance with social and affordable housing policy at Clause 16.01 of the Planning Scheme.
- The method of implementing the strategy, such as by an agreement under Section 173 of the Planning and Environment Act 1987.
- Locations for the affordable housing to be delivered.
- A summary of the range of housing types, densities and sizes.
- Staging requirements and ensuring that affordable housing is provided in a timely manner as development occurs.

## 6.0

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Proposed C432ggee

### Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the development appropriately responds to the design objectives and requirements including building heights, streetwall heights and setbacks in this schedule.
- Whether the development delivers efficient built form outcomes that manages visual and internal amenity through site consolidation.
- Whether the development employs an appropriate massing strategy that reduces visual bulk particularly when viewed from the low scale surrounds.
- Whether the development supports high-quality pedestrian amenity within the public realm, in relation to human scale and overshadowing.
- Whether the application incorporates acoustic treatments to limit the impact from noise from railway operations and activities at Kardinia Park and Stadium.
- Whether the application includes an Affordable Housing Delivery Strategy to the satisfaction of the responsible authority.
- Whether the development incorporates ecologically sustainable design practices.
- Whether the proposal allows for open sightlines and high levels of passive surveillance by users and residents.
- Whether the development avoids:
  - Extensive blank walls along streets.
  - Buildings oriented towards internal spaces rather than streets.
  - Alcoves and recesses that may provide hiding places or that may collect dirt and litter.
- Whether the development provides active street frontages including integration of required servicing into the façade away from key pedestrian and public spaces, co-location of service cabinets internal to loading, waste or parking areas where possible.
- Whether the development achieves design excellence by providing high quality and innovative

- architecture, landscape architecture and urban design which demonstrates function, liveability, sustainability and public contribution to buildings and urban spaces.
- Whether car parking demand can be appropriately managed.

### **Variations to preferred requirements**

Where an application proposes to exceed, or vary a preferred requirement under a discretionary control contained within this schedule consider:

- Whether the design objectives have been met.
- Whether a variation to a requirement avoids or minimises adverse off-site impacts such as visual bulk, overlooking and overshadowing to adjacent residential properties and the public realm.
- Whether the proposal can enable a variation to a requirement without material adverse offsite impacts.
- Whether the proposal presents, or substantially facilitates an improved architectural outcome.
- Whether the proposal results in, or substantially facilitates, the delivery of appropriately secured public benefits including:
  - Provision of pedestrian links or public open space in excess of any minimum requirement in this Scheme.

Map 1 to Schedule 54 to Clause 43.02



GREATER GEELONG PLANNING SCHEME

[CoGG Day 1 Version  
28/10/24](#)

31/07/2018  
VC148

**SCHEDULE TO CLAUSE 72.08 BACKGROUND DOCUMENTS**

**1.0**

Proposed C432ggee

**Background documents**

Name of background document	Amendment number - clause reference
Armstrong Creek Urban Growth Plan Background Report (City of Greater Geelong, 2006)	Clauses 02 and 17
Armstrong Creek Urban Growth Plan (Collie Pty Ltd, 2010, Amended 2012)	Clause 02
Ashby Heritage Review Stage 2 (Dr David Rowe Authentic Heritage Services Pty Ltd and Wendy Jacobs, 2009)	Clauses 02 and 15
Barwon Heads Structure Plan (City of Greater Geelong, 2010)	Clauses 02 and 11
Batman Park & Boat Sheds, The Esplanade, Indented Head, Heritage Review (Dr David Rowe Authentic Heritage Services and Wendy Jacobs, 2013)	Clauses 02 and 15
Bellarine Heritage Study Volumes 1-3 (Huddle, Howe, Lewis and Francis, 1996)	Clauses 02 and 15
Bellarine Peninsula Strategic Plan (City of Greater Geelong, 2006)	Clauses 02 and 11
Belmont Heritage Areas Report Volume 1 (Dr David Rowe, Authentic Heritage Services Pty Ltd, 2007)	Clauses 02 and 15
Belmont Heritage Areas: Inventory of Places Volume 2 (Dr David Rowe Authentic Heritage Services Pty Ltd, 2007)	Clauses 02 and 15
Belmont Heritage Report: Individual Citations Volume 3 (Dr David Rowe Authentic Heritage Services Pty Ltd, 2007)	Clauses 02 and 15
Buckley Falls Management Plan (Department of Conservation and Natural Resources, 1990)	Clause 02
Building in Wildfire Management Overlay Areas (CFA, 2002)	Clause 02 and 11
Central Geelong Framework Plan - A plan for the heart of Djilang (Department of Environment, Land, Water and Planning, 2023)	Clauses 37.08-s1, 43.02-s47, 43.04-s38 to 45
Central West Victoria Estuaries Coastal Action Plan (Western Coastal Board, 2005)	Clauses 02, 12 and 13
Central West Victoria Regional Coastal Action Plan (Western Coastal Board, 2003)	Clauses 02, 12 and 13
Ceres Heritage Citations Project (Dr David Rowe Authentic Heritage Services Pty Ltd and Wendy Jacobs, 2017 updated 2018)	Clauses 02 and 15
City Fringe Heritage Area Review Part 1 and Part 2 (RBA Architects + Conservation Consultants Pty Ltd, 2017 updated 2018)	Clauses 02 and 15
City of Geelong West Urban Conservation Study Volumes 1-2 (Huddle, Aitken and Honman, 1986)	Clauses 02 and 15
City of Greater Geelong Economic Development Strategy (City of Greater Geelong, 2005)	Clauses 02 and 17
City of Greater Geelong Environment Management Strategy 2006 - 2011 (City of Greater Geelong, 2006)	Clauses 02, 12 and 13
City of Greater Geelong Housing Diversity Strategy (alphaPlan, David Lock Associates and City of Greater Geelong, 2007)	Clauses 02, 11, 15 and 16
City of Greater Geelong Housing Strategy Background and Issues Report (Swinburne University, 2005)	Clauses 02, 11, 15 and 16

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<b>Name of background document</b>	<b>Amendment number - clause reference</b>
City of Greater Geelong Retail Strategy 2020-2036 (City of Greater Geelong and SGS Economics and Planning, August 2020)	Clauses 02, 11, 15 and 17
City of Greater Geelong Rural Land Use Strategy (Parsons Brinckerhoff, 2007)	Clauses 02, 14, 17 and 19
City of Greater Geelong Settlement Strategy (City of Greater Geelong, August 2020)	Clauses 02, 11, 15, 16, 17, 19
<i>City of Greater Geelong, South Geelong UDF Heritage Review, November 2023</i>	<i>Clauses 02 and 15</i>
City of Greater Geelong Stormwater Management Plan (WBM Oceanics Australia, 2002)	Clauses 02, 12, 13 and 19
City of Greater Geelong Urban Furniture Style Manual (Taylor and Cullity Pty Ltd, 1996)	Clauses 02, 11, 15 and 16
City of Newtown Urban Conservation Study Volumes 1-4 (Context Pty Ltd, 1991)	Clauses 02 and 15
City of Newtown Urban Conservation Study, Volumes 5(a) and 5(b) (Richard Peterson, 1997)	Clauses 02 and 15
Coastal Spaces Landscape Assessment Study (Planisphere, 2006)	Clauses 02 and 11
Code of Practice for The Greyhound Industry Dealing with Greyhound Premises and The Keeping and Welfare of Greyhounds (Greyhound Racing Victoria, March 2008)	Clauses 14
Corangamite Regional Catchment Strategy 2013-2019 (Corangamite Catchment Management Authority, 2013)	Clauses 02, 12, 13 and 19
Corio Bay Coastal Action Plan (Central Coastal Board and the City of Greater Geelong, 2005)	Clauses 02, 12, 13 and 19
Corio Norlane Structure Plan Background Report (City of Greater Geelong and the Department of Planning and Community Development, 2011)	Clause 02 and 11
Corio Norlane Structure Plan (City of Greater Geelong and Department of Planning and Community Development, 2012)	Clause 02 and 11
Drysdale-Clifton Springs Structure Plan (City of Greater Geelong, 2010)	Clauses 02 and 11
Drysdale Urban Design Framework (City of Greater Geelong, 2012)	Clauses 02 and 11
Former Dennys Lascelles Woolstore 20 Brougham Street Heritage Assessment (Dr David Rowe Authentic Heritage Services Pty Ltd and Wendy Jacobs, 2017)	Clauses 02 and 15
Geelong Biodiversity Strategy (City of Greater Geelong, 2003)	Clauses 02, 12, 13 and 19
Geelong City Urban Conservation Study 'C' Citations, Volume 4(a) (Helen Lardner, 1995)	Clauses 02 and 15
Geelong City Urban Conservation Study, Volumes 2-5 (Graeme Butler for the City of Geelong, 1991)	Clauses 02 and 15
Geelong Economic Development Strategy 2005-2010 (City of Greater Geelong, 2005)	Clause 02, 14, 17 and 18
Geelong Flood Mitigation Strategy (Gutteridge Haskins & Davey Pty Ltd, 1997)	Clauses 02, 12, 13 and 19
Geelong Region Historic Buildings and Objects Study Volumes 1-3 (Allan Willingham, 1986)	Clauses 02 and 15
Geelong Regional Floodland Study (Geelong Regional Commission, 1979)	Clauses 02, 12, 13 and 19

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Geelong Ring Road Employment Precinct Urban Design Guidelines (City of Greater Geelong, 2010)	Clauses 17 and 43.02-s18
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**GREATER GEELONG PLANNING SCHEME**

<b>Name of background document</b>	<b>Amendment number - clause reference</b>
Geelong Sale Yards Conservation Management Plan (Dr David Rowe and Wendy Jacobs, 2007)	Clauses 02 and 15
Geelong Saleyards Precinct Surface Water Management Strategy (BMT, 2021)	Clauses 02 and 11
Geelong Seafood Industry Strategy (City of Greater Geelong, 2003)	Clauses 02 and 17
Geelong Transit City Station Precinct Urban Design Framework (Hassell, Parsons Brinckerhoff and Charter Keck Cramer, 2008)	Clause 02
Geelong Transport Strategy (Victorian State Government and City of Greater Geelong, 2002)	Clauses 02, 11, 18 and 19
Geelong Verandah Study (Authentic Heritage Services Pty Ltd and Wendy Jacobs, 2006)	Clauses 02, 11, 15 and 16
Geelong Waterfront Safe Harbour Precinct - Public Access & Infrastructure Development Project Master Plan (Terroir Pty Ltd, May 2019)	Clause 02
Geelong Western Wedge Framework (Planisphere and Jones & Whitehead Pty Ltd, April 2005, updated September 2005)	Clauses 02 and 37.08 Schedule 1
Geelong Wetland Strategy (City of Greater Geelong, 2006)	Clauses 02, 12, 13 and 19
Greater Geelong Gaming Policy Framework (City of Greater Geelong, 2007)	Clauses 02 and 17
Greater Geelong Outer Areas Heritage Study Volume 1 (excluding Belmont precincts) and Volumes 2-4 (Authentic Heritage Services Pty Ltd, 2000)	Clauses 02 and 15
Goandra Estate Fence Design Guidelines (Hansen Partnership, July 2023)	Clauses 37.01-s8, 43.02-s49 and 52.02s
Heales Road Industrial Estate Framework Plan (MacroPlan Australia and the City of Greater Geelong, 2007)	Clauses 02 and 17
Heritage citation sheet for the Osborne Park bluestone pillar and cast/wrought iron gates (City of Greater Geelong, 2016)	Clauses 02 and 15
Heritage citation sheet for the Vietnam Veterans Memorial and Avenue of Honour (City of Greater Geelong, December 2016)	Clauses 02 and 15
Heritage Report 9 Bridge Road, Barwon Heads (Post-Panel Version) (Context Pty Ltd, May 2017)	Clauses 02 and 15
Hovells Creek, Lara: Flooding – December 10, 1988 (Technical Services Department Shire of Corio, 1990)	Clauses 02, 12, 13 and 19
Indented Head Structure Plan (City of Greater Geelong, 2016)	Clauses 02 and 11
Infrastructure Design Manual (Local Government Infrastructure Design Association, 2021)	Clauses 02 and 19
Jetty Road Urban Growth Plan (City of Greater Geelong, 2007 amended September 2008)	Clauses 02 and 11
Lara Heritage Review Phase 2, Volumes 1, 2 and 3 (Dr David Rowe and Wendy Jacobs, 2013 revised 2016)	Clauses 02 and 15
Lara Structure Plan (City of Greater Geelong, 2011)	Clauses 02 and 11
Lara Structure Plan Retail Development Issues (Tim Nott, 2009)	Clauses 02 and 11
Lara Town Centre Urban Design Framework (David Lock Associates, 2006)	Clauses 02 and 11
Leopold Structure Plan (City of Greater Geelong, 2011 amended 2013)	Clauses 02 and 11

**GREATER GEELONG PLANNING SCHEME**

<b>Name of background document</b>	<b>Amendment number - clause reference</b>
Leopold Urban Design Framework (City of Greater Geelong, 2011)	Clauses 02 and 11
Managing Development in Rural Areas, Planning Policy Review (City of Greater Geelong, 2015)	Clauses 02, 14 and 17
Moolap Coastal Strategic Framework Plan (Department of Environment, Land, Water and Planning, 2019)	Clauses 02 and 11
Newtown Heritage Study Volumes 1-3 (City of Greater Geelong, 2008)	Clauses 02 and 15
Newtown West Heritage Review, Volumes 1, 2, 3, 4 & 5 (Dr David Rowe Authentic Heritage Services Pty Ltd & Wendy Jacobs, 2016)	Clauses 02 and 15
Northern and Western Geelong Growth Areas Framework Plan (City of Greater Geelong, August 2020)	Clauses 02, 11, 15, 16 and 19
Ocean Grove Structure Plan (City of Greater Geelong, 2015 amended 2016)	Clauses 02 and 11
Ocean Grove Urban Design Framework (City of Greater Geelong, 2014)	Clauses 02 and 11
Pakington Street North Urban Design Guidelines (Planisphere, 2004)	Clause 15
Pakington Streetscape Improvements Plan (Loder & Bayly, 1990)	Clause 15
Point Lonsdale Structure Plan (Planisphere, 2009 amended 2011)	Clauses 02 and 11
Portarlington Structure Plan (City of Greater Geelong, 2016 amended 2017)	Clauses 02 and 11
Saleyards Precinct Plan (City of Greater Geelong, 2021)	Clauses 02 and 11
South Geelong Urban Design Framework (City of Greater Geelong, 2022)	Clause 11, 16, and 43.02 Schedule 50 and 54
St Leonards Structure Plan (City of Greater Geelong, 2015)	Clauses 02 and 11
Statement of Significance for the former stables to Armytage House and classrooms 19-21 Hermitage Road, Newtown (Dr David Rowe and Kevin Krastins, 2003)	Clauses 02 and 15
Study of Open Space Networks (City of Greater Geelong, 2001)	Clauses 02, 11, 18 and 19
Wandana Structure Plan (City of Greater Geelong, 1998)	Clause 02

## City South Heritage Area- Statement of Significance

City of Greater Geelong, August 2023

<b>Heritage Place:</b>	City South Heritage Area	<b>PS ref no:</b>	HO1641
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### What is significant?

The City South Heritage Area occurs between the southern edge of the Geelong Central Business District and the Fyans Road industrial estate, a residential subdivision incorporating parts of the City's earliest town plan known as the Hoddle Grid, with streetscapes of Victorian, Federation/Edwardian and Interwar Bungalow housing of consistent scale and setback, set along broad landscaped streets, with street trees and modest gardens set behind low and semi-transparent front fences, with most vehicle access segregated to rear service lanes.

Contributory buildings are identified at the end of this document.

### How is it significant?

This City South Heritage Area is of local architectural, historical, aesthetic and scientific significance to the City of Greater Geelong.

### Why is it significant?

The HO1641 City South Heritage Area is of historical cultural heritage significance for its ability to strongly demonstrate a range of historical and aesthetic values associated with its planning and development as a residential area over the period 1850s-1920s. (Criteria A, D, E)

South Geelong, along with the central Geelong CBD, was defined in the earliest town plan for the City, which connected the Barwon River to Corio Bay. The hierarchal street layouts and the early lot subdivision developed by Government Surveyor Robert Hoddle and his assistant Henry Smythe during 1837-38 – the 'Hoddle Grid' - remain a distinctive feature of Geelong's town planning, are well-demonstrated and predominantly intact across the City South Heritage Area. (Criteria A, E, F, H)

The overall urban landscape of City South Heritage Area is significant in demonstrating the early period of the City's European settlement 1850s – 1920s including the aspirations of the working-middle class residents for detached housing and ornamental front yards, albeit of modest stature, within walking distance of the CBD, and the port and riverside industrial areas. (Criteria A, D)

The aesthetic values of the City South Heritage Area are particularly fine and well-recognised by the community, demonstrating such attributes as wide landscaped road reserves, the segregation of vehicle access to rear service lanes, the consistent form and scale of the residential built development, and the attractive blend of predominantly-intact Victorian, Federation and Interwar Bungalow architecture that occurs throughout. (Criteria E)

As an early developmental area within the City, the City South Heritage Area possesses modest scientific value or research potential associated with early occupation and land use within the City. (Criteria C)

### Primary sources

Allan Willingham, *Geelong Region Historic Buildings and Objects Study Volume 2*, 1986

*Authentic Heritage Services (Dr D Rowe), Heritage Character Study, June 2020*

*City of Greater Geelong, Proposed Revision To Ho1641 City South Heritage Area, October 2021*

*Ecology & Heritage Partners Pty Ltd, South Geelong Heritage Precinct HO1641, Peer Review, August 2022.*

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*City of Greater Geelong, South Geelong UDF Heritage Review, November 2023*

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Graeme Butler, *Geelong City Urban Conservation Study, Volume 1*, 1993  
 Graeme Butler, *Geelong City Urban Conservation Study, Volumes 2-5*, 1991  
 Helen Lardner, *Geelong City Urban Conservation Study, Volume 4(a)*, 1995  
 Helen Lardner, *City of Greater Geelong Heritage and Design Guidelines*, 1997  
 RBA Architects + Conservation Consultants, *City Fringe Heritage Area Review*, 2016

Contributory buildings in HO1641

House	Street	Grade
31	Balliang St	Contributory
31	Balliang St	non contributory
39	Balliang St	Contributory
57	Balliang St	non-contributory
59	Balliang St	Contributory
60	Balliang St	non-contributory
61	Balliang St	Contributory
62	Balliang St	non-contributory
63	Balliang St	Contributory
64	Balliang St	Contributory
65	Balliang St	Contributory
66	Balliang St	non-contributory
67	Balliang St	Contributory
68	Balliang St	Contributory
69	Balliang St	<del>Contributory</del> non-contributory
70	Balliang St	Contributory
71	Balliang St	non-contributory
72	Balliang St	Contributory
73	Balliang St	Contributory
74	Balliang St	Contributory
75	Balliang St	Contributory
76	Balliang St	Contributory
77	Balliang St	Contributory

GREATER GEELONG PLANNING SCHEME

78	Balliang St	Contributory
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GREATER GEELONG PLANNING SCHEME

79	Balliang St	Contributory
80	Balliang St	Contributory
81	Balliang St	Contributory
82-84	Balliang St	Contributory
83	Balliang St	Contributory
85	Balliang St	Contributory
2	Bambra St	non contributory
4	Bambra St	non contributory
16	Bambra St	non contributory
1	Bean St	Contributory
5	Bean St	Contributory
6	Bean St	non contributory
7	Bean St	non contributory
8	Bean St	non contributory
153	Bellerine St	Contributory
157	Bellerine St	Contributory
159	Bellerine St	Contributory
161	Bellerine St	Contributory
163	Bellerine St	Contributory
165	Bellerine St	Contributory
166	Bellerine St	Contributory
168	Bellerine St	Contributory
170	Bellerine St	Contributory
172	Bellerine St	Contributory
180	Bellerine St	non contributory
182	Bellerine St	Contributory
184	Bellerine St	Contributory
186	Bellerine St	non contributory
188	Bellerine St	Contributory
189	Bellerine St	non contributory
191	Bellerine St	Contributory

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GREATER GEELONG PLANNING SCHEME

194	Bellerine St	Contributory
195	Bellerine St	Contributory
198	Bellerine St	Contributory
200	Bellerine St	non contributory
201	Bellerine St	non contributory
203	Bellerine St	non contributory
206	Bellerine St	non contributory
207	Bellerine St	Contributory
209	Bellerine St	non contributory
215	Bellerine St	Contributory
217	Bellerine St	Contributory
219	Bellerine St	Contributory
220	Bellerine St	non contributory
221	Bellerine St	Contributory
222	Bellerine St	Contributory
223	Bellerine St	Contributory
224	Bellerine St	Contributory
225	Bellerine St	Contributory
227	Bellerine St	Contributory
228	Bellerine St	Contributory
229	Bellerine St	Contributory
230	Bellerine St	Contributory
232	Bellerine St	non contributory
233	Bellerine St	non contributory
235	Bellerine St	non contributory
237	Bellerine St	Contributory
239	Bellerine St	Contributory
243	Bellerine St	Contributory
245	Bellerine St	Contributory
<del>264</del> 262	<del>Bellerine St</del> Bellerine St	<del>Contributory</del> Contributory
<del>264</del>	<del>Bellerine St</del>	<del>Contributory</del>

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GREATER GEELONG PLANNING SCHEME

271	Bellerine St	non-contributory
273	Bellerine St	Contributory
275	Bellerine St	Contributory
277	Bellerine St	non-contributory
289	Bellerine St	Contributory
291	Bellerine St	Contributory
293	Bellerine St	Contributory
295	Bellerine St	Contributory
297	Bellerine St	Contributory
9/25	Carr St	non contributory
10/25	Carr St	non contributory
11/25	Carr St	non contributory
12/25	Carr St	non contributory
53	Carr St	non-contributory
59	Carr St	non contributory
5	Dew St	non contributory
1	Douglas Place	non contributory
5	Douglas Place	non contributory
7	Douglas Place	non contributory
31	Fenwick St	Contributory
33	Fenwick St	Contributory
35	Fenwick St	Contributory
37	Fenwick St	Contributory
39	Fenwick St	Contributory
1	Fenwick St Sth	non contributory
2	Fenwick St Sth	non contributory
4	Fenwick St Sth	non contributory
130	Fitzroy St	Contributory
132	Fitzroy St	Contributory
134	Fitzroy St	Contributory
136	Fitzroy St	Contributory

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GREATER GEELONG PLANNING SCHEME

138	Fitzroy St	Contributory
5	Foster St	Contributory
7	Foster St	Contributory
8	Foster St	Contributory
9	Foster St	non-contributory
1/10	Foster St	non-contributory
2/10	Foster St	non-contributory
11	Foster St	Contributory
12	Foster St	Contributory
13	Foster St	Contributory
14	Foster St	non-contributory
16	Foster St	Contributory
17	Foster St	Contributory
18	Foster St	Contributory
19	Foster St	Contributory
20	Foster St	Contributory
22	Foster St	Contributory
23	Foster St	Contributory
24	Foster St	Contributory
25	Foster St	Contributory
27	Foster St	Contributory
29	Foster St	non-contributory
31	Foster St	Contributory
32	Foster St	Contributory
33	Foster St	Contributory
34	Foster St	Contributory
35	Foster St	Contributory
36	Foster St	non contributory
37	Foster St	Contributory
39	Foster St	Contributory
41	Foster St	Contributory

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GREATER GEELONG PLANNING SCHEME

43	Foster St	Contributory
64	Foster St	non-contributory
66	Foster St	Contributory
68	Foster St	Contributory
70	Foster St	Contributory
72	Foster St	Contributory
72	Foster St	Contributory
74	Foster St	Contributory
76	Foster St	Contributory
78	Foster St	Contributory
80	Foster St	Contributory
82	Foster St	Contributory
84	Foster St	Contributory
86	Foster St	Contributory
88	Foster St	Contributory
90	Foster St	non-contributory
92	Foster St	Contributory
4&6	Foster St	Contributory
	Foster St Reserve	Contributory
1/94	Gheringhap St	non contributory
2/94	Gheringhap St	non contributory
3/94	Gheringhap St	non contributory
4/94	Gheringhap St	non contributory
5/94	Gheringhap St	non contributory
96	Gheringhap St	Contributory
98	Gheringhap St	Contributory
100	Gheringhap St	Contributory
104	Gheringhap St	Contributory
126	Gheringhap St	non contributory
128	Gheringhap St	Contributory
130	Gheringhap St	Contributory

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GREATER GEELONG PLANNING SCHEME

132	Gheringhap St	Contributory
134	Gheringhap St	Contributory
136	Gheringhap St	non contributory
138	Gheringhap St	Contributory
1/140	Gheringhap St	non contributory
2/140	Gheringhap St	non contributory
13	Jones Place	non contributory
16	Jones Place	non contributory
1/3	Kilgour Crt	non contributory
2/3	Kilgour Crt	non contributory
7	Kilgour Crt	Contributory
9	Kilgour Crt	Contributory
11	Kilgour St	Contributory
13	Kilgour St	Contributory
15	Kilgour St	non contributory
19	Kilgour St	non contributory
21	Kilgour St	non contributory
23	Kilgour St	Contributory
25	Kilgour St	Contributory
40	Kilgour St	Contributory
42	Kilgour St	Contributory
44	Kilgour St	Contributory
46	Kilgour St	Contributory
48	Kilgour St	Contributory
50	Kilgour St	Contributory
52	Kilgour St	Contributory
54	Kilgour St	Contributory
56	Kilgour St	Contributory
58	Kilgour St	Contributory
60	Kilgour St	Contributory
62	Kilgour St	Contributory

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GREATER GEELONG PLANNING SCHEME

94	Kilgour St	non contributory
95	Kilgour St	Contributory
96	Kilgour St	Contributory
98	Kilgour St	Contributory
99	Kilgour St	non contributory
100	Kilgour St	Contributory
101	Kilgour St	Contributory
102	Kilgour St	non contributory
1/103	Kilgour St	non contributory
2/103	Kilgour St	non contributory
3/103	Kilgour St	non contributory
4/103	Kilgour St	non contributory
5/103	Kilgour St	non contributory
6/103	Kilgour St	non contributory
1/105	Kilgour St	non contributory
2/105	Kilgour St	non contributory
3/105	Kilgour St	non contributory
4/105	Kilgour St	non contributory
108	Kilgour St	Contributory
110	Kilgour St	Contributory
113	Kilgour St	non contributory
115	Kilgour St	non contributory
116	Kilgour St	Contributory
120	Kilgour St	Contributory
124	Kilgour St	non contributory
125	Kilgour St	non contributory
127	Kilgour St	Contributory
128	Kilgour St	non contributory
129	Kilgour St	non contributory
131	Kilgour St	non contributory
135	Kilgour St	non contributory

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137	Kilgour St	Contributory
139	Kilgour St	non contributory
143	Kilgour St	non contributory
144	Kilgour St	non contributory
145	Kilgour St	Contributory
147	Kilgour St	non contributory
148	Kilgour St	Contributory
1/149	Kilgour St	Contributory
2/149	Kilgour St	non contributory
150	Kilgour St	Contributory
151	Kilgour St	Contributory
151	Kilgour St	Contributory
152	Kilgour St	Contributory
153	Kilgour St	Contributory
154	Kilgour St	Contributory
156	Kilgour St	Contributory
158	Kilgour St	Contributory
160	Kilgour St	Contributory
162	Kilgour St	Contributory
164	Kilgour St	Contributory
168	Kilgour St	Contributory
170	Kilgour St	Contributory
172	Kilgour St	Contributory
1/4	Korroit Place	non contributory
2/4	Korroit Place	non contributory
299	Latrobe Terrace	non contributory
315	Latrobe Terrace	non contributory
2	Little Fenwick St	non contributory
4	Little Fenwick St	non contributory
4A	Little Fenwick St	non contributory
6	Little Fenwick St	non contributory

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7	Little Fenwick St	non contributory
8	Little Fenwick St	non contributory
3	Little Maud St	non contributory
6	Little Maud St	Contributory
14	Little Myers St	Contributory
16	Little Myers St	Contributory
17	Little Myers St	Contributory
18	Little Myers St	Contributory
19	Little Myers St	Contributory
21	Little Myers St	Contributory
23	Little Myers St	Contributory
25	Little Myers St	Contributory
27	Little Myers St	Contributory
40	Little Myers St	Contributory
42	Little Myers St	Contributory
44	Little Myers St	Contributory
46	Little Myers St	Contributory
72	Little Myers St	Contributory
74	Little Myers St	Contributory
76	Little Myers St	Contributory
78	Little Myers St	Contributory
80	Little Myers St	Contributory
84	Little Myers St	Contributory
86	Little Myers St	Contributory
88	Little Myers St	Contributory
90	Little Myers St	Contributory
92	Little Myers St	Contributory
2	Lonsdale St	Contributory
6	Lonsdale St	Contributory
8	Lonsdale St	Contributory
10	Lonsdale St	Contributory

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12	Lonsdale St	Contributory
14	Lonsdale St	Contributory
16	Lonsdale St	Contributory
18	Lonsdale St	Contributory
20	Lonsdale St	non contributory
29	Lonsdale St	non contributory
1	Maud St	non contributory
3	Maud St	non contributory
5	Maud St	non contributory
5A	Maud St	Contributory
6	Maud St	non contributory
7	Maud St	Contributory
9	Maud St	Contributory
10	Maud St	non contributory
11	Maud St	Contributory
12	Maud St	non contributory
13	Maud St	Contributory
14	Maud St	Contributory
15	Maud St	Contributory
16	Maud St	non contributory
17	Maud St	Contributory
18	Maud St	Contributory
19	Maud St	Contributory
20	Maud St	Contributory
21	Maud St	non contributory
22	Maud St	Contributory
23	Maud St	Contributory
24	Maud St	Contributory
25	Maud St	Contributory
26	Maud St	Contributory
27	Maud St	Contributory

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GREATER GEELONG PLANNING SCHEME

29	Maud St	Contributory
30	Maud St	Contributory
31	Maud St	non contributory
32	Maud St	Contributory
59	Maud St	Contributory
61	Maud St	Contributory
63	Maud St	Contributory
65	Maud St	non contributory
67	Maud St	non contributory
69	Maud St	non contributory
70	Maud St	Contributory
74	Maud St	Contributory
76	Maud St	Contributory
78	Maud St	Contributory
80	Maud St	Contributory
82	Maud St	Contributory
84	Maud St	Contributory
85	Maud St	Contributory
86	Maud St	Contributory
88	Maud St	Contributory
89	Maud St	non contributory
90	Maud St	Contributory
93	Maud St	Contributory
95	Maud St	Contributory
97	Maud St	Contributory
99	Maud St	Contributory
101	Maud St	Contributory
102	Maud St	Contributory
103	Maud St	Contributory
104	Maud St	Contributory
105	Maud St	Contributory

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GREATER GEELONG PLANNING SCHEME

106	Maud St	Contributory
107	Maud St	Contributory
1/107	Maud St	Contributory
108	Maud St	Contributory
109	Maud St	Contributory
110	Maud St	non contributory
111	Maud St	Contributory
113	Maud St	Contributory
114-116	Maud St	non contributory
115	Maud St	Contributory
117	Maud St	non contributory
122	Maud St	Contributory
128	Maud St	Contributory
132	Maud St	Contributory
134	Maud St	Contributory
136	Maud St	Contributory
138	Maud St	Contributory
140	Maud St	Contributory
142	Maud St	Contributory
146	Maud St	Contributory
148	Maud St	Contributory
150	Maud St	Contributory
152	Maud St	Contributory
1/158	Maud St	Contributory
2/158	Maud St	Contributory
9	McKenzie St	non contributory
11	McKenzie St	non contributory
3	McKillop St	Local
5	McKillop St	Contributory
6	McKillop St	Contributory
7	McKillop St	Contributory

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GREATER GEELONG PLANNING SCHEME

8	McKillop St	Contributory
10	McKillop St	Contributory
11	McKillop St	Contributory
12	McKillop St	Contributory
13	McKillop St	Contributory
14	McKillop St	Contributory
15	McKillop St	Contributory
16	McKillop St	Contributory
17	McKillop St	Contributory
18	McKillop St	Contributory
19	McKillop St	Contributory
20	McKillop St	Contributory
22	McKillop St	Contributory
24	McKillop St	Contributory
26	McKillop St	Contributory
28	McKillop St	Contributory
30	McKillop St	Contributory
32	McKillop St	non contributory
33-39	McKillop St	Contributory
45	McKillop St	Contributory
49	McKillop St	Contributory
66	McKillop St	non contributory
1/66	McKillop St	non contributory
76	McKillop St	non contributory
78	McKillop St	non contributory
82	McKillop St	non contributory
91A	McKillop St	non contributory
91	McKillop St	non contributory
93	McKillop St	Contributory
95	McKillop St	Contributory
1/97	McKillop St	non contributory

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GREATER GEELONG PLANNING SCHEME

2/97	McKillop St	non contributory
3/97	McKillop St	non contributory
4/97	McKillop St	non contributory
99	McKillop St	non contributory
101	McKillop St	Contributory
103	McKillop St	Contributory
105	McKillop St	non contributory
107	McKillop St	Contributory
109	McKillop St	Contributory
111	McKillop St	non contributory
112	McKillop St	Contributory
113	McKillop St	Contributory
114	McKillop St	Contributory
115	McKillop St	Contributory
116	McKillop St	Contributory
118	McKillop St	Contributory
120	McKillop St	Contributory
122	McKillop St	Contributory
126	McKillop St	non contributory
130	McKillop St	Contributory
132	McKillop St	Contributory
134	McKillop St	non contributory
138	McKillop St	non contributory
140	McKillop St	non contributory
144	McKillop St	non contributory
152	McKillop St	non contributory
303	Moorabool St	Contributory
305	Moorabool St	Contributory
307	Moorabool St	Contributory
309	Moorabool St	Contributory
311	Moorabool St	non contributory

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GREATER GEELONG PLANNING SCHEME

1/14	Mundy St	non contributory
2/14	Mundy St	non contributory
3/14	Mundy St	non contributory
5	Pardon Place	non contributory
7	Pardon Place	non contributory
9	Pardon Place	non contributory
11	Pardon Place	non contributory
2	School St	Contributory
4	School St	Contributory
6	School St	Contributory
1	Stringers Lane	non contributory
6	Stringers Lane	non contributory
8	Stringers Lane	non contributory
2	Sutherland St	non contributory
4	Sutherland St	Contributory
6	Sutherland St	Contributory
8	Sutherland St	Contributory
4	Suttons Lane	non contributory
78	Swanston St	Contributory
80	Swanston St	Contributory
82	Swanston St	Contributory
88	Swanston St	non contributory
90	Swanston St	non contributory
1/93	Swanston St	non contributory
2/93	Swanston St	non contributory
3/93	Swanston St	non contributory
4/93	Swanston St	non contributory
5/93	Swanston St	non contributory
6/93	Swanston St	non contributory
97	Swanston St	non contributory
98	Swanston St	non contributory

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GREATER GEELONG PLANNING SCHEME

99	Swanston St	Contributory
100	Swanston St	Contributory
101	Swanston St	non contributory
102	Swanston St	Contributory
103	Swanston St	non contributory
105	Swanston St	non contributory
106	Swanston St	Contributory
108	Swanston St	Contributory
109	Swanston St	non contributory
111	Swanston St	non contributory
111C	Swanston St	non contributory
116	Swanston St	non contributory
131	Swanston St	Contributory
133	Swanston St	Contributory
135	Swanston St	Contributory
137	Swanston St	Contributory
139	Swanston St	Contributory
141	Swanston St	Contributory
143	Swanston St	Contributory
145	Swanston St	Contributory
147	Swanston St	Contributory
149	Swanston St	Contributory
151	Swanston St	Contributory
153	Swanston St	Contributory
155	Swanston St	Contributory
157	Swanston St	Contributory
159	Swanston St	Contributory
161	Swanston St	non-contributory
162	Swanston St	Contributory
163	Swanston St	Contributory
164	Swanston St	Contributory

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GREATER GEELONG PLANNING SCHEME

165	Swanston St	Contributory
167	Swanston St	Contributory
169	Swanston St	Contributory
170	Swanston St	Contributory
171	Swanston St	Contributory
173	Swanston St	Contributory
174	Swanston St	Contributory
175	Swanston St	Contributory
177	Swanston St	Contributory
178	Swanston St	non-contributory
179	Swanston St	Contributory
181	Swanston St	Contributory
184	Swanston St	Contributory
185	Swanston St	non-contributory
185	Swanston St	non-contributory
187	Swanston St	Contributory
188	Swanston St	non-contributory
189	Swanston St	Contributory
190	Swanston St	Contributory
191	Swanston St	non-contributory
193	Swanston St	Contributory
195	Swanston St	Contributory
197	Swanston St	Contributory
199	Swanston St	Contributory
3	Verner St	Contributory
5	Verner St	Contributory
6	Verner St	non contributory
1/7	Verner St	non contributory
2/7	Verner St	non contributory
3/7	Verner St	non contributory
4/7	Verner St	non contributory

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GREATER GEELONG PLANNING SCHEME

8	Verner St	non contributory
9	Verner St	Contributory
11	Verner St	non contributory
13	Verner St	Contributory
14	Verner St	Contributory
15	Verner St	Contributory
16	Verner St	Contributory
17	Verner St	Contributory
18	Verner St	Contributory
19	Verner St	Contributory
20	Verner St	Contributory
22	Verner St	Contributory
24	Verner St	Contributory
112	Verner St	non-contributory
114	Verner St	non-contributory
117	Verner St	Contributory
119	Verner St	Contributory
120	Verner St	Contributory
121	Verner St	Contributory
122	Verner St	Contributory
123	Verner St	Contributory
124	Verner St	Contributory
125	Verner St	non-contributory
126	Verner St	Contributory
127	Verner St	Contributory
128	Verner St	Contributory
129	Verner St	Contributory
130	Verner St	Contributory
131	Verner St	Contributory
132	Verner St	Contributory
133	Verner St	Contributory

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GREATER GEELONG PLANNING SCHEME

134	Verner St	Contributory
135	Verner St	Contributory
137	Verner St	Contributory
139	Verner St	Contributory
141	Verner St	Contributory
159	Yarra St	Contributory
169	Yarra St	non contributory
170	Yarra St	Contributory
172	Yarra St	Contributory
1/173	Yarra St	Contributory
2/173	Yarra St	Contributory
3/173	Yarra St	Contributory
174	Yarra St	Contributory
175	Yarra St	non contributory
176	Yarra St	Contributory
177	Yarra St	non contributory
178	Yarra St	Contributory
179	Yarra St	Contributory
180	Yarra St	Contributory
182	Yarra St	Contributory
183	Yarra St	Contributory
184	Yarra St	Contributory
185	Yarra St	Contributory
186	Yarra St	Contributory
187	Yarra St	Contributory
189	Yarra St	non contributory
191	Yarra St	non contributory
193	Yarra St	non contributory
197	Yarra St	Contributory
199	Yarra St	Contributory
201	Yarra St	Contributory

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GREATER GEELONG PLANNING SCHEME

203	Yarra St	Contributory
205	Yarra St	Contributory
207	Yarra St	Contributory
211	Yarra St	Contributory
213	Yarra St	Contributory
220	Yarra St	Contributory
222	Yarra St	Contributory
224	Yarra St	Contributory
226	Yarra St	Contributory
228	Yarra St	Contributory
229	Yarra St	Contributory
230	Yarra St	Contributory
231	Yarra St	Contributory
232	Yarra St	Contributory
233	Yarra St	Contributory
234	Yarra St	Contributory
235	Yarra St	Contributory
238	Yarra St	Contributory
238	Yarra St	Contributory
239	Yarra St	Contributory
241	Yarra St	Contributory
242	Yarra St	Contributory
246	Yarra St	Contributory
247	Yarra St	non contributory
248	Yarra St	Contributory
249	Yarra St	non contributory
254	Yarra St	Contributory
255	Yarra St	non contributory
256	Yarra St	Contributory
1/263	Yarra St	non contributory
2/263	Yarra St	non contributory

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GREATER GEELONG PLANNING SCHEME

3/263	Yarra St	non-contributory
4/263	Yarra St	non-contributory
5/263	Yarra St	non-contributory
6/263	Yarra St	non-contributory

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# City South Heritage Area Heritage Design Guidelines

City of Greater Geelong, July 2023

<b>Heritage Place:</b>	City South Heritage Area	<b>PS ref no:</b>	HO1641
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## Demolition or removal

Retain existing and intact significant places that are of moderate and major contributory value.

## Landscape and Subdivision Character

Promote conservation of the landscape and subdivision character along primary and secondary streets:

- Encourage ongoing evolution of the existing roadways to accommodate ongoing vehicle, cycle, pedestrian, etc, traffic needs and public safety.
- Encourage street tree planting along nature strips or along central medians.
- Encourage nature strips to be grassed or otherwise treated with soft landscaping. Hard paving may be permitted to facilitate mixed use activities at former or existing corner stores but should otherwise be discouraged.
- Encourage front boundary fencing to dwellings, of an appropriate height and transparent or semi-transparent character, so as to retain visual connection between front gardens and the street.
- Encourage retention of front setbacks consistent with the contributory historical building median on that street, and garden areas between dwellings and front property boundaries.
- Retain the appearance of the historical allotment configuration in that street, typically being a detached dwelling extending across most, but not all, of each lot frontage.
- Encourage separation between neighbouring buildings consistent with the contributory historical building median on that street; discourage buildings that are constructed side boundary to side boundary.
- Encourage vehicle access to be off service lanes and minor streets when these are available. Where verge crossings have to be installed they should be kept to a single vehicle width.
- Discourage double width verge crossings and minimise hard paved areas between the street building frontages and the street; encourage any necessary hardstand for parking or the like to occur at the rear or sides of the building.
- Consolidation or subdivision of the front halves of lots, typically incorporating the original houses, should be discouraged. Consolidation or subdivision of the rear halves of lots may be considered, on the proviso subdivision will not cause new vehicle accommodation and verge crossings to be constructed on the street frontage.

## Bulk, form and appearance

Promote conservation and appropriate adaptive reuse of the contributory historical dwellings within the area:

- Encourage existing Victorian, Edwardian, and Interwar Bungalow houses to be retained with appropriate adaptation to accommodate ongoing use.
- Encourage retention of significant character attributes and detailing, especially to exterior elements visible from the public domain.
- Discourage additions or alterations to the street frontage other than works that are restoring original details or returning the building to a form closer to its original appearance.

- Encourage additions or alterations that are reasonably visible from the public domain, excluding from service lanes of minor streets, to utilise a roof form and eave height, materials palette, architectural style and detailing, that complements the original style of the building, and which retains the original building's street frontage as the dominant visible feature of the property.
- For additions and alterations to the rear of the original building and new buildings within the rear part of the property that are not reasonably visible from the public domain or are visible only from service lanes or minor streets, high-quality modern architectural design adaptations that complement the original style may be incorporated.
- Encourage garages or carports to be provided at the rear, otherwise at the side but setback from the front facade, of any building. Any garage doors visible from the street frontage should be single width. Carport design may either match the joinery style and form of the original building or may pursue a high-quality minimalist design with a thin fascia-less or floating roof plane that minimises visual impact on the setting of the house.
- Discourage painting of exterior surfaces that were originally unpainted, eg face brick.
- Encourage use of colour schemes on painted surfaces that are appropriate to the stylistic period of the house.

Promote conservation of the wider historical architectural character of the area through incorporating appropriate design characteristics within new infill development on the front half of the original lot:

- Present to the main street as a detached, single storey buildings, whilst allowing two storey additions or separate buildings upon the rear half of lots.
- Present single fronted, narrow facades or double fronted large facades to the main streets.
- Roof forms should complement the nearest contributory historical buildings, typically of a hipped, gabled or mixed form, with a pitch between 20-35 degrees.
- Highest point of the roof and the eave height or the equivalent roof pitching/springing height should complement those of the nearest contributory historical buildings.
- The main street façade should incorporate an entry that addresses the street within either a full width roofed verandah, or a covered or recessed porch.
- Windows should be of a rectangular vertical format (ie height substantially exceeds width) but can be grouped to form a horizontal bank.
- Wall cladding should complement contributory historical buildings within the neighbouring block, typically horizontal timber weatherboards or rendered masonry, with face brick in limited areas.
- Roof cladding should be corrugated sheet metal in a galvanised or bonded colour finish, avoiding dark colours and bare zincalume.
- For new infill building development that is not within the front half of a lot, there may be discretion as to side and rear setbacks, form and style provided the height does not exceed two stories and the form and massing does not detract from the character of the historical contributory buildings visible at the main street frontage.

### **Primary source**

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*Geelong Region Historic Buildings and Objects Study Volume 2 (Allan Willingham, 1986)*

*Geelong City Urban Conservation Study, Volume 1 (Graeme Butler, 1993)*

*Geelong City Urban Conservation Study, Volumes 2-5 (Graeme Butler, 1991)*

*Geelong City Urban Conservation Study, Volume 4(a) (Helen Lardner, 1995)*

*City of Greater Geelong Heritage and Design Guidelines (Helen Lardner, 1997)*

*City Fringe Heritage Area Review (RBA Architects + Conservation Consultants, 2016)*

*City of Greater Geelong, South Geelong UDF Heritage Review, November 2023*

*~~South Geelong Urban Design Framework – Heritage Character Study (Authentic Heritage Services, June 2020)~~*

*~~South Geelong Urban Design Framework – Proposed Revision to HO1641 City South Heritage Area (City of Greater Geelong, October 2021)~~*

This document is an incorporated document in the Greater Geelong Planning Scheme pursuant to section 6(2)(j) of  
the  
*Planning and Environment Act 1987*

# 'Kelvendon' - Statement of Significance

City of Greater Geelong, 2023

<b>Heritage Place:</b>	House, 'Kelvendon'	<b>PS ref no:</b>	HO2023
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## What is significant?

'Kelvendon' at 332 Moorabool Street, Geelong is a single storey, hipped roofed, brick dwelling in the Late Victorian style, built in 1887 and extended in 1901. The significant features of the place are:

- The original 1887 8-room house including polychromatic brickwork and chimneys, double-hung timber windows, centrally located entry door with side and high lights, hipped roof form, and cast-iron verandah columns.
- The 1901 house addition to the south-west including polychromatic brickwork and double-hung timber windows.

Features that are not of significance include all outbuildings and fences.

## How is it significant?

'Kelvendon' is of historical significance (Criterion A), and associative significance (Criterion G & H) to the City of Greater Geelong.

## Why is it significant?

'Kelvendon' is an example of housing commissioned and lived in by the wealthy and aspirational trade sector from the mid to late nineteenth century. 'Kelvendon' is significant for its ability to demonstrate the effects of the land boom which saw the growth of the trade and commercial sectors and rise of speculators, architects, and builders in Geelong. (Criterion A).

'Kelvendon' is significant as an example of second-generation migrant wealth achieved through the trade sector by its owner, Solomon Jacobs. The place remains substantially unchanged since his occupation thereby providing an example of Jacobs' original residence and living conditions. Its connection to mid-nineteenth century migration of Eastern Europeans to Victoria and the history of the Jewish community in Geelong is significant. (Criterion G).

'Kelvendon' is significant for its association with draper and Mayor of Geelong, Solomon Jacobs, the son of one of the earliest Jewish families to settle in Geelong. It is also associated with architect A. J. Derrick, plumbers Edwin and Frank Drew, and philanthropist Mary Cheetham. (Criterion H).

## Primary sources

[Bantow, J., \*Stage 3 Submission, South Geelong Interim Final Urban Design Framework\*, 10 April 2022](#)

[City of Greater Geelong, \*South Geelong UDF Heritage Review\*, November 2023](#)

[Rowe, D., \*About Corayo: A Thematic History of Geelong\*, City of Greater Geelong, 2021](#)

[Ecology & Heritage Partners Pty Ltd, \*South Geelong Heritage Precinct HO1641, Peer Review\*, August 2022.](#)

# Kardinia Villa - Statement of Significance

City of Greater Geelong, 2023

<b>Heritage Place:</b>	Kardinia Villa	<b>PS ref no:</b>	HO2024
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## What is significant?

'Kardinia Villa' at 344 Moorabool Street, Geelong is a single storey, hipped roofed, brick dwelling in the Late Victorian style, built in 1883 and extended in the early 20th Century. The significant features of the place are:

- The original brick house including tuck pointed façade with bi-chromatic brickwork, bluestone foundations, double hung windows either side of an elaborate four-panelled door with high lights, sandstone windowsills, verandah with timber balustrade and decorative ironwork in the 'Rinceau' pattern, hipped roof form, and three rendered brick chimneys.
- The weatherboard addition to the west including hipped and skillion roof forms.

A garage with access from Carr Street dates from the 2007-08 restoration and is not significant.

## How is it significant?

'Kardinia Villa' is of historical significance (Criterion A), aesthetic significance (Criterion E), and associative significance (Criterion H) to the City of Greater Geelong.

## Why is it significant?

'Kardinia Villa' is an example of the type of housing commissioned by the wealthy and aspirational trade sector from the mid to late nineteenth century. 'Kardinia Villa' is significant for its ability to demonstrate the effects of the land boom which saw the growth of the trade and commercial sectors and rise of speculators, architects, and builders in Geelong. (Criterion A)

'Kardinia Villa' is significant for its associations with ironmonger Thomas Baldock, and prolific Geelong architect, Joseph Watts. (Criterion H)

'Kardinia Villa' is of aesthetic significance. The place is an example of a Late Victorian residence that incorporates distinctive bi-chromatic brickwork, decorative ironwork in the 'Rinceau' pattern, Barrabool Stone windowsills and a geometric veranda balustrade. (Criterion E)

## Primary sources

[Bantow, J., \*Stage 3 Submission, South Geelong Interim Final Urban Design Framework\*, 10 April 2022](#)

[City of Greater Geelong, \*South Geelong UDF Heritage Review\*, November 2023](#)

[Rowe, D., \*About Corayo: A Thematic History of Geelong\*, City of Greater Geelong, 2021](#)

[Ecology & Heritage Partners Pty Ltd, \*South Geelong Heritage Precinct HO1641, Peer Review\*, August 2022-](#)

# 'Penlea' - Statement of Significance

City of Greater Geelong, 2023

<b>Heritage Place:</b>	House, 'Penlea'	<b>PS ref no:</b>	HO2025
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## What is significant?

'Penlea' at 343 Moorabool St Geelong is a single storey weatherboard dwelling in the Federation style incorporating Arts and Crafts influences, built in 1912-13.

The significant features of the place are:

- The original weatherboard house form including steeply pitched hipped roof form with front gable and contiguous verandah roof
- The original house decorative elements including four tall, red brick chimneys with bands of rough cast render, projecting timber-framed bay window with original stained-glass, other timber-framed windows, other original stained-glass glazing, and verandah turned timber posts, eave brackets and fretwork.

Features that are not significant are modern metal-framed windows, metal window safety bars and timber access ramp; the modern metal shed at the rear of the property, and rough stone fence border and low timber fence.

## How is it significant?

'Penlea' is of historical significance (Criterion A), aesthetic significance (Criterion E), and associative significance (Criterion H) to the City of Greater Geelong.

## Why is it significant?

'Penlea' is historically significant as the earliest documented domestic house designed by notable Geelong-born architect Percy Everett. (Criterion A)

'Penlea' is significant for its association with architect Percy Everett who was the first architectural student at Gordon Institute of Technology and became the Chief Architect of the Victorian Public Works Department. (Criterion H)

'Penlea' is of aesthetic significance as an intact example of a Federation Arts and Crafts house which has retained its original footprint and architectural features. (Criterion E)

## Primary sources

[Bantow, J., \*Stage 3 Submission, South Geelong Interim Final Urban Design Framework\*, 10 April 2022](#)

[City of Greater Geelong, \*South Geelong UDF Heritage Review\*, November 2023](#)

[Rowe, D., \*About Corayo: A Thematic History of Geelong\*, City of Greater Geelong, 2021](#)

[Ecology & Heritage Partners Pty Ltd, \*South Geelong Heritage Precinct HO1641, Peer Review\*, August 2022.](#)