

MINUTES

COUNCIL MEETING

Tuesday 25 October 2022
6:00 pm

City Hall
57 Little Malop Street, Geelong 3220

LIVE STREAMED ON THE CITY'S WEBSITE:

www.geelongaustralia.com.au/meetings

COUNCIL:

Cr P Murrihy (Brownbill Ward) - Mayor
Cr T Sullivan (Bellarine Ward) - Deputy Mayor
Cr S Asher (Bellarine Ward)
Cr J Mason (Bellarine Ward)
Cr E Kontelj (Brownbill Ward)
Cr S Mansfield (Brownbill Ward)
Cr B Harwood (Kardinia Ward)
Cr B Moloney (Kardinia Ward)
Cr R Nelson (Kardinia Ward)
Cr A Aitken (Windermere Ward)
Cr K Grzybek (Windermere Ward)

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Present: Crs P Murrhiy (Mayor), A Aitken, S Asher, K Grzybek, B Harwood, J Mason, B Moloney, R Nelson, T Sullivan

Also Present: K Phyland (Acting Chief Executive Officer), G Smith (Director City Planning and Economy), Guy Wilson-Browne (Director City Services), R Stevens (Director Community Life), B Prosser (Acting Director Customer & Corporate Services), S McKew (Manager Governance)

1. PROCEDURAL MATTERS

1.1. Acknowledgement of Country

Council acknowledges the Wadawurrung People as the Traditional Owners of the Land, Waterways and Skies. We pay our respects to their Elders, past and present. We acknowledge all Aboriginal and Torres Strait Islander people who are part of our Greater Geelong community today.

1.2. Apologies

Crs S Mansfield and E Kontelj (Leave of Absence)

1.3. Leaves of Absence

Cr Aitken moved, Cr Sullivan seconded -

That Leave of Absence be granted to Cr B Harwood for the period 16 November to 28 November, inclusive.

Carried

Cr Harwood moved, Cr Mason seconded -

That Leave of Absence be granted to Cr S Mansfield for the period 10 November to 12 December, inclusive.

Carried

1.4. Declarations of Conflicts of Interest

Nil

1.5. Confirmation of Minutes

Cr Harwood moved, Cr Grzybek seconded –

That the Minutes of the Council Meeting held on 27 September 2022 be confirmed.

Carried

Cr Asher moved, Cr Mason seconded

That the Minutes of the Council Meeting held on 12 October 2022 be confirmed.

Carried

1.6. Public Question and Submission Time

The following persons submitted questions prior to the Council Meeting on the following various subjects:

1. Kevin Krastins – Geelong Grand Final Celebrations, Support for Campaspe Shire Flood
2. Mary Ramia – Tree Removal – South Geelong Rail Corridor
3. Graham Hobbs – Highton UDF
4. Matt Goulter – Highton UDF
5. Matthew Portbury – Highton UDF
6. Jeff McFarlane – Tree Removal – South Geelong Rail Corridor
7. Erin Heer – Highton UDF
8. Katerina Izdebskaia – Urban Forest Strategy
9. Bill Marshall – Urban Forest Strategy
10. Tina Smallman – Highton UDF
11. Sally Kirner – Pakington Street UDF
12. Andrew Katos – Highton UDF
13. Jennifer Bantow – Marshall Bluestone Cottage
14. Angela Mangan – Pakington Street UDF
15. Caitlin Kirby – Pakington Street UDF
16. Claude Eagles – Highton UDF
17. Dave Speirs – Highton UDF
18. Jessica Sullivan – Pakington Street UDF
19. Simon Nardi – Highton UDF
20. Prue Beck – Highton UDF
21. Pati Seiler – Pakington UDF
22. Brendan John Quirk – Pakington Street UDF
23. Katherine Talbot – Dean Street
24. Dr Jane Mooney – Pakington Street UDF

Kevin Krastins:

Question 1:

I'd like to thank Councillors in particular Cr Harwood, the Mayor, Cr Aitken, Cr Nelson, Cr Mansfield, Cr Kontelj, Cr Mason and other Councillors who strongly advocated and supported the Geelong Cats live site @ St Mary's oval in 2022. Well done and a big thank you. I trust there is a framework to support the live sites when Geelong makes a future grand final.

Thank you for your submission, Kevin. We are pleased you enjoyed the game.

Question 2:

Could the Mayor/CEO please write/phone the Mayor/Council of Campaspe in Echuca at the appropriate time passing on our Geelong support and strength to all the residents/Council hoping they can get through the massive flood crisis that is evolving.

Thank you for your suggestion that we reach out to the Shire of Campaspe with a message of support to their communities as the flood crisis emerges. We acknowledge the impact of the flooding emergency on the many communities across Victoria. In December 2020, the Municipal Association of Victoria (MAV) invited councils to sign up to the MAV Emergency Management Sharing Protocol. The City of Greater Geelong is one of 69 councils which has signed on to the sharing protocol. The sharing protocol sets out an agreed position between councils regarding the provision of resources and support to assist other municipalities with response and recovery tasks during and after emergencies. The City will contribute to the sharing protocol as required, should it be activated.

Mary Ramia:

The Council have let me know that basically under the Transport act there is nothing they can do to stop any works from going ahead along the rail corridor in South Geelong.

Question 1:

I would like to know if the land that currently belongs to the Council along Strong street will remain a Council reserve or will it be given to Vic Track?

Thank you for your questions, Mary.

The City owned land adjacent to the VicTrack boundary along Strong Street will be returned to the City after the Rail Project Victoria (RPV) project is complete.

Question 2:

If the trees have to come out what is the plan for remediation after the works have been completed and when will we actually see the plans in writing?

RPV has assured us it understands that mature trees are a really important feature of the area around South Geelong and that every effort has been made to preserve as many trees as possible through the design of the project.

RPV is working closely with the City, Barwon Water, VLine and VicTrack on replanting and landscaping plans for the area. The City will continue to advocate for the best outcome for the community regarding tree removal and replacement.

RPV will be replanting more trees than what is being removed and will work with the City to identify other locations for additional trees. RPV will share landscaping plans with the community in coming months.

Question 3:

Why was the community kept in the dark on the extent of the tree removal along this part of Geelong was it done deliberately to avoid the community being upset and protesting?

RPV has advised that it has undertaken community consultation during planning and design of the project over the past 12 months, as well as notifying the South Geelong community about upcoming works and vegetation removal in their area.

During August and September, door knocks, community information sessions and one-on-one meetings occurred with residents across South Geelong and Breakwater to support the release of updated designs for South Geelong Station and Fyans Street level crossing removal and to discuss construction impacts including tree removal with residents and the community.

RPV will continue to inform residents about upcoming tree and vegetation removal occurring in November and December. RPV is door knocking residents in areas where tree removal is occurring and encourages anyone who wants further information to get in contact with the project team.

RPV will continue the process of consulting the community on urban design, landscaping and creative opportunities for the project, with more detailed information about revegetation and landscaping plans to be shared with the community over the coming months.

Graham Hobbs

Question 1:

Having read the petition of four thousand plus users, ratepayers, residents, traders and business owners and their clients, plus the enclosed Geelong News article of December 1996 as to the community and Rotary Highton Clocktower, and recognizing the overwhelming current community wishes, will the Council dismiss the current October 2022 Highton Urban design proposals, (as amended 5 times since 2016), save for upgrading the surfaces and levels, traffic signages and Bellevue Arcade (as per my petition and detailed submissions of 2017 and 2021, correspondence and several invitations to Councillors and Council's urban designers), to finalise this project now with minimal disruption to the unique, very functional and boutique Highton Village and to stop wasting the precious resources of the ratepayers of our Geelong community.

In summary, will the City of Greater Geelong Council (as our elected representatives) be smart, bold, listen and act in accordance with the community's current and overwhelming express and written directions.

Thank you for your questions, Graham.

The matter is listed for a decision tonight. The community submissions form an important part of decision making in planning projects like this one.

Jeff McFarlane (asked by Mary Ramia)

Question 1:

Are Councillors aware that for no good reason the regional rail project intends felling street trees along Carr Street and Strong Street in South Geelong, including a Banksia Marginata on Strong Street, which is a nesting site for a pair of New Holland Honey eaters?

Thank you for your question, Jeff.

The City has raised your concerns regarding tree removal with Rail Projects Victoria (RPV) and it acknowledges that this extent of tree removal is difficult for the community, but this is an important project for the region that will deliver more frequent and reliable services and better stations for passengers in growth areas south of Geelong.

RPV has also noted that further trees and vegetation will need to be removed in or near the rail corridor because they are directly in the path of where new infrastructure will be located, or so close to works that the tree root structure will be damaged to the extent that it causes a safety risk.

RPV ensures qualified wildlife ecologists are onsite to inspect all trees and vegetation prior to removal and handlers will safely relocate any wildlife to a suitable habitat nearby.

Erin Heer

Question 1:

How can the COGG justify the expense (which is anticipated to be hundreds of thousands of dollars, if not millions), to drastically change Highton Village - when the overwhelming community feedback is opposed to the Council's UDF. We have over 4500 signatures on a petition that supports pavement upgrade and signage improvements, and the upgrade of the village walk. The community is strongly opposed to disruption to traffic flow and super tall , 3-4 storey buildings. How can this feedback not be taken on board?

Thank you for your questions, Erin.

Funding for the refurbishment of Belle Vue Arcade and Village Walk was approved by Council in the last financial year. All other works are subject to future budget bids or other government grants.

Proposed adjustments to traffic flow

The proposed changes to the flow of traffic throughout the village have been based on the recommendations of three independent traffic reports, which identified a range of road safety issues. Several traffic flow arrangements were independently tested by consultants, who proposed a layout that encourages slower vehicle speed, pedestrian and vehicle safety, ease of movement for both pedestrians and vehicles, has limited impact on travel times for all road users and improves public realm and amenity.

3-4 storey buildings

The proposed three storey height limits are less than what is possible under the current planning controls for Highton Village. They are marginally higher than the current tallest building on Belle Vue Avenue at 9m. The proposed controls include a two-storey street wall with setbacks at the third level. The proposed four storey height limits interface with the reserve and include a two-storey street wall and setbacks at the upper two levels.

Question 2:

The COGG say the intention is to protect the village, as there are no height restrictions currently. Why not limit development to 2 storey?

The centre is beginning to come under some development pressure as can be seen by the change in land use on adjacent Roslyn and Barrabool roads. A three-storey building has also already been approved under current policy where the Commonwealth Bank is located.

Implementing design guidelines through the UDF allows for redevelopment and renewal of the village and provides protection from inappropriate development by introducing building height and setback guidelines.

Question 3:

The Library has been given a 5 year protection- but the community want certainty.

Will the council give surety to the people of Highton, and give the Library 20 years?

Stop with the long term plans to further develop the COGG carpark and the Woolworths site. The UDF seems designed to benefit large corporate agenda and has forgotten the community wants.

In line with the Geelong Regional Library Corporation Infrastructure Development Plan and the City's Social Infrastructure Plan, the Highton Library will be retained in its current location in the short-medium term. A review is proposed to be undertaken in five years' time to determine the future options for the library.

Katerina Izdebskaia

Question 1:

The Data Exchange Overview page

(<https://www.geelongdataexchange.com.au/pages/urban-forestry/#overview>) quotes the 2015 Urban Forest Strategy, highlighting that the Strategy 'is focused on enhancing the public and private tree population in the suburbs and townships across Greater Geelong' and that 'the sum of all these trees and associated vegetation is called the urban forest'.

(P 5) Moreover, the Urban Forest Strategy specifically recognises the importance of the private realm and of all open space in Geelong in contributing to the urban forest. (P 7) The CoGG Urban Forest Data Exchange Detailed View page

(<https://www.geelongdataexchange.com.au/pages/urban-forestry/#detailed-view>) reflects the 'Net tree gains over time' since 2015

Can the Council confirm whether the 'net tree gains over time' information on the City web page includes trees on private and on public land?

Thank you for your questions, Katerina.

The 'net tree gains over time' information on the Urban Forest page on the Geelong Data Exchange only reflects changes to trees on public land.

Question 2:

The CoGG Urban Forest Data Exchange overview page

(<https://www.geelongdataexchange.com.au/pages/urban-forestry/#overview>) reports the 'total number of trees identified'; the 'number of tree species identified'; and the 'number of trees planted specifically in 2022'. However, the page does not contain information on tree loss and or removal. Such information is relevant to assist in ascertaining the changes in canopy cover and our progress from 14% cover in 2015.

Can the Council please indicate where residents can access information on the number of trees removed?

The information on the number of public trees removed and the changes in the City's tree population can be found on the 'Detailed View' tab of the Urban Forest page on the Geelong Data Exchange. The annual removal numbers are shown in the Net Gain data. The link is: <https://www.geelongdataexchange.com.au/pages/urban-forestry/overview#detailed-view>

Question 3:

Can the Council please indicate where residents can access information on tree loss in urban Geelong?

Please refer to the previous answer.

Bill Marshall

During the August Council meeting, City Services responded that One of the actions in the Urban Forest Strategy Action Plan is to measure all Key Performance Indicators in 2020 and report progress. The progress report is published on the Urban Forest Strategy – Action Plan page of our website. However, the Urban Forest Strategy - Action Plan page (<https://www.geelongaustralia.com.au/urbanforest/article/item/8d3022262ef1ca1.aspx>) on the CoGG website lists the Key Performance Indicators but does not contain a 2020 or any KPI assessment and/or progress report The Urban Forest (Overview and Detailed View) sections on the Geelong Data Exchange (<https://www.geelongdataexchange.com.au/pages/urban-forestry/#detailed-view>) contains helpful information thank you. However, the information does not respond to the actions and KPIs of the Action Plan. For example, the site contains undated data about the number of trees identified, but it does not reflect the City's canopy cover status. It also contains information about age, species and ULE, but it does not address progress on appropriateness and diversity.

Can the Council provide information as to where residents can access the complete KPI assessment and progress report?

Thank you for your questions, Bill.

The 2020 update on key performance indicators is contained in the 'Related Documents' section on the Urban Forest Strategy - Action Plan page of our website. The link is: <https://www.geelongaustralia.com.au/urbanforest/article/item/8d3022262ef1ca1.aspx>

Question 2:

The overview introduction on the CoGG Urban Forest Data Exchange page (<https://www.geelongdataexchange.com.au/pages/urban-forestry/#overview>) states the following:

The City's Urban Forest Strategy is focused on enhancing the public and private tree population in the suburbs and townships across Greater Geelong. The sum of all these trees and associated vegetation is called the urban forest. Under the heading 'How is our urban forest changing?', the page reflects the 'total number of trees identified'; the 'number of tree species identified'; and the 'number of trees planted specifically in 2022'.

Can the Council confirm whether the 'total number of trees identified' reflects trees on public and on private land??

References to numbers of trees planted or removed relate to trees on public land. The City does not have a mechanism to count the numbers of trees planted or removed on private property.

Question 3:

Can the Council confirm whether the 'number of trees planted specifically in 2022' reflects trees on public and on private land?

As per the previous answer, the number of trees planted refers to trees on public land.

Tina Smallman

The Highton Village Urban Design Framework is unacceptable as is. Proposed 3 and 4 storey height limits are absolutely rejected by our community on the grounds that they will detract from the Village's beauty and ambience, close off the sky, overshadow the trees and shoppers and lead our much-loved community hub along the path to becoming a generic 'run of the mill' shopping centre.

Do you understand that Highton Village is representative of the charm, uniqueness, history and liveability that has drawn people to Geelong recently and that the proposed height increases will ultimately detract from this success?

Thank you for your questions, Tina.

The UDF introduces design guidelines to protect the village character from inappropriate development by introducing building height guidelines where none exist now.

A mixture of development, including some 3-4 storey development, will maintain a village feel, locating jobs, shops, cafes and community services near each other, providing access to everyday needs and creating active, social places that people choose to spend time in.

Question 2:

The Highton Village UDF is unacceptable as is. We have something unique in Highton, something different, something people friendly, something that works, something that 'ain't broke' and doesn't need 'fixing', apart from a little TLC. The charm, quirkiness and history of our little clock tower is a huge part of the attraction and ambience of the Village. Surely Geelong Council loves and values it's beautiful iconic features and buildings, removing them at their peril.

Will you leave our beloved and welcoming clock tower right where it is please, where it is wanted and needed as a vital part of the Village?

The Final UDF does not specify the removal of the clock tower; rather, in responding to community feedback, it provides the opportunity for different installations to be considered in a newly created public space.

Question 3:

Generic shopping centres are so common now, full of rushing, stressed people with no time to talk and certainly no inclination to hang about in an ugly environment. Anxiety has become a sad symptom of our modern society affecting all ages. Undeniably, fresh air, trees, birdlife, space, sky, community connections and just a pleasant place to sit can alleviate the sadness.

Do you appreciate and care that maintaining Highton Village as the leafy, people friendly, low-rise, charming and special place it is now can promote happiness and better mental health in our community, now and in the future?

It is standard process to update UDFs to respond to changing conditions, including population growth, the demand for employment and retail space, as well as the need for public realm upgrades. The current controls for Highton Village allow for significantly more development than what is proposed in the Final UDF. The new proposed controls are therefore intended to support changing conditions while protecting the character of the village.

Sally Kirner

Question 1:

Due to recent approval being granted by Councillors to raise the overall building heights to a maximum of 10 storeys at the Sale Yards development site, will COGG now commit to restricting building heights in Pako North to no more than 3 storeys to ease pressure on the area and its infrastructure?

Thank you for your question, Sally.

When planning for infill development, individual character and infrastructure capacity is considered for each local neighbourhood. The design controls for Pakington Street North will be considered based on the analysis and design review that has been undertaken specific to the local area.

The saleyards precinct has been through the same local planning process, and the building heights and design controls are subject to independent review at an upcoming planning panel hearing.

Andrew Katos

Question 1:

The proposed Highton Urban Design Framework (UDF) has many sensible improvements including upgrades to footpaths, lighting and street furniture along with additional plantings and street art. I have been consulting broadly with the community and whilst these improvements have support there are several aspects of the UDF that are not in line with community and business owner's expectations.

From my consultation the following aspects of the proposed UDF are not supported by the majority of the community:

- A proposed four story building adjacent to Nardi's Celebrations;
- Substantial loss of car parking;
- The removal of the roundabout at Taylor Court;
- Unnecessary additional pedestrian crossings that will impede the flow of vehicle traffic;
- Changes to the Barrabool Road service road which restrict entry from the Taylor Court roundabout, remove the current car parking arrangements and the planting of trees which obscure vehicles when turning into Barrabool Road.

Will Council amend the Highton UDF to remove proposed aspects that are not supported by the majority of the community and business owners?

Thank you for your questions, Andrew. To address each of your points:

A proposed four storey building adjacent to Nardi's Celebrations

It is being incorrectly reported that the extension of the retail space next to Nardi's is four storeys. The Final UDF proposes a three-storey height limit for this site, in line with all of Belle Vue Avenue.

Substantial loss of car parking

An independent study of parking showed that demand peaked at 90% in off-street car parks during the week and at the weekend for less than one hour. Parking demand on-street peaked at 70% during the week and lower on the weekends. These findings show that there is always parking available in the village. The Final UDF proposes a net loss of 7 spaces to accommodate significant upgrades for the public realm. However, the potential future relocation of the library would result in a net gain of 13 parking spaces.

The removal of the roundabout at Taylor Court

The proposed removal of the roundabout has been based on the recommendations from three independent traffic consultants, all of whom identified it as noncompliant with current standards as well as posing a safety risk.

Additional pedestrian crossings

Based on the State Government's Movement and Place Framework, activity centres should be low speed environments given the concentration of pedestrians. The pedestrian crossings improve safety and accessibility for the most vulnerable road users, encouraging shoppers to visit more businesses in the village.

Changes to the Barrabool Road service road

Access to the service road is maintained off Belle Vue Ave. Tree planting will be completed in accordance with Australian Standards to avoid causing any safety issues.

Question 2:

The proposed Highton Village Urban Design Framework is seeking to remove the Taylor Court roundabout.

Is Council seeking to remove this roundabout at the behest of the Victorian State Government Department of Transport?

The proposed removal of the roundabout has been based on the recommendations from three independent traffic consultants, all of whom identified it as noncompliant with current standards as well as posing a safety risk. This included consultation with the State Government.

Jennifer Bantow

Question 1:

There are three final outcomes motivating the Marshall bluestone cottage community group two-year campaign, LOCATION and AUTHENTICITY and FUTURE USE. These three outcomes are defined in OPTION 1, described in Attachment 2.3.2 in tonight's agenda papers - Marshall Bluestone Cottage Community Group's response to Council's consultant's Feasibility Study July 2022. LOCATION A landmark is defined as an object or feature of a landscape or town that is easily seen and recognised from a distance, especially one that enables someone to establish their location. OPTION 1 The Cottage on its original site at the intersection south-east corner was a well-known Marshall landmark.

The MBCCG Proposal is that the Cottage relocation at the same intersection diagonally opposite on the NW is the best option, with high visibility closest proximity to the original site (as specified in the Burra Charter,) and can be seen from four directions. OPTION 3 location is in an area off Field Court north-west of Marshall Reserve, which is set well back from Barwon Heads Road and is not clearly visible to passers-by and obscured by car parking. This location does not enable the Cottage to be a 'landmark', the location of Option 3 is unsuitable in comparison to Option 1. This obscurity is liable to lead to vandalism.

Will Council please amend the Officer's recommendation 2 and recommendation 3 and delete Option 3, and replace it with Option 1?

Several factors were considered in selecting a preferred site for the rebuilt cottage: location within the historical area of Marshalltown was paramount; visibility from the main road; and accessibility. Both Options 1 and 3 occur within Marshalltown. Option 1 might offer marginally better visibility than the Option 3, however, it lacks the ease of access and safety provided by the latter, in that parking is more distant and not readily apparent, and there are potential safety issues should visitors try to park along the side of either road to take a photo or enter the cottage.

Question 2:

AUTHENTICITY OPTION 1 proposes a genuine rebuild, according to Burra Charter principles which are described in the Marshall Bluestone Cottage Community Group's Attachment 2.3.2 - in particular Article 8 which describes Setting Conservation requires the retention of an appropriate setting. This includes retention of the visual and sensory setting, as well as the retention of other cultural relationships, in this case denoting the original area of Marshall Town Also Burra Charter Article 1 says - Reconstruction means returning a place to a known earlier state OPTION 3 does not provide for authentic restoration of the Cottage, but proposes a heritage facade only rebuild, which is a mockery of true conservation.

Also for this reason will Council please amend the Officer's recommendation 2. and recommendation 3. and delete Option 3, and replace it with Option 1?

The City's heritage advice is that the Burra Charter allows for flexibility when accommodating sustainable new uses into any historical building, and for buildings where relocation is the only means of ensuring their survival then greater flexibility is allowed over buildings being conserved upon their original site.

Question 3:

FUTURE USE the proposal of OPTION 1 to become Marshall Town History House, with multiple uses including Tourist Information, and repository for archival records of Marshall's significant industrial history, as well as local group usage, school groups and coach tours, and added advantage of provision of public conveniences for users of the adjacent tennis court and playground.

Also for this reason will Council please amend the Officer's recommendation 2. and recommendation 3. and delete Option 3, and replace it with Option 1?

The only use-related differences between Options 1 and 3 are that Option 3 provides better accessibility and the partial removal of some internal partitions will accommodate larger groups of people, more diverse uses, and improved safety/security through better sightlines. Retention of all the internal partitions as proposed in Option 1 would leave small rooms in which it would be difficult for more than 6 people to interact in an activity.

Angela Mangan

Question 1:

Will Council please acknowledge and confirm so it is placed on public record that the of CoGG engagement events I have outlined above occurred as I have described.?

Thank you for your submission and questions, Angela.

The following community engagement was undertaken by the City for the preparation of the Pakington Street UDF. Due to COVID restrictions some of the engagement had to be changed. All engagement was designed to ensure it was fitting for the specific phase and goal of engagement.

1	Establish a shared vision <i>Several in-person workshops with 72 people attending as well as a survey conducted. In total 320 submissions and 510 survey responses were received.</i>	March – April 2018
2	Feedback on draft UDF <i>This involved online forums with 51 participants, written submissions and online feedback. In total 77 written submissions were received.</i>	March – August 2020
3	Feedback on Interim Final UDF <i>In total, 379 submissions were received. A local campaign opposing the interim final UDF was initiated by community members.</i>	6 August – 24 September 2021
4	Increased understanding of Pakington North Precinct	11 August – 11 September 2022

It can be confirmed that all consultation meeting dates as set out in your email have occurred.

Question 2:

Will the City of Greater Geelong stop the Pakington street North UDF process and instead begin adequate community consultation that is transparent and genuine?

A decision on the Pakington North Precinct is scheduled for next month's Council meeting.

Caitlin Kirby

Question 1:

Now that the Community Engagement Report for the Pakington North UDF has been presented, how does your team plan to “respond to the issues that were highlighted during consultation” and ensure solutions are reflected in the final UDF?

For the record the main concerns were:

1. Building heights (how truly upsetting for the residents that live directly next to the proposed 30m+ residential/commercial buildings);
2. Significantly increasing traffic volume in an area that already struggles with traffic concerns at peak times and a lack of parking options;
3. Destroying a distinctly unique, historically valued and celebrated piece of Geelong heritage with multiple 4/6/8 storey buildings;

4. A lack of neighbourhood services and amenities required to support projected population volume; and
5. Placing a significant amount of additional stress on an area that already feels the negative impact of decrepit infrastructure.

* *It appears flagged environmental concerns such as developing in an area that already has little to no green space and also pedestrian safety concerns were missed in the presented report.*

Thank you for your questions, Caitlin.

The Pakington North Precinct is scheduled for next month's Council meeting. The Council Report will include a response to the key issues raised during the last consultation.

Question 2:

Will the final Pakington North UDF (complete with amendments that reflect the public's concerns) be available for the public to view/comment on prior to next month's meeting? If so, by what date? If not, what is the reason for this?

The Pakington North Precinct is now scheduled for the Council Meeting in November. As per normal practice, documents are released when the agenda is published on Council's website. This occurs the Friday before the Council Meeting.

Question 3:

Do you believe that less than one month is an appropriate timeframe for members of council to review the final UDF and confidently vote on it?

Councillors have been engaged in the consultation process for the Pakington Street and Gordon Avenue UDF since 2018 and adopted the Gordon Avenue and Heritage Core Precincts of the Pakington Street UDF in December 2021.

Claude Eagles

Question 1:

Re a redevelopment of Highton shopping village. Is this 4 storey building on Council land and is this land being sold to a developer, and who is the developer involved?

Thank you for your questions, Claude.

The proposed height limit for this potential future development site is three-storeys, rather than four-storeys. Development of the site will be subject to a future feasibility study. The site is not currently for sale.

Question 2:

With the removal of so many carparks where will all the cars park, the parking situation almost full to capacity most days now and with all this extra development will come extra cars?

An independent study of parking showed that demand peaked at 90% in off-street car parks during the week and at the weekend for less than one hour. Parking demand on-street peaked at 70% during the week and lower on the weekends. These findings show that there is always parking available in the village. The Final UDF proposes a net loss of 7 spaces to accommodate significant upgrades for the public realm. However, the potential future relocation of the library would result in a net gain of 13 parking spaces.

Question 3:

I note this centre already has a flooding problem as I see at the moment the fruit shop at the centre has sand bags at his door ready for use the other night. This 4-story building proposed is being built over a creek which we know floods. The arrangement of the traffic at the moment works well. This development will destroy the village atmosphere with the changing of parking, traffic configuration.

The Highton Flood Study identifies areas of high hazard flooding within the precinct. Public realm works offer opportunities to improve the situation particularly the car park adjacent to Barrabool Road/ Highton Reserve and Taylor Court. For any future development potential flooding must be considered and appropriate measures must be put in place. The guidelines make some suggestions to protect buildings from flooding while still achieving good public realm outcomes.

Dave Speirs**Question 1:**

REDUCTION IN PARKING LOTS: at busy times the parking is at 80 to 90% capacity. With the increase in retail space, additional office and accommodation, on-street parking will increase. What parking issues are predicted and what actions can be implemented to restore an attractive shopping village?

Thank you for your question, Dave.

The Final UDF proposed the development of a parking precinct plan to ensure parking time limits more closely reflect the needs of businesses and services in the village. By doing so, it is possible to increase the utilisation of the existing parking supply. The Final UDF proposes a net loss of 7 spaces to accommodate significant upgrades for the public realm. However, the potential future relocation of the library would result in a net gain of 13 parking spaces.

Jessica Sullivan (asked by Dr Jane Mooney)

Question 1:

Noting a UDF clause already exists in the planning scheme that was developed through extensive consultation, why are Council seeking to unduly influence the community by refusing to do urgent maintenance and repair work on Pakington St North until this specific iteration of the UDF is passed?

Thank you for your questions, Jessica.

Council's program for urgent repairs is fully separated from the preparation of long-term strategic planning documents such as the proposed UDF.

Question 2:

The North Pakington Street UDF references the Housing Strategy 2007 (from which the Settlement Strategy was developed) as a key driver for the North Pakington Street UDF. Both these strategies, as well as the original UDF framework and current clauses of the Planning Scheme all make various references to discouraging building heights above 3 storeys in increased housing diversity areas and discouraging building heights more than 1 storey above the prevailing building height on Pakington Street North specifically.

Why then have all public consultation documents for the Pakington Street North UDF exceeded and disregarded these guidelines?

The increased housing diversity area (IHDA) policy applies to residential neighbourhoods, not commercial shopping strips. Pakington Street is a commercial shopping strip. As noted in the aforementioned documents, the IHDA is a transition area between commercial zones and lower rise residential areas. UDFs are updated to reflect changes in conditions, including population growth, employment demand and retail demand.

Simon Nardi

Question 1:

Dear Councillors, council staff and members of the City of Greater Geelong. Having worked in Belle Vue Avenue for the last 19 years, I believe I have a great feel for the Highton villages strengths and weaknesses. The biggest weakness that I believe the village has, and 90+% of the community can acknowledge, is car parking. With the current UDF there will be considerably less car parking. In a growing region of Victoria more car parks are needed to future proof the village.

How can a new development be considered without more car parks for the community?

Thank you for your question, Simon.

An independent study of parking showed that demand peaked at 90% in off-street car parks during the week and at the weekend for less than one hour. Parking demand on-street peaked at 70% during the week and lower on the weekends. These findings show that there is always parking available in the village. The Final UDF proposes a net loss of 7 spaces to accommodate significant upgrades for the public realm. However, the potential future relocation of the library would result in a net gain of 13 parking spaces.

Pati Seiler

Question 1:

Please refer to "Unique Geelong West Street Pattern" map supplied under separate cover. My two questions are about the substandard Traffic Network Impact Assessment of the UDF. Question #1 with reference to the "Unique Geelong West Street Pattern" highlighter map please note carefully that Geelong West has very few north-south alternative streets to Pako, and is a vastly different street pattern compared to the typical 250m interval "tartan" grid that you can see in the Geelong CBD and in other comparable inner city Melbourne suburbs that have a regular grid of major roads and laneways. Geelong West street cannot absorb concentrated traffic density. As such the management strategy on page 4 of the Traffic Network Assessment which states that "as specific roads/ streets/ intersections become more congested, a redistribution of traffic naturally occurs as a proportion of drivers find alternative (quicker) routes" is an absolute fantasy notion and a failure of responsible urban design; and the future repercussions of traffic congestion would be severe on both residents and business.

The question is does Council now recognise that the traffic objections expressed by so many ordinary residents in the consultation period, are actually based in an objective reality of the existing street layout?

Thank you for your questions, Pati.

The concept of traffic redistribution is well-established but is not relied upon as the sole mitigation strategy for future traffic demand in the study area. The City expedited an action in the Final UDF to prepare a public parking plan for Pakington Street. The City is also currently undertaking the development of an integrated transport plan for Geelong, which will include proposals for a more balanced transport system that is less reliant on private cars by encouraging a shift to public transport, walking, cycling and various forms of rideshare options. Furthermore, the Final UDF proposes that Local Traffic Management Plans are developed to mitigate issues for residents. Finally, all planning applications are individually assessed for traffic impacts and must demonstrate available capacity in the existing and future road network.

Question 2:

The management strategy on page 4 which states that "in order to minimise the impact on residential streets, Local Area Traffic Management measures such as traffic calming, parking restrictions and ONE-WAY TRAFFIC MOVEMENTS should be explored and, where appropriate, implemented." On Isabella, Britannia, Clonard and Collins Street you would have to drive about 800m from Pako before you reach the first north-south intersection being at Elizabeth Street, which is already an extremely heavily used but small scale neighbourhood street (with historical serious resident concerns about pedestrian access and safety).

Does Council now recognise that ONE-WAY streets are unlikely to be a feasible solution to handle increased traffic on Pako and that ONE-WAY streets would create a range of problematic consequences in Elizabeth Street and Shannon Avenue; and that the UDF which only studied a very small area of the traffic map is therefore deeply flawed by relying on such ill-considered Traffic Analysis?

The one-way street is suggested as a Local Area Traffic Management measure which, among other measures suggested, should be explored. This is future work as part of the UDF implementation.

Brendan John Quirk (asked by Dr Jane Mooney)

Question 1:

This question, in two parts, relates to the City of Greater Geelong's interactions with the Victorian State Government regarding town planning outcomes for Geelong West:

Has the City of Greater Geelong, including its representatives: mayor, councillors, council officers or otherwise, formally sought to acquire, or have acquired, from the Victorian State Government, the Geelong rail maintenance yards, that in the proposed North Pakington Precinct UDF make up the area of the proposed 8 storey development and public space, in the proposed plans?

If not, why is this included in the Proposed UDF?

Thank you for your questions, Brendan.

Neither the City nor Council has sought to acquire this site. A UDF is not bound by land ownership. The Geelong rail maintenance yards is an area which is considered part of the Pakington North Precinct.

Question 2:

Has the City of Greater Geelong, including its representatives: mayor, councillors, council officers or otherwise, written/communicated to the Victoria State Government, requesting that the C300 Planning Amendment, as adopted and passed by the Council and State Government, be repealed / cancelled to allow for greater development in Geelong West, and / or Greater Geelong, as was the council's intention prior to the residents and community groups opposing this and lobbying for the current outcome?

It is neither Council's intention, nor is it necessary, to repeal or cancel planning scheme amendment C300. Planning scheme amendment C300 applies to the residential areas across the City, including Geelong West. It does not apply to shopping strips and commercial areas such as Pakington Street.

C300 limits development in residential streets in Geelong West and this continues to be Council's policy. The UDF directs medium density housing development to the north end of Pakington Street and Gordon Avenue to minimise development pressures and protect the neighbourhood character of the residential areas.

Katherine Talbot

Question 1:

Whilst the Council is engaging a Community Housing Provider to develop a plan for 5A Dean Street.

Is there also a plan being developed for community consultation that show what the space will look like if retained as a community open space such as parkland, playground, other community facilities.

Do Council plan to run an open consultation process such as 'Have your say' that allows community members to contribute ideas?

Thank you for your submission and questions, Katherine.

The City intends to undertake a collaborative design process in 2023 to work together with an appointed community housing provider and the local community on the potential social housing developments as the first priority option. As part of this approach, the process undertaken will examine other possible alternative uses.

Question 2:

I urge the Council to do a traffic impact study on the Dean Street site and the streets and laneways that surround Dean Street, in particular Dorothy Avenue and Lloyd Street.

Why is this being left up to those developing the site?

Surely it would be a priority before a decision on the site is made. Dean Street and Dorothy Avenue are both extremely busy during school pick up and drop off times. It is almost impossible to drive down them at these times. The TLC staff carpark further contributes to traffic and congestion on the street.

I urge Councillors to view this for themselves at 3.15pm on a school day when school is dismissed.

The City has heard the concerns raised about traffic, access and parking from the local community. Impacts on the adjoining neighbourhood will be considered as part of the next steps of the investigation and community engagement process.

Question 3:

Has the TLC Aged Care facility been consulted by the council? Given their property runs the whole length of the Dean Street Site what concerns/feedback have they raised?

Their property has several emergency exits that lead onto the site.

The City intends to undertake a collaborative community design process in 2023 to work together with an appointed community housing provider. Key stakeholders such as the TLC Aged Care organisation will be consulted as part of this planned process.

Dr Jane Mooney

A resident of Elizabeth Street told me that the City of Greater Geelong removed the chicanes on Elizabeth St Geelong West a few years ago, so more traffic could flow down Elizabeth Street because there was already too much traffic going down Shannon Ave and the Northern end of Pakington Street. I understand there are many residents who are concerned about the high volume of traffic along Elizabeth Street and the many dangers this poses. I also understand that there have been numerous communications with CoGG officers and Councillors about the problems including an on-street meeting with residents and some Councillors to explain the traffic problems and possible solutions.

The street pattern of Geelong West surrounding the Northern part of Pakington Street is very old and was never designed with future population growth in mind. Typical street grids in Melbourne suburbs have evenly distributed streets that run in both east/west and north/south directions. The area surrounding the northern part of Pakington Street is unique in that it has very limited street connectivity that runs North/South. There is Pakington Street and Shannon Avenue and with only narrow Elizabeth Street that runs between them.

The traffic assessment report that was released late last year (2021) does not provide any assessment of the current traffic problems along Elizabeth Street or Shannon Avenue that result from this unique lack of connectivity. The study area was extremely narrow and excluded any analysis of the current pressure on Elizabeth Street or Shannon Avenue and their intersections to determine the impact of further traffic being directed there as a result of the overdevelopment proposed in the Pakington North UDF.

Question 1:

Can CoGG please confirm when and why the chicanes were removed from Elizabeth Street and given that Council is well aware of the history of traffic problems along Elizabeth Street why was it and Shannon Avenue not included in the scope of the traffic assessment and why was a proper traffic assessment not performed as a requirement of the Pako North UDF.

Thank you for your questions, Dr Mooney.

The chicanes were removed between 2005 and 2009 and replaced with speed humps along the street. Speed humps are considered a more effective treatment to physically control driver speed.

Both Elizabeth Street and Shannon Avenue are outside of the commercial zone for Pakington Street, which was the focus of the UDF. As such, there was no requirement to include these streets, or any others, in the traffic assessment.

Question 2:

The City of Greater Geelong and some Councillors have been well aware of the resident concerns about the existing traffic conditions and problems along Elizabeth St for some time. Can COGG please explain why residents should not hold them accountable for knowingly creating more dangerous traffic conditions by pushing ahead with this ill-considered overdevelopment of Pakington North?

Elizabeth Street is outside of the commercial zone for Pakington Street, which was the focus of the UDF. As such, there was no requirement to include these streets, or any others, in the traffic assessment. There is no evidence that the proposed development of Pakington North Precinct will create dangerous traffic conditions on Elizabeth Street.

Question 3:

The Help Save Pakington Street resident action group has produced a new hard copy petition, which says:

We oppose the proposed high-rise development in the Pakington North Precinct (according to the final Urban Design Framework), because we are concerned about its adverse impacts on amenity, local traffic, parking, and the destruction of the soul and character of our neighbourhood.

We demand that the City of Greater Geelong stop the Pakington Street North UDF process and instead begin adequate community consultation that is transparent and genuine.

We started collecting signatures on Saturday. Despite the inclement weather we have already collected over 250 resident signatures and we will continue collecting more.

Whilst speaking to people that passed by I was struck by how many said they had come from far away to enjoy a day shopping in Pakington Street. Places like Anglesea, Barwon Heads, Torquay, Melbourne, Sunshine, East Geelong, Manifold Heights even someone from Birregurra. When these people looked at the map showing the overdevelopment proposed in the Pakington Street North UDF all of these visitors were absolutely shocked and amazed that the City of Greater Geelong would even be considering this. All of them said that regrettably they would have to find somewhere else to dine and shop due to the increased traffic and car parking problems and they were all really sad to think that the soul of Pakington Street would be lost.

Why is the City of Greater Geelong not taking steps to protect Pakington St and the things that make it so attractive to visitors? Why aren't they developing a plan that extends these attractive features from the heritage core along the Northern end of Pakington Street?

It is standard process to update UDFs to respond to changing conditions, including population growth, the demand for employment and retail space, the need for public realm upgrades, as well as the need to address urban degeneration and decay. In addition to addressing these changing conditions, the Pakington North Precinct clearly needs renewal. Unlike the Heritage Core Precinct, the North Precinct does not warrant the same heritage protection. At the same time, it is important to protect the retail experience in the Heritage Core, rather than put it at risk by encouraging more of the same in the northern precinct. The aim is to create the opportunity for a more diverse retail, commercial and service offering in the North Precinct that will complement, rather than compete with the Heritage Core.

Submissions

The following submission was presented by Dr Jane Mooney relating to Pako North UDF/Community Engagement from Angela Mangan:

I am Angela Mangan, a resident of Geelong West in Pako North, and my comments and questions relate to the Pako North UDF/ community engagement process.

At the previous CoGG meeting the issue was raised about the lack of proper community consultation and engagement relating to the Pako North UDF. There is a requirement by COGG under its own community engagement policy to and I quote “to use methods to produce the most meaningful feedback”, and also “participants are to have access to appropriate information and time and space to deliberate”.

I would like to provide details of some examples of how CoGG has not adhered to its own community engagement policy during the Pako UDF “consultation” process. I kept records because it was so gobsmacking to me that CoGG would even consider it appropriate to release such important documents when residents were dealing with COVID lockdown situations.

Some specific instances where CoGG did not adhere to its own community engagement policy are as follows.

1. Releasing the interim final draft of the UDF on the first day Geelong went into a Covid pandemic lock down on 6th August 2021 obviously meant people were focused on other issues, were unable to gather, hold public meetings or street meetings or interact in a normal way due to the covid restrictions which prevented meaningful feedback from residents.
2. CoGG did not include information about the UDF on its own Facebook page or put-up bench seat posters in the Pako heritage core publicising the UDF until the five week mark of the original six week public consultation period. This only occurred after members of the GW community contacted CoGG about the community at large being unaware of the UDF.
3. It is a requirement in CoGG’s own engagement policy and I quote “participants have access to appropriate information and time and space to deliberate”. CoGG released the traffic impact assessment report less than 24 hours before the UDF public feedback deadline closed (it was released after the close of business on 23rd September 2021). This was also on the eve of a public holiday long weekend when most people would not have been aware this had been released. This allowed no time for residents to assess the 35-page report and provide feedback.
4. In December last year when the final Pako UDF was due to be uploaded onto the CoGG website we had been originally advised this would be made available one week before it was being voted on by Council. It ended up being uploaded on the Friday before the Tuesday CoGG meeting leaving no time for residents to be able to assess it despite us being originally told it would be available one week beforehand. Conflicting information was provided over a number of days by various people at CoGG about the UDF document availability date.
5. During the further community engagement period for the Pako North UDF in September 2022 the on-street neighbourhood chats with a CoGG urban planner and residents were conducted on noisy and windy streets, with no seating provided so it was not conducive for discussion and was an inappropriate setting.

Some residents when wanting to register for these chats were told that they were full - how can an on-street meeting be full when no COVID gathering restrictions applied - and some were told by CoGG they could not attend for this reason, and others registered but never received emails providing details of the meeting locations. An urban planner new to the UDF who attended these chats could not answer many questions. These questions were noted down by the consultant but to the best of my knowledge answers were not provided afterwards directly to those who asked them. Instead the questions were included as "issues raised" in the consultant's report but with no answers.

6. The Thursday 18th August webinar with Urban Planning was conducted the week before these on-street chat meetings and was arranged with only a matter of days' notice giving inadequate time for residents to be aware of it and register. When I contacted Council to say this was inadequate short notice of this webinar I was told that was the only time and date the urban planners were available. Letters were hand delivered by Council to residents mailboxes on the Saturday 13th August and the original closing date for people to register and submit questions was Monday 15th August – a totally inadequate time frame given people would probably not check their mailboxes on a weekend.. This deadline was eventually extended to the Wednesday 17th August after repeated requests and complaints, but anyone reading the Council hand delivered letter which referred to the Monday 15th August as the deadline would have assumed they missed that deadline and may not have bothered to do anything. People who used the chat function during that webinar said afterwards many of their pre submitted questions were not answered during the webinar or subsequently.
7. The only town hall style meeting that has been held during this entire UDF process was the one arranged by the Help Save Pako Geelong West community itself with 170 people attending, with Brownbill Councillors Mansfield and Kontelj attending. We asked when organising this meeting if it was possible for a senior Urban design team member to attend and that request and invitation was declined. CoGG has never instigated or organised a town hall meeting with residents. It was left to residents to arrange this.
8. The May / June 2022 parking plan meeting held between Council and Pako traders - it was stated on the CoGG website that this would not include residential parking concerns and that this issue would be dealt with separately and at a later date. These meetings with residents on parking have not occurred as indicated. When will these occur?

The whole engagement process has been about CoGG imposing its view on residents and it has been a one way flow and therefore has not been actual engagement. It appears that CoGG has structured this entire engagement process with the objective of minimising community engagement and feedback rather than maximising it as required under its own engagement policy, by releasing documents on the day Geelong went into a lockdown, or on the eve of public holidays, or within short 24-hour timeframes before public comment periods closed, made the registration process of neighbourhood chats difficult. Virtually all of the community engagement to publicise the UDF has been done by the GW community via the Help Save Pako Street facebook group, not by CoGG. Or publicity has only occurred by CoGG after residents have made repeated contact with CoGG to complain. Residents should not have to be doing CoGG's job !!!

The recent further community engagement period was less than one month; the neighbourhood chat sessions failed to address resident concerns, residents felt angered at this being a "re- education" campaign and not being genuine community engagement and it was out of this that the HSP group contacted the Brownbill Councillors to request a town hall meeting.

I recently read an article about what defines good urban planning. The overseas urban planner interviewed commented that for urban planning projects to be successful, residents in the community need to be genuinely embraced and brought in and involved as a genuine part of the process and recognise local knowledge and the impact on residents who have to live with the final result, are all vital pieces of input into successful urban planning and design outcomes. This same overseas urban planner commented that bad urban planning process and bad outcomes result when urban planners treat the community and residents as adversaries and obstacles.

To have our concerns minimised or disregarded and be described by some as NIMBYS is offensive in the extreme. We are not against sensitive development and genuine rejuvenation but are totally opposed to the overdevelopment proposed in the final Pako North UDF.

1.7. Petitions

Cr Nelson presented a petition containing 88 signatures in relation to the rear exit gate at Armstrong Green Retirement Village.

Daniel Senia presented a petition on behalf of the Future Residents of Queens Park containing approximately 620 signatures asking for Council's support in renaming Lower Highton Area to Queens Park.

2. REPORTS

2.1. Youth Council Third Report 2022

Source: Community Life
Director: Robyn Stevens

Purpose

1. To note the third report to Council from the Youth Council Advisory Committee 2022.

Background

2. Twelve local young people were elected to the City of Greater Geelong Youth Council Advisory Committee (Youth Council), and during their 12-month term will serve as the region's official 'youth voice'.
3. Aged between 12 and 17 years, the Youth Council members represent a spread of local towns and suburbs, with three representatives from each of the four Council wards: Bellarine, Brownbill, Kardinia and Windermere.
4. Youth Council provide advice and updates for Council's consideration through monthly Youth Council meetings at City Hall, through collaboration with Councillors via the Councillor Connect Mentoring Program and by consulting with young people through various community engagement activities.

Key Matters

5. Deputy Junior Mayor Angelle Mackay has authored the Youth Council Third Report to Council (attached) and has included Youth Council's activities, discussions and preliminary work since their induction in January of this year.
6. The third report provides input and advice on areas of importance for young people.
7. The input and advice presented focuses on the progression of an Integrated Youth Hub in Central Geelong and the Youth Council support of the project.

Deputy Junior Mayor, Angelle Mackay provided the gallery with an update on the Youth Council's activities since their induction in January 2022.

RESOLUTION – Item 2.1

Cr Mason moved, Cr Harwood seconded

That Council:

1. **Note the third report provided by the Youth Council Advisory Committee 2022; and**
2. **Request the Chief Executive Officer consider the advice regarding the establishment of a Pilot / Interim Youth Hub in Central Geelong.**

Carried

Financial Sustainability

8. Funding for the Youth Council program and activities has been allocated as part of Council's annual budget process.

Community Engagement

9. Youth Council has engaged young people in their wards via social media, using polls and questions to validate recommendations and gain required feedback and input.

Social Equity and Sustainability

10. The Youth Council provides an opportunity for young people to contribute to and be included in civic life.

Relevant Law/Policy/Legal Implications

11. The activities of the Youth Council comply with the adopted Terms of Reference (TOR).

Alignment to Community Plan and Vision

12. This report aligns with Our Community Plan 2021-2025 strategic priority: Healthy, caring and inclusive community.
13. This report aligns with the Community led 30-year Vision, "Greater Geelong: A Clever and Creative Future" community aspiration:
An inclusive, diverse, healthy and socially connected community.

Conflict of Interest

14. No officer involved in the preparation of this report declared a general or material conflict of interest.

Risk Assessment

15. Youth Council provides advice to Council on specific issues relating to young people in our community and does not speak on behalf of the Council or the City. The TOR guides the Youth Council.

Environmental Sustainability

16. There are no environmental implications arising from the subject of this report.

Attachments

1. D22 418132 Youth Council Third Report 2022 25 October [2.1.1 - 8 pages]

THE CITY OF
GREATER GEELONG

YOUTH COUNCIL THIRD REPORT TO COUNCIL

Tuesday 25 October 2022

DEPUTY JUNIOR MAYOR –
ANGELLE MACKAY

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Deputy Junior Mayor's Message

INTRODUCTION

Youth Council would like to acknowledge the Wadawurrung people, the traditional owners of the land, waterways, and skies, and pay their respects to Elders past, present, and emerging.

Youth Council would like to thank Councillors for their support and mentoring provided throughout the year. Youth Council members value the feedback and advice received from Councillors, which has helped shape and inform the recommendations presented in the third Youth Council report for 2022.

PURPOSE

The purpose of this report is to outline the progress and achievements of Youth Council since the previous report was presented to Council on 26 July. This report also contains advice Youth Council would like to provide to Councillors on matters of importance for young people as explored with Councillors during the Councillor Connect Mentoring Program (CCMP) sessions.

WHAT WE HAVE ACHIEVED SO FAR

Youth Council Meetings

In the May and July Youth Council meetings, members heard from the Coordinator Community Infrastructure Planning, and two external consultants responsible for the development of the Youth Hubs Feasibility Study that Councillors have recently received. Youth Council had previously heard from the 2021 Youth Council about the important work they had completed in this space, advocating for an Integrated Youth Hub in Central Geelong to address the significant increase in mental health issues and the social impacts COVID 19 has had on young people over the past few years. Youth Council were excited to be invited to a codesign workshop to unpack the needs of young people and help inform the design, location, and accessibility requirements of this crucial piece of Social Infrastructure. Youth Council are excited to see the progress Council has made in the establishment of this much needed Youth Hub.

Sustainability and environment have been an important focus for the 2022 Youth Council group. In the July Youth Council meeting, members heard from a representative from the Sustainability Advisory Group. The presenter discussed environment and sustainability in Geelong, the formation of the Sustainability Advisory Committee and unpacked how council engages with this advisory group to have input into key strategic documents to apply a sustainability lens. In the August meeting, the City's Senior Sustainability Officer spoke to Youth Council about the Climate Change Response Plan and the

actions Council is taking to combat climate change. Youth Council was inspired at the possibilities and impressed by current projects including the anti-erosion mussel reef - floating in Portarlington and Council's focus on empowering and funding community groups to create change through its grant rounds. These presentations outlined the importance of the serious but positive discussions surrounding climate change which have filled Youth Council members with hope for the future.

Youth Council also heard from the Coordinator Open Space Planning who provided an overview of the District Ride Centre, a new mountain bike park that is being established in Ocean Grove. Youth Council were delighted to hear that Council is now progressing the establishment of a mountain bike park to address the need highlighted by young people in the community and as was featured in Youth Council's third report, 2021. Youth Council were informed that Council has identified several potential sites and developed a draft concept plan for the park. Youth Council are excited to further discuss and explore this project with Councillors in the Councillor Connection Mentoring Program.

Finally, Youth Council engaged with the Aboriginal and Torres Strait Islander, Senior Policy Officer about January 26 and the consultation process that has commenced with the regions First Nations Peoples and interested community members. Youth Council strongly support the opportunity for community members to have their say about celebrations on January 26 and look forward to participating in the consultation process.

Youth Hubs Feasibility Study Workshops

The City's Community Infrastructure Planning team ran two, one day workshops to canvas the ideas and thoughts of young people through a codesign processes as was advised by the 2021 Youth Council in their second Youth Council Report. The workshop included this year's Youth Council who worked alongside previous years members, GASP, and participants from the fOrT to discuss the needs, opportunities, and concerns relating to the concept of a youth space and to inform the design of an Integrated Youth Hub in Central Geelong. Participants heard about the project, presenters reinforced its importance, and discussed what it would mean for young people to be able access a Central Youth Hub. Participants were then involved in a tour of four theoretical locations chosen by the consultants, where they performed an analysis of each site. Once participants returned from the tour, they worked in groups to design an operating model and the design elements - including the use of materials, furniture, and colours to create a functional and appropriate youth space. Throughout both days of the

workshop, location was identified as the most important consideration for a central youth hub. Youth Council members were able to hear from a broad cross section of youth community about what is most important to them and how the Hub can be designed to ensure everyone feels safe, connected, and able to access the supports they need in a safe, youth informed environment.

EVENTS AND OPPORTUNITIES

Barwon Water Community Consultation

In the last week of July, the Junior Mayor attended the Barwon Water Community Consultation session exploring their 2023 price submission. The workshop aimed to understand and review Barwon Water's approach to combat the negative effects that the expanding population, and climate change will have on the region's water systems. In attendance were a variety of professionals and advisory committees akin to Youth Council, as well as Barwon Water representatives, and Councillors from across the G21 region. This was a valuable opportunity for members to explore the planning processes and scope of long-term projects, as well as understand how decisions are made. The consultation was a valued and meaningful avenue to learn from others and provide recommendations in an open platform. It was a great opportunity to be a part of, and it was reassuring to see Barwon Water addressing some of the concerns of climate change and population growth by including and hearing from diverse voices from the community in their decision-making process.

Growing up in Cities

Youth Council were thrilled to attend Deakin Universities, 'Growing Up In Cities' workshop. Participants toured the city taking photos of locations and areas they felt were safe and youth enabling as well as capturing those that they felt were less safe for young people to occupy. Once back at the studio, researchers helped participants' draw a city and identify the most important elements needed to support communities to thrive. Once this was completed, participants were supplied with recycled materials to create and build one piece of infrastructure they thought was important to include. Once this was completed participants then put all their models together to create a town centre. This workshop explored crucial features of a city that promotes youth enjoyment, improved sense of safety, and that an environment that will foster youth involvement in community life. For example, the collaborative model had a focus on the arts, open air music and theatre events, and accessible green space. Youth Council members discussed and shared their support for family inclusive events as they often help young adults

feel more comfortable in the city due to increased visibility. Participants also drew on the importance of incorporating sustainable infrastructure into their design as they believed it greatly improves quality of life and alleviates some aspects of climate-based stress. Youth Council would like to thank everyone involved and note that the session was conducted in manner that allowed young people's voices to be developed, amplified, and heard through a fun and collaborative experience.

Raw + Real Racism Forum

Youth Council members were delighted to attend the Raw and Real Racism Forum hosted by Track C Consulting in late July alongside Youth Development Unit staff and Councillor Mason. A big highlight was hearing from Lowell Hunter, a proud Nyul saltwater man from the Kimberley's in Western Australia and experiencing the dance and song of the 'One Connexion' dance group. Lowell spoke about his art, connection to Country, and his experiences of racism in his community and workplaces growing up. Other speakers included Claire Coleman a Noongar woman from the south coast of Western Australia, who is an author and activist, who shared her insightful and evocative poetry; Leesa Timbi a Papua New Guinian woman who founded the Papua New Guinea Women's Association of Victoria and shared her experience with lateral racism in Papua New Guinea (a form of violence that happens within marginalised and disempowered communities), and finally Catherine Jonathan, a South African woman who is passionate about social change and started her own PR business 'Emerging Brand Africa' that aims to bring visibility to African businesses in Australia. All speakers shared their experience and reflections on racism in Australia today, Youth Council members found this to be a welcome and sobering reminder of how far Australia still has to go to address racism in everyday life. Youth Council members were thankful to be invited to attend this forum as hearing from people with lived experience inspired members to be agents of change and encourage others to challenge their preconceptions about racism. The Junior Deputy Mayor would like to encourage as many people as possible to attend this event in the coming years, especially those from local organisations, local government, and especially our Councillors.

RESIGNATION OF YOUTH COUNCIL MEMBER

Jason Binding, a Youth Council member from the Windermere Ward resigned from the Youth Council program to focus more on his employment opportunities and his studies. Thank you to Jason for all his contributions to the Youth Council program, Youth Council wish Jason well in all his future endeavours.

YOUTH COUNCIL ADVICE

Youth Council would like to commend Council on acting upon advice provided in the third Youth Council report in 2021 and referenced in the City's Social Infrastructure Plan – Generation 1, 2020-2023.

Youth Council would also like to restate and reinforce the importance of having an Integrated Youth Hub in Central Geelong that is located in a safe and accessible space, close to public transport, shops, universities and the skate park as well as open green space.

Youth Council acknowledge that the costs of creating as space in Central Geelong could be viewed as inhibitive but would like Council to consider the human cost for not establishing this space. Young people need a space in Central Geelong now, more than ever, and urge Councillors to work with all levels of Government to progress this work. The safety and wellbeing of Young People can be safe guarded through the establishment of this space, and support and services available from within it.

Youth Council's advice;

- Request that the City increase the intensity and frequency of advocacy work with State and Federal Government for additional funding to progress the project as described in the Youth Hub Feasibility Study.
- Council establishes a pilot/interim, Central Youth Space while work continues on securing and raising additional funding for the Integrated Youth Hub in Central Geelong.

OUR PRIORITIES FOR THE NEXT TWO MONTHS

Youth Council members are looking forward to the remaining Youth Council meetings, hearing from a range of guest speakers and continuing to work together to formulate discussion topics and test advice for upcoming Councillor Connect Mentoring sessions. Youth Council will also be working hard to shape the final recommendations made in the final Youth Council report. Finally, all members are looking forward to attending their final Councillor Connect Mentoring session with all Councillors' later in November.

CONCLUSION

The last 3 months have been very busy for Youth Council, and members are getting so much from the Youth Council program. Youth Council look forward to the final months of the program and to the opportunity to work together to provide advice to and learn from Councillors.

2.2. Highton Village Urban Design Framework

Source: City Planning & Economy
Director: Gareth Smith

Purpose

1. To adopt the Final Highton Village Urban Design Framework (UDF). Refer **Attachment 1**.

Background

2. The Final Highton Village UDF establishes short and medium-term objectives, strategies and actions to guide future development, streetscapes, movement and access in Highton Village.
3. The Final Highton Village UDF includes a strategic framework, public realm improvements and built form controls.

Key Matters

4. Preparation of the Final UDF commenced in 2016 and has been informed by five phases of engagement with residents and traders between 2016 and 2021, including the release of an Interim Final UDF in response to the impacts of COVID-19.
5. The UDF supports Council's goal of providing for 50 per cent of future housing needs through urban infill, as well as delivering greater housing diversity and affordability.
6. It supports future retail needs, as identified in Council's Retail Strategy and protects the 'village' character of the area and human scale in laneways.
7. The UDF proposes future development will be predominantly low to medium-rise mixed use with commercial, retail and residential uses.
8. The UDF also provides concepts for streetscape upgrade works, and addresses movement challenges for vehicles, pedestrians and cyclists aiming to enhance accessibility and walkability in the area.

Original Recommendation

Cr Harwood moved, Cr Moloney seconded -

That Council:

1. **Adopts the Final Highton Village Urban Design Framework:**
 - 1.1. **To enable the delivery of infrastructure upgrades; and**
 - 1.2. **To enable the preparation of an amendment to the Greater Geelong Planning Scheme to apply a new Design and Development Overlay Schedule to implement the built form objectives of the Urban Design Framework.**

ALTERNATIVE MOTION

Cr Nelson moved, Cr Grzybek seconded -

That Council:

- 1. Adopts the Highton Village Urban Design Framework subject to the following changes:**
 - 1.1 A maximum preferred building height of 9m on Belle Vue Avenue to protect the character of the Highton Village;**
 - 1.2 Retain the roundabout on Belle Vue Avenue;**
 - 1.3 Retain the Clock Tower;**
 - 1.4 Retain access to the service road from Belle Vue Avenue and Barrabool Road; and**
 - 1.5 There will be no net loss of car parking, with no additional pedestrian crossings.**
- 2. Request the Minister for Planning authorise the preparation and exhibition of a planning scheme amendment to introduce policy and development controls into the Greater Geelong Planning Scheme consistent with the recommendations from the Highton Village Urban Design Framework.**
- 3. Note the significant community benefit that will come from recommendations in the Urban Design Framework including:**
 - 3.1 A masterplan for streetscape upgrades for Belle Vue Avenue, Belle Vue Arcade, Village Walk, Barrabool Service Lane;**
 - 3.2 Design guidelines to protect the village character from inappropriate development by introducing building height guidelines where none exist now;**
 - 3.3 Retention of the library; and**
 - 3.4 A righthand turn lane from Barrabool Road heading eastbound to Belle Vue Avenue.**
- 4. Spend the 2022-23 budget allocation of \$606,000 on paving and laneway upgrades as soon as practically possible.**

Carried

Division:

For: Crs Nelson, Sullivan, Asher, Harwood, Moloney, Grzybek, Mason, Aitken

Against: Cr Murrighy

Financial Sustainability

9. Once endorsed, the Final UDF will need to be incorporated into the Greater Geelong Planning Scheme, which, in addition to officer time, may entail costs associated with the procurement of expert witnesses and legal representation.
10. Council has allocated \$606,000 to the design and delivery of a laneway and Barrabool Road service lane upgrades.
11. Delivery of Belle Vue Avenue will be subject to further work, including detailed design and council funding through the business case process. These works, once delivered, will be subject to ongoing maintenance costs.
12. Barrabool Road intersection upgrade will be subject to further work with Regional Roads Victoria including detailed design and funding from the State Government.

Community Engagement

13. Five phases of community consultation have been undertaken between 2016 and 2021.
14. The first phase was undertaken in 2016, which included in-person workshops with the community and traders. In total 117 people participated, and 20 submissions were received.
15. The second phase was undertaken over six weeks in 2017, with consultation on the draft UDF involving an online survey. In total 130 submissions were received.
16. The third phase was undertaken during 2018, with consultation on alternate options for Belle Vue Avenue involving workshops with traders, survey and online submissions. In total 30 people participated and 50 submissions were received.
17. The fourth phase was undertaken during 2020, with consultation on alternate options for Belle Vue Avenue involving workshops with traders and 20 submissions were received.
18. The fifth phase was undertaken over six weeks during 2021, with consultation on the Interim Final UDF. In total, 65 submissions were received.
19. The Community Consultation Summary Report (see **Attachment 2**) identifies the main themes the community supported or were concerned about.
20. The Final UDF has also been informed by input received from departments across the City and from external stakeholders including the Department of Transport.
21. If the UDF is adopted, the community will have further opportunities to engage and provide feedback during the planning scheme amendment process and as part of the design of all future public realm upgrades.

Social Equity and Sustainability

22. The UDF contains social equity considerations, providing opportunities for increased housing supply, diversity and affordability and contributing to Council's goal of providing for 50 per cent of future housing needs through urban infill.
23. Universal design is a key consideration in the delivery of future public realm improvements, enhancing the walkability and cyclability in and around the village.

Relevant Law/Policy/Legal Implications

24. The Final UDF is consistent with state and local planning policies relating to the delivery of urban consolidation, a range of housing types in well serviced locations, environmentally sustainable design and the preservation of neighbourhood character.
25. The Final UDF is consistent with the Housing Diversity Strategy, Settlement Strategy, One Planet Living principles and the Clever and Creative vision.

Alignment to Community Plan and Vision

26. This report aligns with Our Community Plan 2021-2025 strategic priority:
Healthy, caring and inclusive community.
Strong local economy.
Sustainable growth and environment.
27. This report aligns with the Community led 30-year Vision, “Greater Geelong: A Clever and Creative Future” community aspiration:
A destination that attracts local and international visitors.
A prosperous economy that supports jobs and education opportunities.
An inclusive, diverse, healthy and socially connected community.
People feel safe wherever they are.
Sustainable development that supports population growth and protects the natural environment.

Conflict of Interest

28. No officer involved in the preparation of this report declared a general or material conflict of interest.

Risk Assessment

29. Council’s Retail Strategy has identified demand for increased gross floor area in Highton Village, however, there is currently limited supply. Without new development, as proposed in the Final UDF, the activity centre may stagnate over time.
30. The informal roundabout at the intersection of Belle Vue Avenue and Taylors Court was identified as a safety risk by an independent road safety auditor during the development of the UDF, and Council may be liable if not addressed.

Environmental Sustainability

31. The UDF features “retain and enhance the tree canopy and greenery of the Village”, “encourage Environmentally Sustainable Design (ESD) in the architecture and design of buildings and public spaces” and “improve pedestrian and cyclist safety and amenity when designing new streets and buildings” as some of its core urban design guiding principles. Such principles and outcomes have important environmental implications.
32. The UDF meets the requirements of Greater Geelong Planning Scheme for solar access.

Attachments

1. Highton UDF [2.2.1 - 72 pages]
2. Highton UDF Feedback Summary Report [2.2.2 - 7 pages]



THE CITY OF
GREATER GEELONG

HIGHTON VILLAGE

URBAN DESIGN FRAMEWORK

OCTOBER 2022
FINAL

ACKNOWLEDGEMENT OF COUNTRY

The City of Greater Geelong acknowledges the Wadawurrung People as the Traditional Owners of this land.

It also acknowledges all other Aboriginal and Torres Strait Islander People who are part of the Greater Geelong community today.

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01

INTRODUCTION

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Purpose

Background

An Urban Design Framework (UDF) uses words, plans and drawings to show how a precinct can grow sustainably.

The centre is thriving; the great businesses, the lovely treed character and unique shopping and dining experiences attract customers from far and wide. With this success brings challenges and opportunities for improvement. It has been many years since the streetscapes in Highton Village have been updated and some infrastructure is old and tired and presents hazards for walkers that need attention.

Without an overall plan for investment, upgrades and maintenance of infrastructure will be replaced in an ad hoc way.

Content

In this UDF, you will find:

- A community vision for the future Highton Village
- An overall plan to guide future investment and development
- Concepts for streetscape upgrade works
- An access and movement plan for vehicles, walkers and cyclists
- A staged and costed implementation plan
- Planning guidelines for new developments within and around the centre to enhance the village character

Format

This UDF is structured in six chapters:

Chapter 1 – Introduction

Chapter 2 – Developing the UDF

Chapter 3 – Existing conditions and opportunities

Chapter 4 – Urban Design Framework

Chapter 5 – Movement and access

Chapter 6 – Implementation

Site Context

Highton Village is located 5km from Central Geelong on Barrabool Road, which is the primary movement corridor between Central Geelong, the Barwon River and the Geelong Ring Road. Roslyn Road to the south provides access to Belmont High Street 1.5km to the east.

Highton has experienced strong population growth from 2001 to 2011 with an annual population change of 2.6% over the period. A higher rate of growth than Geelong overall. In 2016, the total population of Highton-Wandana Heights - Ceres was estimated to be 21,244 people. It is expected to increase by over 2,600 people to 23,906 by 2026. This is based on an increase of over 1,600 households during the period. Population growth after 2025 will steady as the new housing estates to the west of the village are completed.

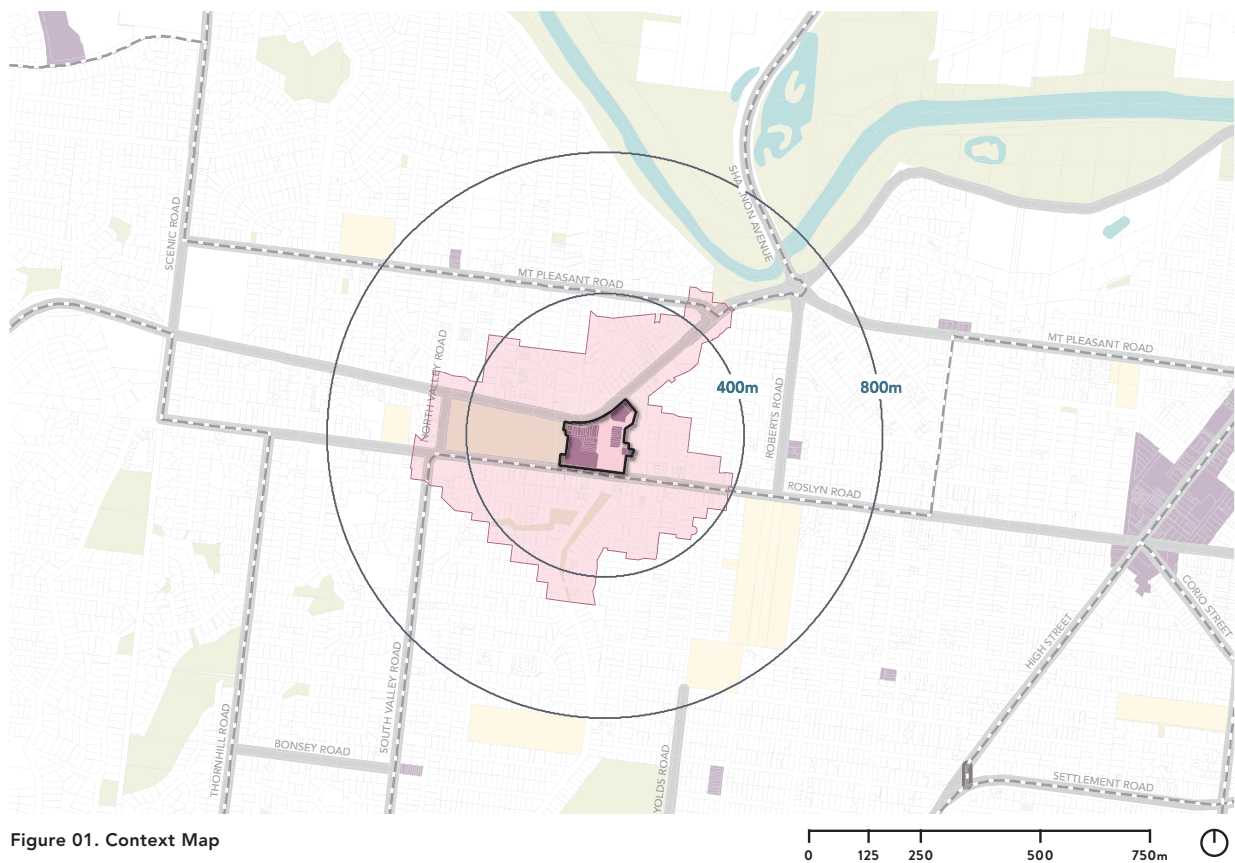


Figure 01. Context Map

KEY

- | | | |
|-------------|------------------------|----------------------------------|
| Study Area | Property boundary | Increased Housing Diversity Area |
| Major Roads | Waterbodies | Open Space |
| Bus Routes | Shopping Centre | |
| 400/800m | Educational Facilities | |

Role of Highton Village

The thriving centre plays a local day-to-day retail and hospitality role within Highton. Given that the nearby much larger centres of Waurin Ponds and Belmont cover the wider catchment, Highton will continue to play a local, neighbourhood centre role.

Retail

The Centre currently comprises 7,000sqm of retail floor space (see Table 1). The high quality environment, existing mix of unique and independent stores offer Highton Village the opportunity to continue to capitalise on the trend in retailing and consumer behaviour to seek 'experiences' rather than just a market place for goods and services.

The Geelong Retail Strategy identifies the need for an additional 1,000sqm of floorspace in the Highton Village by 2031 to meet the shopping needs of the community.

This UDF identifies how this additional floorspace could be provided.

TABLE 1. Existing floorspace

FLOORSPACE	SQM
Supermarket	2,200
Speciality Food	1,700
Speciality Other	2,100
Hospitality	1,000
TOTAL RETAIL	7,000



Figure 02. Study Area

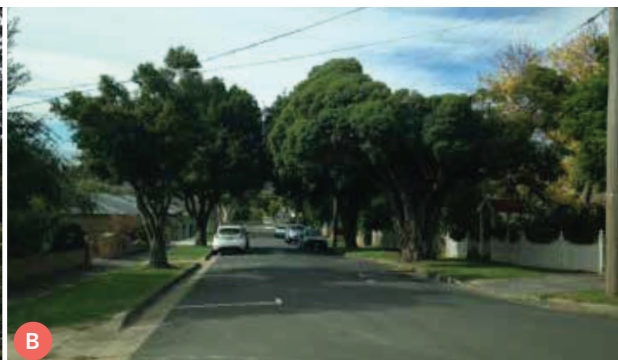
KEY

- Study Area
- Major Roads
- 400/800m
- Property Boundary
- Waterbodies
- Shopping Centre
- Open Space
- Increased Housing Diversity Area
- Community Facilities

Increased Housing Diversity

The Highton Increased Housing Diversity Area (IHDA) is located within the walkable catchment of the Highton Village activity centre and is earmarked to accommodate residential growth and increased housing diversity including medium density housing, townhouses, units and other multi-dwelling developments.

Currently, this area comprises of low site coverage, established gardens, low front fences and sloping topography. Mature trees are a prominent feature on the ridge line to the south of Barrabool Road. The IHDA is within the General Residential Zone - Schedule 4 and is also influenced by a Special Buildings Overlay in some areas. Clause 22.63 - Increased Housing Diversity Areas require the design of new buildings to respond to the unique attributes of the local context. In this aspect, the current Policy and Zoning provisions are appropriate to deliver the housing diversity objectives. Additional guidelines pertain to response to topography including encouraging buildings that follow the natural slope of the land and reduce the need for site excavation or filling to ensure buildings do not dominate long distance views or the pedestrian experience.



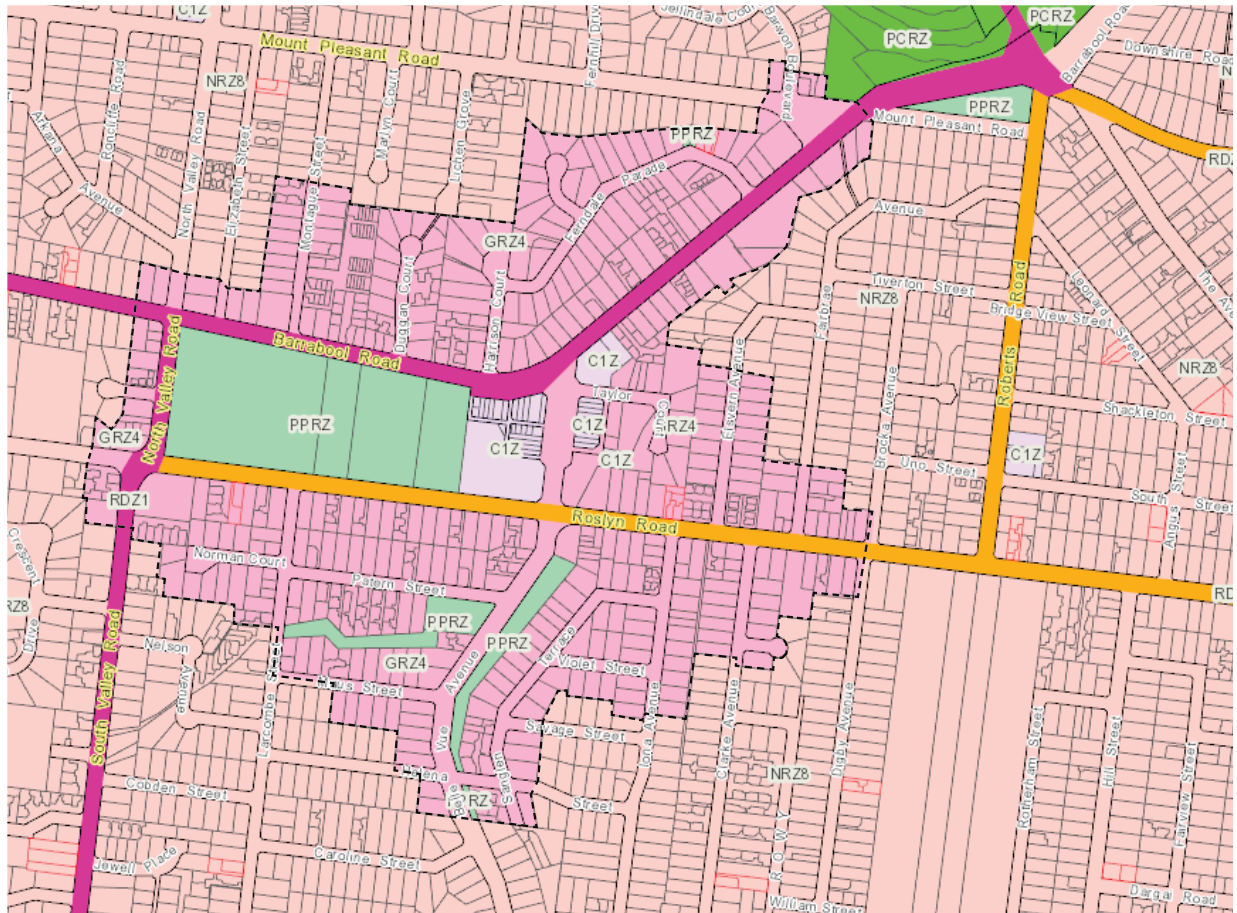


Figure 3. Existing Zones

KEY

- | | |
|--|---|
| General Residential Zone 4 | Road Zone Category 1 |
| Neighbourhood Residential Zone 8 | Road Zone Category 2 |
| Commercial 1 Zone | Increased Housing Diversity Area |
| Public Conservation And Resource Zone | |
| Public Park And Recreation Zone | |

02

EXISTING CONDITIONS & OPPORTUNITIES

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Existing Conditions

The established fine grain character of Highton Village is supported by public realm amenity. In order to better understand various attributes that may require upgrades, a series of studies were undertaken including pedestrian counts, pedestrian amenity analysis and urban quality analysis.



Figure 04. Urban Quality Map

KEY

- Retail Core Study Area
- Active Street Frontage
- Potential For Interface Improvement
- Tree In Public Realm



Figure 05. Pedestrian Volumes during Weekday Afternoon Peak Hour (with % of 'At Risk' Pedestrians)

The pedestrian movement surveys (undertaken by PFC Services) were undertaken on various days throughout the Village as follows:

- (i) Thursday 06/10/16, 3:00-6:00pm at the Roslyn Road roundabout,
- (ii) Thursday 13/10/16, 3:00-4:30pm along Belle Vue Avenue,
- (iii) Wednesday 08/06/16, 3:00-4:30pm along Village Walk. The pedestrian peak hour was 3:30-4:30pm

KEY

- Peak Volume Pedestrian Corridor
- Loading Bay / Pedestrian Conflict Point



Figure 06. Pedestrian Movement Map

Albeit a good network of pedestrian connections, there exists gaps within the internal laneways. Additionally pedestrian movement at the round-a-about problematic.

KEY

- Retail Core Study Area
- Main Village Path
- Local Footpath
- Path Through Park
- Crossing
- Desired Path

Analysis and Opportunities



The following describes the key elements of the existing conditions and their implications.

Connection to the Community

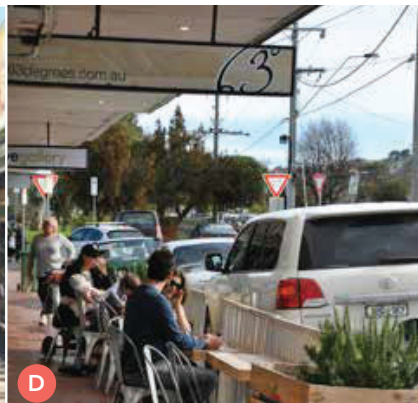
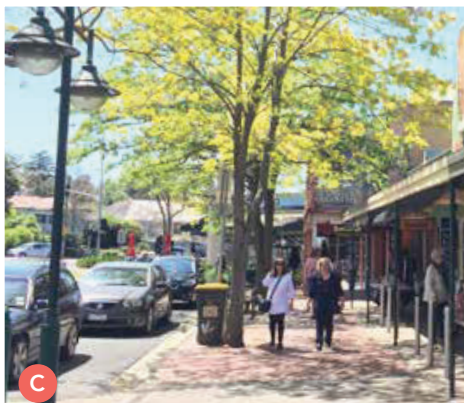
WHAT IS WORKING WELL	WHAT NEEDS IMPROVEMENT
<ul style="list-style-type: none"> • Hub of the community • Traders and shoppers know each other • Highton is like a little country community where everyone says hello • Community events – Christmas festival; raffles etc. – involving local schools, fire brigade, scouts, local organisations and groups (but running them is demanding on volunteers) • Lots of visitors come to the sporting facilities • Independent stores that are unique to Highton Village • No vacancies in the stores • Traders develop relationships with customers • The Village enjoys a reputation as having ‘the best of everything’ and high quality goods and services • The Village is a destination, not just a shopping centre • Attracting visitors from other suburbs • Harnessing the rise of the ‘coffee culture’ in consumer behaviour • Has a Woolworths store that meets everyday grocery needs 	<ul style="list-style-type: none"> • No-man’s land after normal business hours with limited natural surveillance and a limited evening economy • Limited functions, activities and spaces for young people within the Village





Character & Urban Design

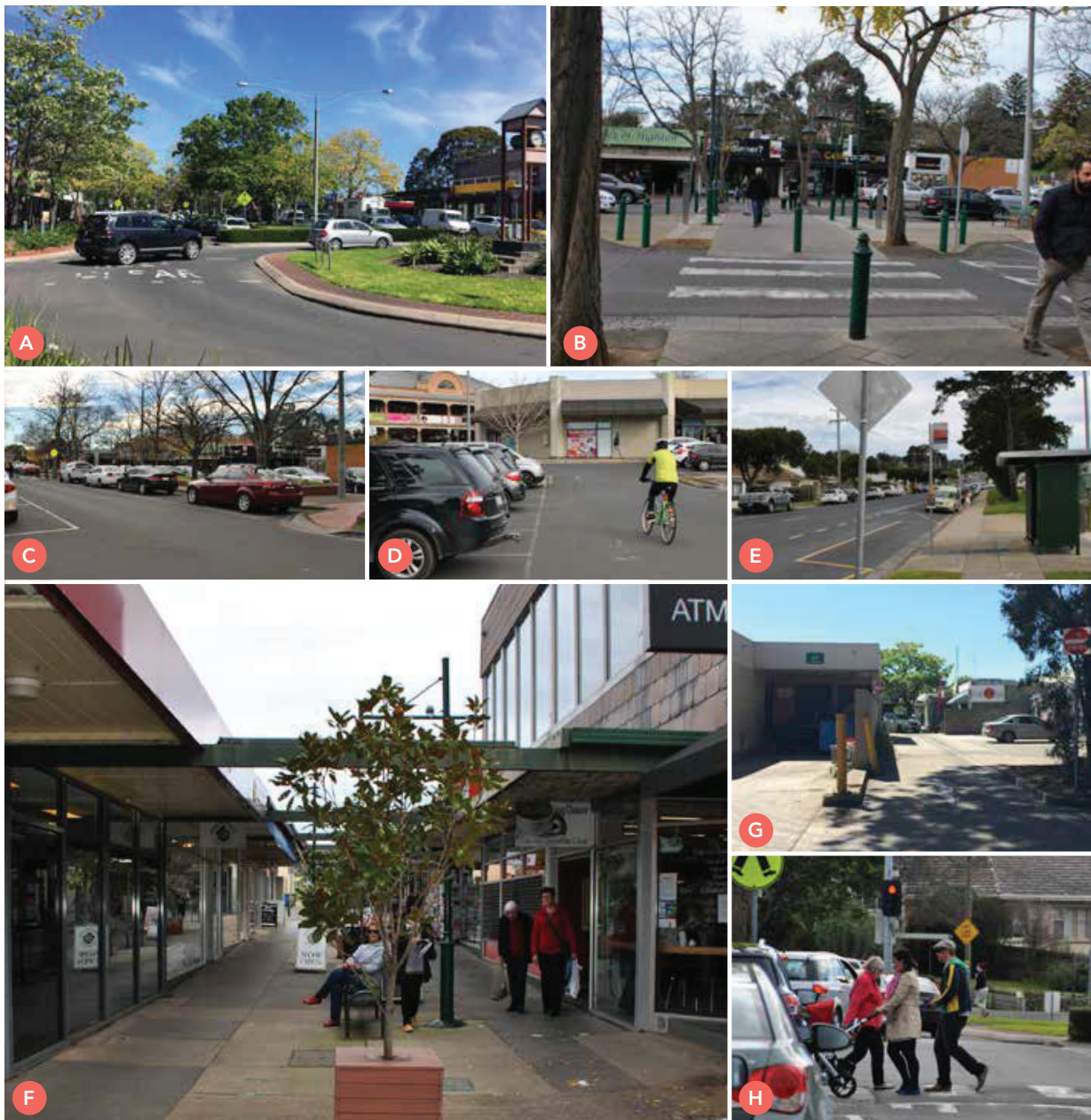
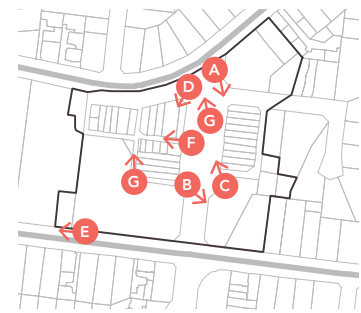
WHAT IS WORKING WELL	WHAT NEEDS IMPROVEMENT
<ul style="list-style-type: none"> • Mature trees and greenery contribute to the amenity of the area and provide shade • Unique character - High Street feel • Fairy lights on all year round • Lots of awnings provide shelter from the rain and summer sun • Compact and dense centre buzzing with activity 	<ul style="list-style-type: none"> • Backs of shops and loading bay fronting key pedestrian linkages • No housing in the centre despite it being a highly accessible place • The lack of investment in the public realm • Unused road space and large areas of asphalt • Core retail stock is old and outdated (hidden mostly by advertising signage) • Street furniture (e.g. benches, seats, tables, bicycle racks, bins, planter boxes, bollards and street lights) are old and outdated • Use of different pavement materials and finishes providing inconsistent public spaces • The Village Walk and Belle Vue Arcade is looking tired • Some awning structures are looking old (possibly 30 years old beyond the design life) • Back lane has become a waste management station (around 20 bins/ smells) • Greenery that represents the character of the Village isn't found on Barrabool Road frontage



Analysis and Opportunities

Movement and Connections

WHAT IS WORKING WELL	WHAT NEEDS IMPROVEMENT
<ul style="list-style-type: none"> • For the most part, traffic volumes are within manageable levels for each roadway's functionality • The Village is relatively compact, so destinations are a convenient walk from the majority of car parking facilities. • Vehicle speeds along Belle Vue Avenue are relatively low • With low vehicle speeds, priority crossings and a short crossing distance, there is good pedestrian amenity for movements across Belle Vue Avenue • Council management of Village parking sees public car parking restrictions consistent with best practice where short term (high turnover) parking is provided within the commercial core and longer term parking is provided around the periphery • The pedestrian path network provides good connectivity and permeability throughout the Village • Bus stops are located within a convenient walking distance to the Village centre. 	<ul style="list-style-type: none"> • Barrabool and Roslyn Roads are a major pedestrian barrier due to high traffic volumes and speeds • The bus stop from Geelong Station is across Roslyn Road • The Village is close to the River and Highton Reserve but the connections are not clear and access for pedestrians and cyclists are not easy • There are a lot of vehicular movement throughout the village and many different parking areas making for unsafe road environment for children and elderly • Traffic island at Barrabool Road, Taylor Court and Belle Vue Avenue has confusing markings and is not really a roundabout but looks like one • Some areas of the pavers are trip hazards • Pedestrian connection from the Village Walk to Woolworths is very poor with car park and loading bay creating a pedestrian barrier • Rear laneway along Belle Vue Arcade and Village Walk is the only connection between Barrabool Road parking and Woolworths parking areas • Often difficult to find a car park • Striking the right balance between long stay and short stay parking • Infrequent enforcement of parking restrictions • High demand for on-street parking from sporting activities in the Reserve • Traders using short term spaces



03

DEVELOPING THE UDF

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Research and Consultation

Research

A number of steps were undertaken as part of the research including:

- Conduct a review of existing plans, policies, strategies and infrastructure programs.
- Perform a systematic analysis of the study area’s use patterns, movement patterns, urban form and structure.
- Conduct an analysis of local strengths, weaknesses, opportunities and threats.
- Prepare a draft urban design framework report.

Consultation

The timeline and engagement with the community and other stakeholders is presented in Figure 7 below.

The Shape Highton Village workshop was undertaken in November 2016 with residents, businesses, community associations, schools and government agencies to start a masterplan for the future of the Highton Village. A total of 130 representatives gathered at the Highton Bowls Club to uncover the community aspirations for the Highton Village and discuss ideas presented by the Design Team. The workshop was designed and conducted by independent consultation specialists.

The city conducted two workshops with the trader group, in October 2017 and May 2021. The workshops focused on public realm options within the Village. The discussion was framed around proposed changes to movement and access, uses of spaces and look and feel.

Consultation was also undertaken with key stakeholders, including Regional Roads Victoria and local businesses.

Formal submissions were received on the Draft UDF in 2017 and Interim Final UDF in 2021, through the Have Your Say webpage on the City’s website.

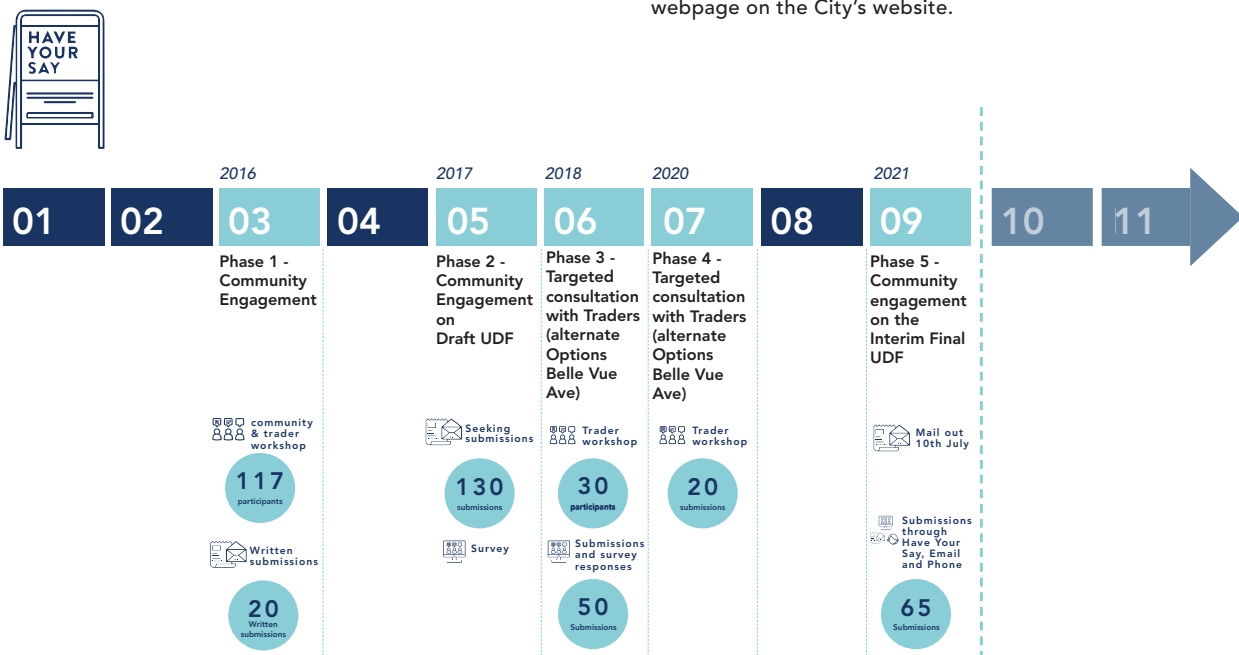


Figure 07. Consultation Snapshot

What we heard

The consistent themes that were identified from the recording of the discussion and written comments are:

Parking	Parking was the most frequently mentioned topic. More is needed - this could be provided within the Reserve, southern part of Bell Vue Avenue in the drainage reserve or in a multi deck car park. There is a need to encourage workers to park out of the centre to free up spaces for shoppers. Angle parking in the shopping area of Belle Vue will impact on traffic flow and safety.
Traffic improvements	Traffic improvements to Barrabool Road traffic signals to improve traffic flow is supported. Need to consider where right hand turns are appropriate within the Village centre to improve traffic flow and safety. Removing or altering the non-roundabout roundabout is necessary to improve safety.
Rear Laneway	Rear laneway behind Woolworths should be closed or the appearance improved. Support from many participants to relocate the Woolworths loading bay and create an alternative access road between Barrabool Road and Roslyn Road.
Safety	Safety of walkers and cyclists is very important and needs improvement.
Residential Apartments	Residential apartments in the centre will impact on the Village character.
Council Car Park Site	Redevelopment of Council car park site with retail, commercial, apartments and multi-deck car parking is supported by many participants.
Public Spaces	Public spaces, play areas, meeting spaces and spaces for young people are limited and should be increased. The library service should be retained but not necessarily in the existing building.
Central Median	Central median space in Belle Vue Avenue should be better utilised. Alfresco dining and after hours trading should be encouraged. Pop-up activities such as retail, events, markets and food trucks to create activity in public spaces are an opportunity for the Village.
Uniformed Pavement Surface	Uniformed pavement surface and removal of tripping hazards was strongly supported. Warm and natural materials should be used for pavements.
Village Walk	Pedestrian areas should be upgraded. A covered walkway was a popular suggestion.
Lighting	Lighting upgrades for functional and decorative purposes are needed. Seating, bins, toilets and water fountains should be expanded and upgraded.
Trees	Trees and greenery are important to the existing character and should be retained and enhanced.
Public art	Public art and local history references in the streetscape upgrades are important.

Vision

Highton Village is and will be the heart of the community with a unique, vibrant, leafy feel. High quality public spaces will encourage visitors to linger longer and promote community interaction. The movement network and design of buildings will provide for a range of activities and prioritise the pedestrian experience.

Urban Design Principles

The vision statement is supported by the following overarching principles which underpin the design responses set out later in this document.

- | | |
|---|---|
| <p>1</p>  <hr/> <p>Retain and enhance the tree canopy and greenery of the Village</p> | <p>2</p>  <hr/> <p>Create new public spaces in the Village that are flexible and suit different uses, ages and seasons.</p> |
| <p>3</p>  <hr/> <p>Celebrate the valued local village character that makes the Highton Village as a special place to live and visit.</p> | <p>4</p>  <hr/> <p>Enhance Highton Village as a unique destination for local shopping and recreation.</p> |
| <p>5</p>  <hr/> <p>Improve pedestrian and cyclist safety and amenity when designing new streets and buildings.</p> | <p>6</p>  <hr/> <p>Encourage Environmentally Sustainable Design (ESD) in the architecture and design of buildings and public spaces.</p> |



Figure 08. Artist Impression - Belle Vue Avenue

04

URBAN DESIGN FRAMEWORK

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Framework

The primary recommendations and projects identified in the Urban Design Framework are:

1

Consolidate commercial land uses within the retail core to promote a compact village centre.

2

Promote high quality mixed use development up to four storeys that respects the low scale built form of the Village.

3

In the long term, redevelop the northern portion of the Council car park for a mixed use development.

4

Redesign the junction of Belle Vue Avenue and Barrabool Road to reduce vehicle conflicts.














5

Undertake streetscape upgrade works in Barrabool Road service lane, Village Walk, Belle Vue Arcade, the rear laneway and Belle Vue Avenue to emphasise pedestrian routes and create a unified material palette throughout the village.



Figure 09. Framework Plan









KEY

- | | | |
|--|--|--|
| <ul style="list-style-type: none">  Landmark corner point  The existing Highton Library retained and its long term future options reviewed in 5 years.  Intersection upgrade  Expanded public median space | <ul style="list-style-type: none">  Potential development sites  Potential ground floor retail  Active frontages  Vehicle movements  Rear access & loading | <ul style="list-style-type: none">  Key Pedestrian Routes  Secondary pedestrian routes  Secondary pedestrian routes through future development site - flexible location  Residential interface |
|--|--|--|

Concept Plan

KEY

- 1 Existing field
- 2 Belle Vue Avenue reconfiguration - refer to following detail concept
- 3 Laneway upgrade - refer to following detail concept
- 4 Service lane upgrade
- 5 Barrabool Service Rd car park reconfiguration
- 6 Potential development site to include retail, office, residential, possible community uses.
- 7 Potential for longer term on street parking
- 8 Improve pedestrian connections
- 9 Barrabool Rd bike lanes
- 10 Intersection upgrade
*Configuration to be determined with Council & Regional Roads Victoria
- 11 The existing Highton Library retained and its long term future options reviewed in 5 years

-  Existing trees (dotted centre)
-  Proposed trees (crossed centre)
-  Garden bed
-  New pavement - e.g. concrete with saw cuts to help differentiate space
-  New furniture and fixtures -e.g. lighting, bollards, bins, incorporated seating, benches and bike racks
-  Pedestrian crossings
-  Potential development site subject to feasibility study
-  Opportunities for local artists engagement
 - murals for blank building facades - art piece, history or aid in wayfinding
 - wayfinding signage
 - pavement 'tapestry' within laneways and in areas of belle vue avenue
 - childrens play elements weaved into the public realm

- Note:
- All potential building modifications or redevelopments are subject to coordination with owners.
 - Highton Flood Study identifies areas of high hazard flooding within the precinct. Public realm works offer opportunities to improve the situation, particularly for the car park adjacent to Barrabool Road/Highton Reserve, and Taylor Court.



Figure 10. Highton Village Streetscape Concept Plan



Streetscape Projects

There are four strategic streetscape projects identified that can be implemented in a staged manner to manage impact on traders and meet the City's budget cycles.

-
- 1 Belle Vue Avenue
 - 2A Belle Vue Arcade and Village Walk
 - 2B Service Lane
 - 3 Barrabool Road

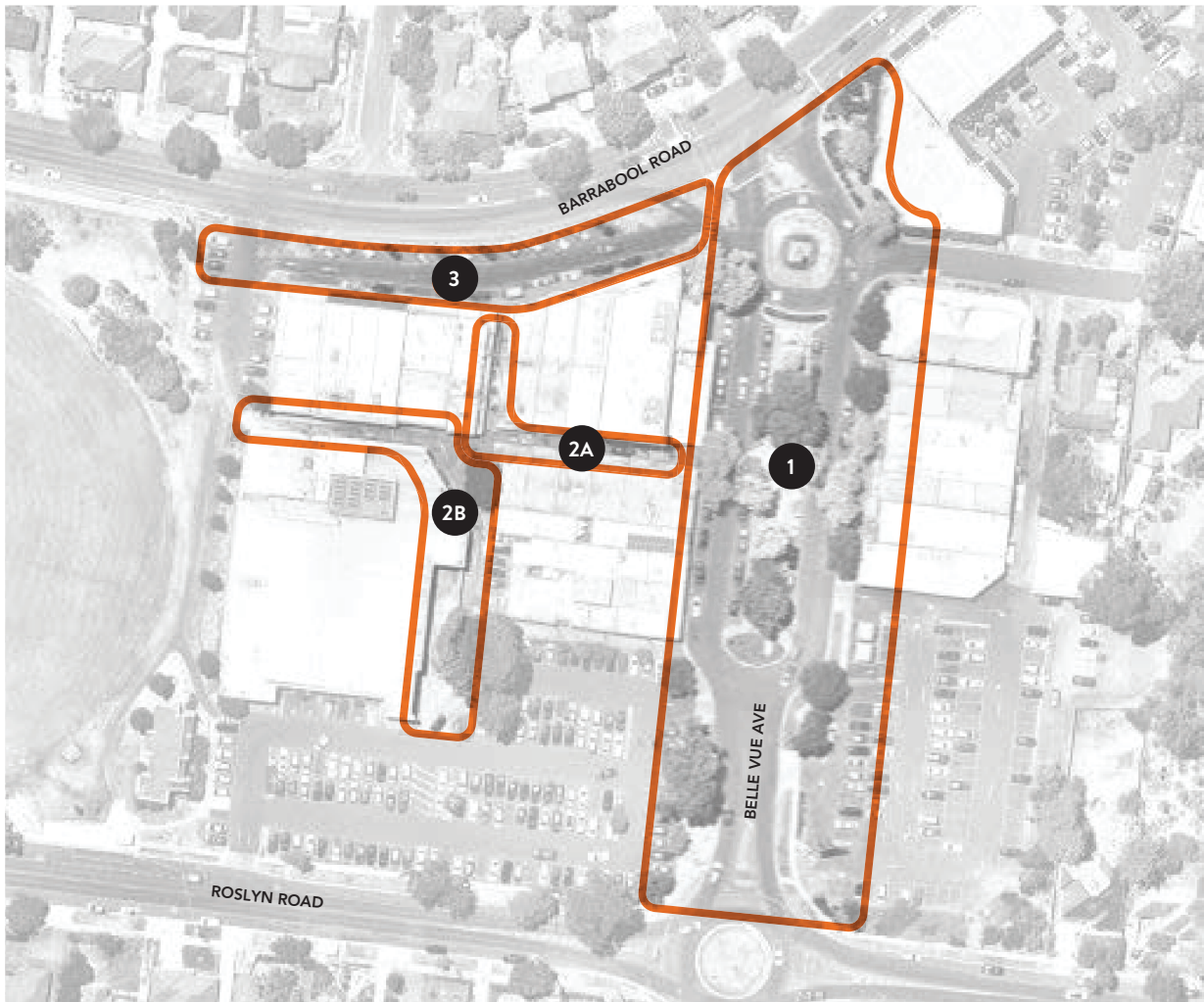


Figure 11. Staging Plan



Project 1. Belle Vue Avenue

Belle Vue Avenue streetscape plan maintains the capacity of the road network (one lane each way), low impact of car parking supply (4 less spaces) and retains existing robina pseudocacia canopy trees in raised planters with inbuilt seating.

The design allows for more planting, seating, cycle parking and alfresco dining. The pedestrian linkage from Belle Vue Arcade to Belle Vue Avenue is maintained and enhanced. Two additional pedestrian crossings are proposed at Taylor Court and Barrabool Road service lane. A 30km/ h speed limit will further enhance pedestrian amenity and cyclist safety.

Over the course of the urban design framework development four options were developed and tested with the community and Trader Group.

- Cross section 1 - Retain the existing conditions
- Cross section 2 - Locates vehicular traffic to the western side creating a large public space
- Cross section 3 - Maintain a median and divided carriageways and provide angle parking
- Cross section 4 - Maintain a median and divided carriageways and reduce carriageway widths to create a larger median public space

All upgrade options provide a traffic lane in each direction and a parking lane in each direction. They therefore do not represent a major change to the vehicular through carrying function of the roadway.

Each option was considered against the objectives identified earlier.

Table 2. Belle Vue Avenue Options Assessment

CRITERIA	CROSS SECTION 1	CROSS SECTION 2	CROSS SECTION 3	CROSS SECTION 4
Increase in Useable Public Space	X	✓	X	✓
Prioritise the Needs of Pedestrians Over Cars	X	✓	X	✓
Increases Parking Supply	-	X	✓	X
Improve Cyclist Safety	-	X	X	✓
Activation Of Adjoining uses Through Street Trading and Alfresco Dining	-	✓	X	X
Retention of Existing Vegetation	✓	✓	✓	✓

The option that best achieves the community objectives and delivers the most benefit from a substantial investment by the City is the creation of an exciting new public plaza on one side of the road way – Option 2.

This layout maintains the capacity of the road network (still one lane each way), moderate impact on parking supply (11 less spaces) and retains most of the existing Robinia pseudoacacia canopy in raised planters with inbuilt seating.

The design allows for more planting, seating, cycle parking, children’s play areas and alfresco dining. The public space is proposed on the east side of the road as this allows the most opportunity for interaction with the existing businesses through street trading and alfresco dining and good solar orientation for afternoon sun.

The Trader Group however had significant concerns with the proposal to reduce car parking within the Village, particularly on Belle Vue Ave (Option 2).

Following a series of workshops with the Trader Group it was agreed that Option 1, which utilises the existing cross section would be further developed.

The concept design shown in Figure 7 represents the compromise reached between minimal loss of car parking (4 less spaces on Belle Vue Ave), limited changes to traffic flow and provides public realm improvements, such as new pavements, planting and furniture.



Figure 12. Artist Impression - Belle Vue Avenue





KEY









-  Existing trees (dotted centre)
-  Proposed trees (crossed centre)
-  Garden bed
-  New pavement - e.g. concrete with saw cuts to help differentiate space
-  New furniture and fixtures -e.g. lighting, bollards, bins, incorporated seating, benches and bike racks
-  Pedestrian crossings
-  Potential development site subject to feasibility study
-  Opportunities for local artists engagement
 - murals for blank building facades - art piece, history or aid in wayfinding
 - wayfinding signage
 - pavement 'tapestry' within laneways and in areas of belle vue avenue
 - childrens play elements weaved into the public realm

Figure 13. Belle Vue Avenue Streetscape Plan



KEY DESIGN PRINCIPLES:

- Consistency of materials
- Prioritise pedestrian movement through pavement hierarchy
- Adaptable and dynamic spaces for a variety of uses to suit the needs of the community, events, trading and alfresco dining opportunities
- Improve Barrabool Road/Belle Vue Road intersection for all users

INDIVIDUAL AREAS

- 1 Clear pedestrian access for shop frontages defined by sawcuts
- 2 Street trading area
- 2a Pergola structures with climbers and small garden beds. Creates green cover and can be adapted to suit shop needs e.g. lighting, heaters etc.
- 3 Raised pedestrian crossings
- 4 Raised lawn with concrete/timber seating edge
- 5 Garden beds around existing and proposed trees to green pedestrian zone and soften road edge
- 6 Clock removed and alternative civic monument or art installed in public space

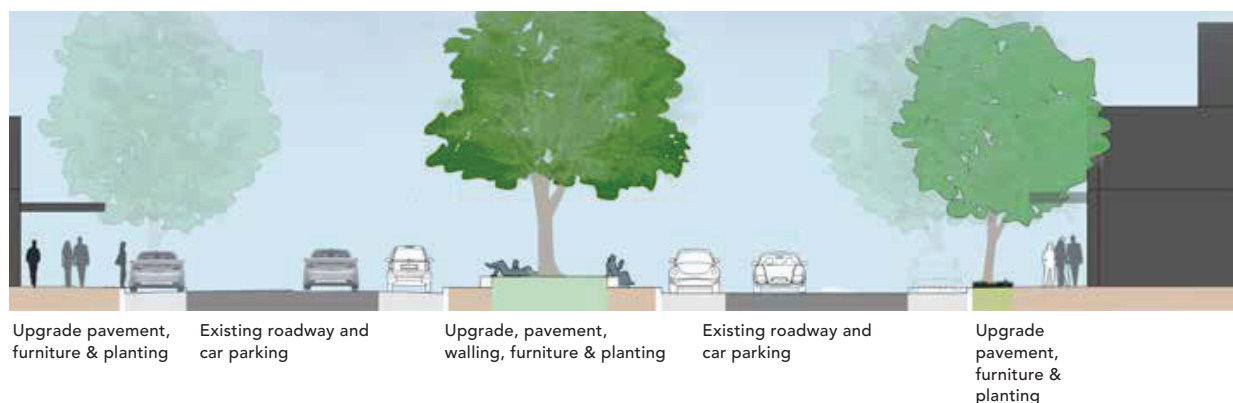


Figure 14 . Belle Vue Avenue Indicative Streetscape Section

Project 2a. Belle Vue Arcade and Village Walk

The existing materials and tired conditions of Belle Vue Arcade and Village Walk do not do justice to the high quality mix of businesses and pedestrian quality in the laneways

To rectify this situation the fine grain character of the laneways will be retained. The pavement will be replaced to match the new materials in Belle Vue Avenue.

The existing steel beams will be integrated into a pergola structure that can be adapted to suit each of the different shops i.e. lighting, weather protection and on street dining.

The pergolas provide opportunities for garden beds, climbing plants and unique paving created by a local artist to brighten the laneway and soften the feel of the space.

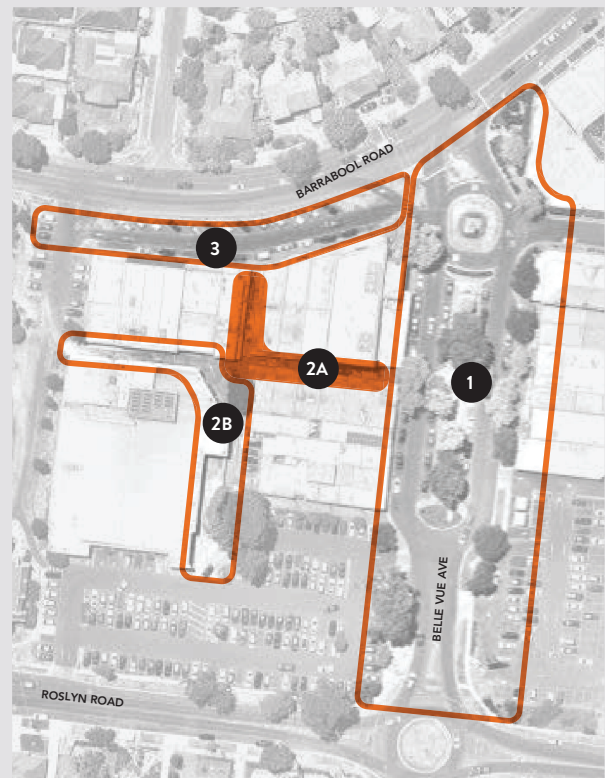


Figure 15. Belle Vue Avenue Arcade & Village Walk project Location

KEY

- 1** Belle Vue Avenue
- 2A** Belle Vue Arcade and Village Walk
- 2B** Service Lane
- 3** Barrabool Road



Figure 16. Artist impression Belle Vue Arcade and Village Walk



Figure 17. Belle Vue Arcade and Village Walk Concept Plan

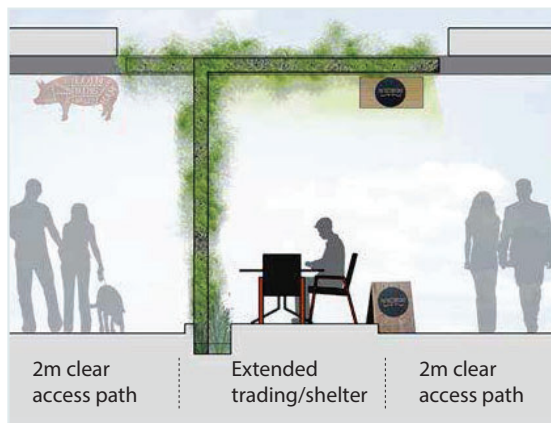


Figure 18. Concept Section B-B

KEY



Proposed Trees (Crossed Centre)



Pergola and Vine Planting



New Pavement - e.g. Concrete With Saw Cuts to Help Differentiate Space



New Furniture And Fixtures - e.g. Lighting, Bollards, Bins, Incorporated Seating, Benches and Bike Racks



'Tapestry' Pavement



Opportunities for Local Artists Engagement

KEY DESIGN PRINCIPLES:

- Source of enclosure with adaptable pergola structure. Opportunity for heating, lighting, planting, signage, shade and rain shelter
- Brighten laneway with vegetation to pergola structure and garden beds to soften space
- Consistency of materials to match village centre but with fine grained character

INDIVIDUAL AREAS

- 1 Clear pedestrian shop frontages defined by frequency of sawcuts
- 2 Plaza space with furnishings and fixtures to create a link between village lanes to promote pedestrian movements
- 3 'Tapestry' pavement design to be developed with local artists for extended trading area and to bring colour and interest to the lane
- 4 Pergola structures to integrate into existing steel beams with climbers and small garden beds. Creates cover and can be adapted to suit shop fronts e.g. lighting, heaters etc.

Project 2b. Service Lane

The service lane is a key pedestrian route but the current environment offers extremely low amenity for pedestrians.

The existing pavement will be replaced with a single grade pavement that clearly identifies pedestrian and vehicle spaces through the use of unique saw cuts in the pavement. A best practice 10km/h shared zone will be implemented.

The existing unused bin structure will be removed and an attractive screen installed on the north east wall where the bins are currently stored.

The wall on the north side of the supermarket offers the opportunity to create a vertical garden to soften the blank wall and further enhance pedestrian amenity.

Vehicle access will be maintained through the area and access to existing car parking areas retained.

Best practice design elements for shared laneways include a surface treatment which has the look of a pedestrian path, being at the same level as adjacent footpaths.

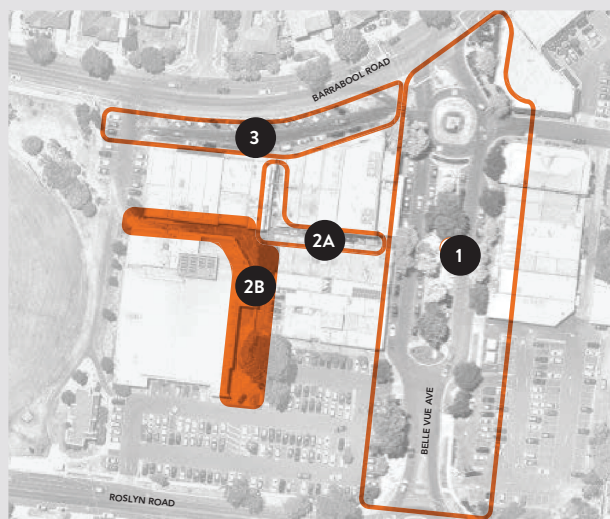


Figure 19. Service Lane Project Location

DESIGN PRINCIPLES:

- Consistency of materials to match Village centre but with fine grained character
- Prioritise pedestrian movement through increased safety, encourage/inviting laneway character and movement with at grade, trip free pavement giving hierarchy through increased concrete saw cuts for pedestrian movement
- Vertical treatment to soften Woolworths rear wall and screen bin storage with green wall and timber cladding

INDIVIDUAL AREAS:

- 1 Clear pedestrian shop frontages - defined by frequency of saw cuts
- 2 Green wall and timber cladding to soften facade
- 3 Plaza space with furnishings and fixtures to create a link between village lanes to promote pedestrian movements
- 4 Bin storage screened with timber cladding
- 5 Existing loading bay
- 6 Private parking

KEY:





-  Proposed trees
-  Garden bed
-  New pavement - e.g. concrete with saw cuts to help differentiate space
-  New furniture and fixtures e.g. lighting, bollards, bins, incorporated seating, benches and bike racks



Figure 20. Service Lane Concept Plan



Figure 21. Green Wall Elevation C-C

The supermarket rear facade softened/broken through the use of a green wall installation and integrating timber cladding to add variety and texture while wrapping corner to screen the bin storage area.

Note - elevation design indicative, scale, arrangement and proportions to be developed further and can be adapted to suit other blank walls within the Village.

Project 3. Barrabool Road

It is proposed to reconfigure the Barrabool Road service lane to provide a single row of angle parking spaces adjoining the footpath. In doing so, the footpath can be widened to allow for on-street dining, street tree planting and more seating.

This new parking arrangement will mean pedestrians will not have to cross the road to access their vehicles.

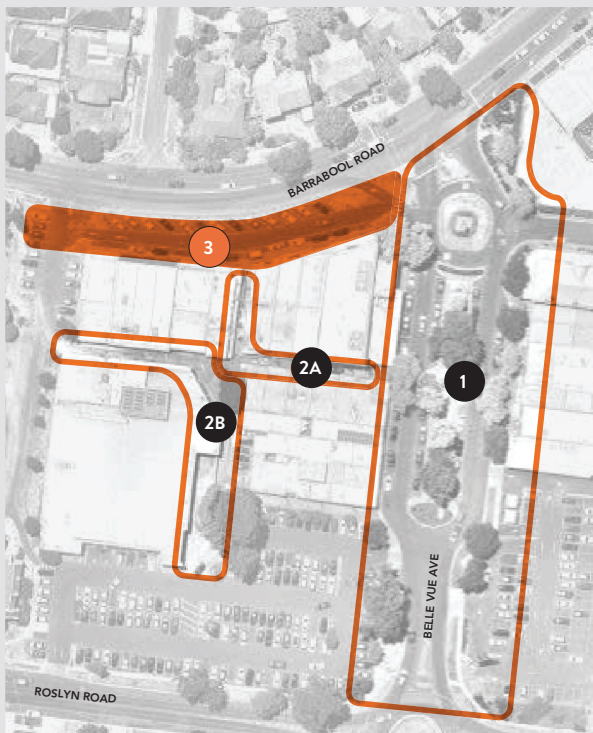


Figure 22. Service Lane Project Location

KEY

- 1 Belle Vue Avenue
- 2A Belle Vue Arcade and Village Walk
- 2B Service Lane
- 3 Barrabool Road



Figure 23. Artist's Impression – Barrabool Road and Carpark



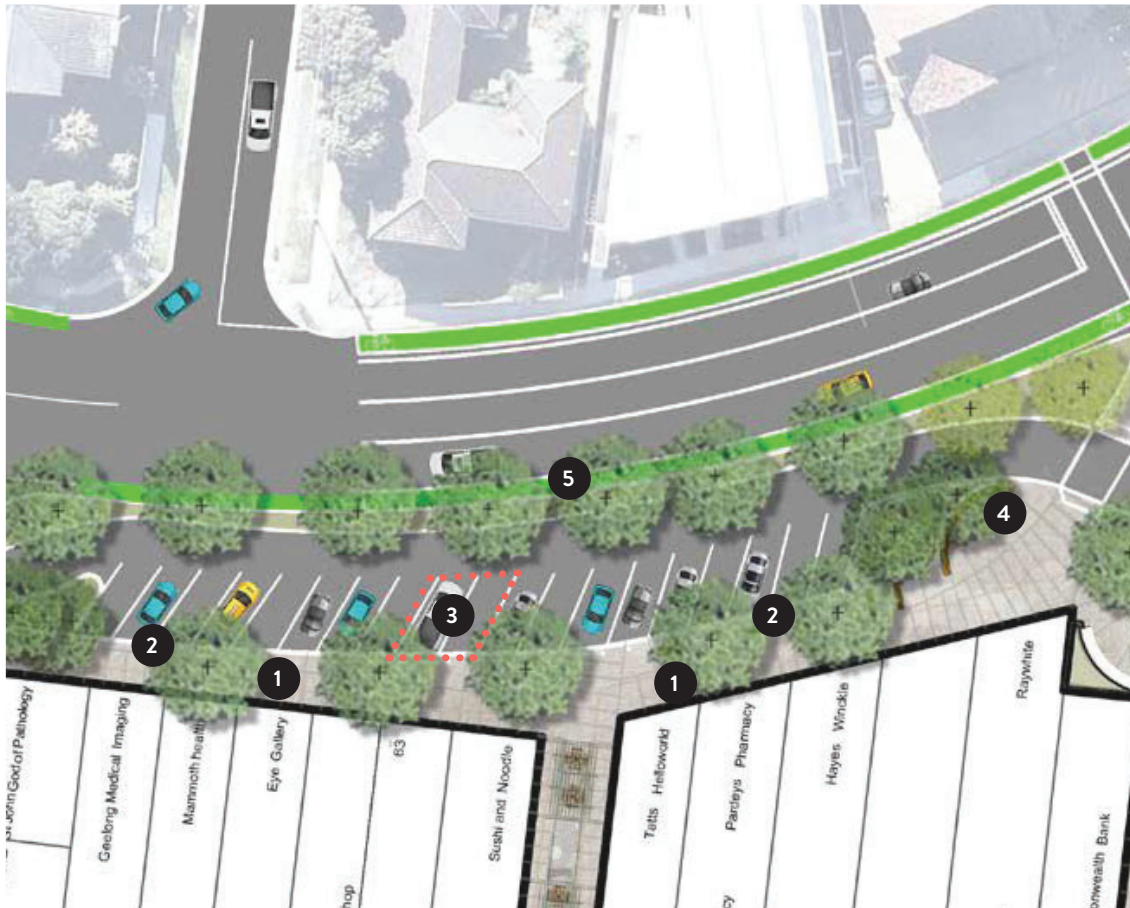


Figure 24. Barrabool Road Service Lane Concept Plan

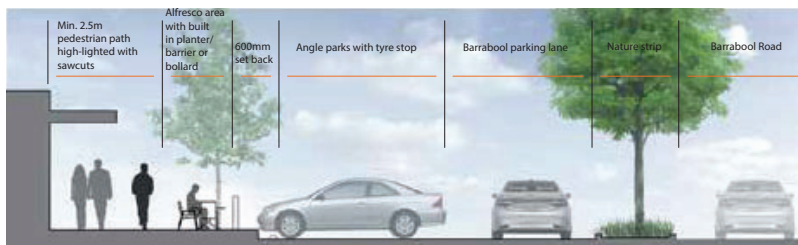






Figure 25. Barrabool Serive Road Section DD

KEY

	Proposed Trees (Crossed Centre)
	Garden Bed
	New concrete pavement with decorative saw cuts
	New Furniture And Fixtures - e.g. Lighting, Bollards, Bins, Incorporated Seating, Benches and Bike Racks

KEY DESIGN PRINCIPLES:

The proposed reconfiguration of Barrabool Rd carpark removes the existing parallel and angle spaces and replaces them with one row of angle spaces along the footpath edge.

This allows footpath widening for extended trading, alfresco dining, street tree planting, seating and other furnishings. Pedestrians are also no longer required to cross the road to access their vehicles.

- Provide opportunities for on street trading
- Consistency of materials to match village centre
- Introduce low planting and trees to break the scale of Barrabool Rd pavement giving hierarchy through increased concrete saw cuts for pedestrian movement
- Vertical treatment to soften woolworths rear wall and screen bin storage with green wall and timber cladding

INDIVIDUAL AREAS

- 1 Pedestrian path emphasised with decorative saw cuts
- 2 Extended alfresco dining, bike storage, planting and seating space
- 3 Potential parklet location (dependent on council and trader needs)
- 4 Meeting/resting place with seating and garden beds
- 5 Street trees and low shrub planting to nature strip behind vehicle barrier



Figure 26. Barrabool Service Road Section DD - Parklet Option



Parklet Precedent

Materials

The above shows an indicative material palette for the overall concept of Highton Village.

The simple use of minimal pavements such as in situ concrete and bluestone highlights, compliment but contrast with the vehicular asphalt road highlighting pedestrian priority.

Through the introduction of stainless steel, blackened steel, colour highlights and timber in street furniture and fixtures, a new character of the Village will emerge.

Using local artists to help create a new identity will meld continuity and diversity. This will bring interest and vitality and a renewed sense of place.



Figure 27. Overall Material Palette (image source: Outlines Landscape Architecture)

Furniture

Incorporated Furniture

Incorporate simple yet sophisticated timber elements into the built structure of the village centre. Grounds and integrate within the larger hard elements. The contrasting materials of timber, concrete and steel compliment and highlight key moments.



Free Standing Furniture

Simple, sophisticated and contemporary style. Some flexibility in form to create subtle variation - coupled with groupings and placement to work with the specific arrangement of spaces.



Feature Colour

Opportunity to use childrens' play, artist installations and customising off the shelf and/or inbuilt items as vehicles to introduce colour into the village. Colour can be localised to differentiate the main street and provide variation to key spaces, or alternatively pulled through the village centre to give a consistency throughout.



Figure 28. Overall Furniture Palette (image source: Outlines Landscape Architecture)

Plantings

Key Principles

- Strong boulevards and striking features.
- Increased greening across village centre providing colour and textural interest.

Suggested species

Trees

1. *Robinia pseudoacacia*
2. *Gleditsia triacanthos* 'Sunburst'
3. *Ulmus parvifolia*
4. *Fraxinus griffithii*
5. *Eucalyptus torquata*
6. *Lagerstroemia indica*
7. *Eucalyptus* 'Little Spotty'
8. *Tristanopsis laurina*
9. *Angophora hispida*

Shrubs and grasses

10. *Rhoeo spathacea* 'Nana'
11. *Brachyscome multifida*
12. *Argyranthemum* sp.
13. *Atriplex cinerea*
14. *Correa glabra*
15. *Lomandra* 'Lime Tuff'
16. *Myoporum parvifolium*
17. *Dianella* 'Little Jess'
18. *Lomandra filiformis*
19. *Lomandra* 'Tanika'
20. *Eremophila* 'Silver Ball'
21. *Dietes grandiflora*
22. *Hibbertia scandens*
23. *Senecio scandens*
24. *Lomandra fluviatilis* 'Shara'
25. *Lomandra hystrix* 'Tropic Cascade'
26. *Westringia fruticosa* 'Grey Box'
27. *Westringia fruticosa* 'Mundi'



Figure 29. Overall Planting Palette



Design Guidelines

Principles

In developing the design guidelines key considerations include:

- Maintaining the low scale character of the Village
- Maintaining the intimate character of the laneways
- Minimising overshadowing of new and existing public open spaces.

Objectives

Public and private sector investment should deliver the following objectives and meet design guidelines:

- To ensure the height and setbacks of development maintain and enhance the established streetscape and traditional, low-rise, high street character of Belle Vue Avenue and the compact intimate character of the laneways.
- To achieve excellence in design and high quality, contemporary architecture that efficiently utilises the site, enhances the character of the Village and enhances the public realm.
- To ensure active uses are provided at ground level and contribute to vibrant street activity
- To ensure building design embodies the highest environmental standards, adopts innovative ways to achieve ecologically sustainable outcomes, and achieves the highest level of durability, robustness and adaptability throughout the building life cycle.

Guidelines

Building Design

- Provide active uses on frontages identified in the overall framework plan.
- Blank walls are discouraged on facades fronting public areas. Where blank walls cannot be avoided, windows or openings should be provided to promote interaction with the street. If windows or openings are not appropriate other design treatments may be considered to break up the surface and provide visual interest and amenity such as mural painting or vertical gardens.
- Encourage a high level of facade articulation and design treatments that:
 - Maintain the fine grain shop front patterns and consistent vertical proportions;
 - Distinguish recessed upper level built form from the main building facade;
 - Provide variation to surface alignment and materials.
- Encourage architectural detailing that accentuates the prominent corners at key intersections.
- Ensure that the built form minimises impacts on solar access to balconies of adjoining buildings and sites.
- Utilise construction materials and products that incorporate low-embodied energy, are robust and weather resilient and are low maintenance to maximise their life cycle.
- Maximise passive opportunities for energy efficiency in building siting and design.
- Balconies and upper floor setbacks should be provided to reduce the visual bulk and dominance of development.
- Direct connection should be established at grade to usable spaces within ground level tenancies, with level transitions contained within the building envelope. A variety of opportunities exist to ensure good activation in SBO areas. These include internal transitions in floor levels to building entries, as well as some elevated active edges against the street boundary with softening vegetation at the public realm interface.
- Ensure continuity of ground floor activity along streets and laneways.
- Encourage new buildings to provide direct pedestrian access from both the street and from on-site car parking areas.
- Encourage the incorporation of weather protection for pedestrians along street frontages in the form of verandas, awnings or canopies over the footpath, maintaining a minimum clearance of 3.5m above the footpath level.
- Encourage residential premises at the upper levels to address the street.
- Encourage residential premises at the upper levels to address the public open space with recessed balconies, where west facing.
- Encourage lot consolidation to reduce the impact of potential vehicle crossings on Belle Vue Arcade west and Village Walk -south.
- Upper level development where no vehicle access is possible (such as the block on the south western corner of Belle Vue Ave and Barrabool Rd) could be considered for non residential uses. Encourage shared car schemes with appropriate car parking set aside.

Building Height and Setbacks

- Buildings should be constructed to the property boundary unless the setback is:
 - Designed as part of the public domain; and
 - fully accessible to the public.
- Development above the street wall height should comply with the preferred minimum upper storey setbacks identified in the Setback plan (fig 31).
- The setback for development above the street wall height is measured from the face of the building along the primary street frontage (fig 30).
- A preferred maximum development of up to three storeys is suggested on development fronting main streets, as shown in Figure 30 and 31.
- A preferred maximum development of up to two storeys is suggested on development fronting laneways, as shown in Figure 30 and 31.
- A preferred maximum developments of up to four storeys is suggested on development fronting public open space, as shown in Figure 30 and 31.

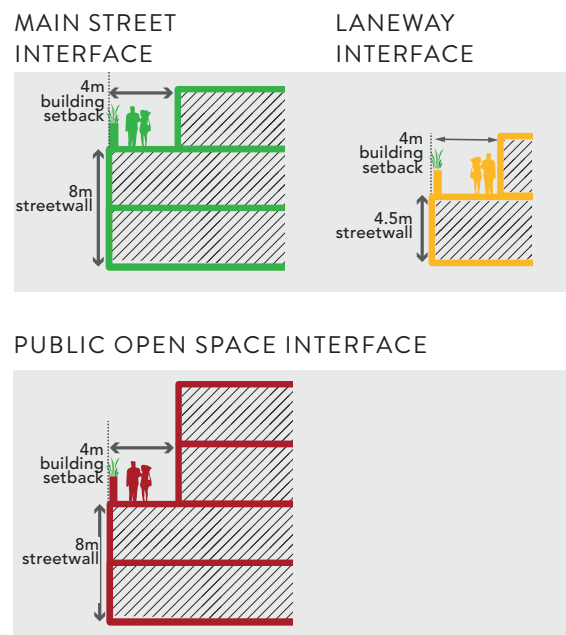


Figure 30. Building height and setbacks







Street/Open Space Setbacks

- At the property boundary fronting Belle Vue Avenue, Barrabool Road, Taylor Court, Highton Reserve and Roslyn Road a building can be constructed to a maximum street wall height of 8m (two storeys).
- Buildings should be setback 4m above a height of 8m.
- The setback for development above the street wall height is measured from the face of the building along the side street frontage.
- An application to reduce the preferred upper storey setbacks must demonstrate how the relevant design objectives have been met.
- Balconies within an upper level setback must not house airconditioning or plant equipment.



Figure 31. Street wall, building heights and setbacks plan

KEY

-  Main street interface: 8m street wall height, with 4m building setback of upper level. (3 storey preferred maximum height). No vehicle crossovers.
-  Possible vehicle crossover. Lot consolidation may be required.
-  Laneway interface: 4.5m street wall height, with 4m building setback of upper level. (2 storey preferred maximum height). No vehicle crossovers.
-  Possible vehicle crossover with lot consolidation
-  Public open space interface: 8m street wall height, with 4m building setback of upper two level. (4 storey preferred maximum height)
-  Future development of this site is dependent on the outcome of Library review.

Amenity

- Encourage development that minimise the impacts of overshadowing and overlooking on the amenity of adjoining residential properties and habitable room windows.
- Ensure building layout and design respect the privacy of adjacent dwellings. Ensure internal amenity such as cross ventilation and solar access are not compromised for both the proposed development as well as adjacent properties.

Public Realm

- New parking areas, drainage and streetscape upgrades should include water sensitive urban design treatments for storm water.
- Parking associated with residential development should be integrated with the development.
- Landscaping that contributes to the greenery of the Village should be provided in new developments.
- Soft landscaping should be provided on balconies and roof tops to limit overlooking and soften built form.
- Streetscape upgrades should incorporate art and cultural references that reflect the history of the local area.

Advertising Signage

- New signage should not dominate the streetscape and detract from the local village character.
- Signage should be located below verandahs or incorporated into the building facade in an innovate way.
- Signage on the back or side walls of buildings is discouraged, particularly at upper levels.
- The design and colours of signage should respect the local character of the Village. Animated and electronic signage should be avoided.

Development Sites

1 19 Belle Vue Avenue

The Council car park site on Belle Vue Avenue offers a long-term opportunity to provide a 'book end' development to the existing shops. New development should address both Belle Vue Avenue and the car park and house a mix of uses such as community uses, retail, office and residential above.

A larger proposal was initially discussed with the community. This included a mixed-use development which sliced a multi-level car park. This proposal aimed to increase car parking within the centre and provide new opportunities for retail development as identified in the Geelong Retail Strategy. This proposal was not supported by the community or Trader group due to concern over the perceived impact on the character of the Village. The final concept shows a compromise smaller scale development.

2 12 Belle Vue Avenue

This site is a substantial holding within the Village, however it isn't well integrated with the centre and does not reflect the fine grain character of the rest of the Village. If the existing supermarket is redeveloped at any time in the future it offers the opportunity to better integrate with rest of the centre and provide residential development at the upper floors that will not impact on the low scale character of Belle Vue Avenue. Redevelopment of the car park site is dependent on the Library review in 5 years time.

The redevelopment of the supermarket site must consider the following design requirements:

- Respect the low scale character of the Village by providing development up to four storeys with recessed upper floor levels
- Provide an active frontage on any development fronting Belle Vue Avenue
- Provide an active frontage on key pedestrian desire lines such as major car parking areas to Village Walk
- Sleeve car parking with active uses to provide an attractive interface to public spaces
- Upper floors should be oriented to provide passive surveillance of surrounding public spaces
- Avoid vehicle crossing on to Belle Vue Avenue

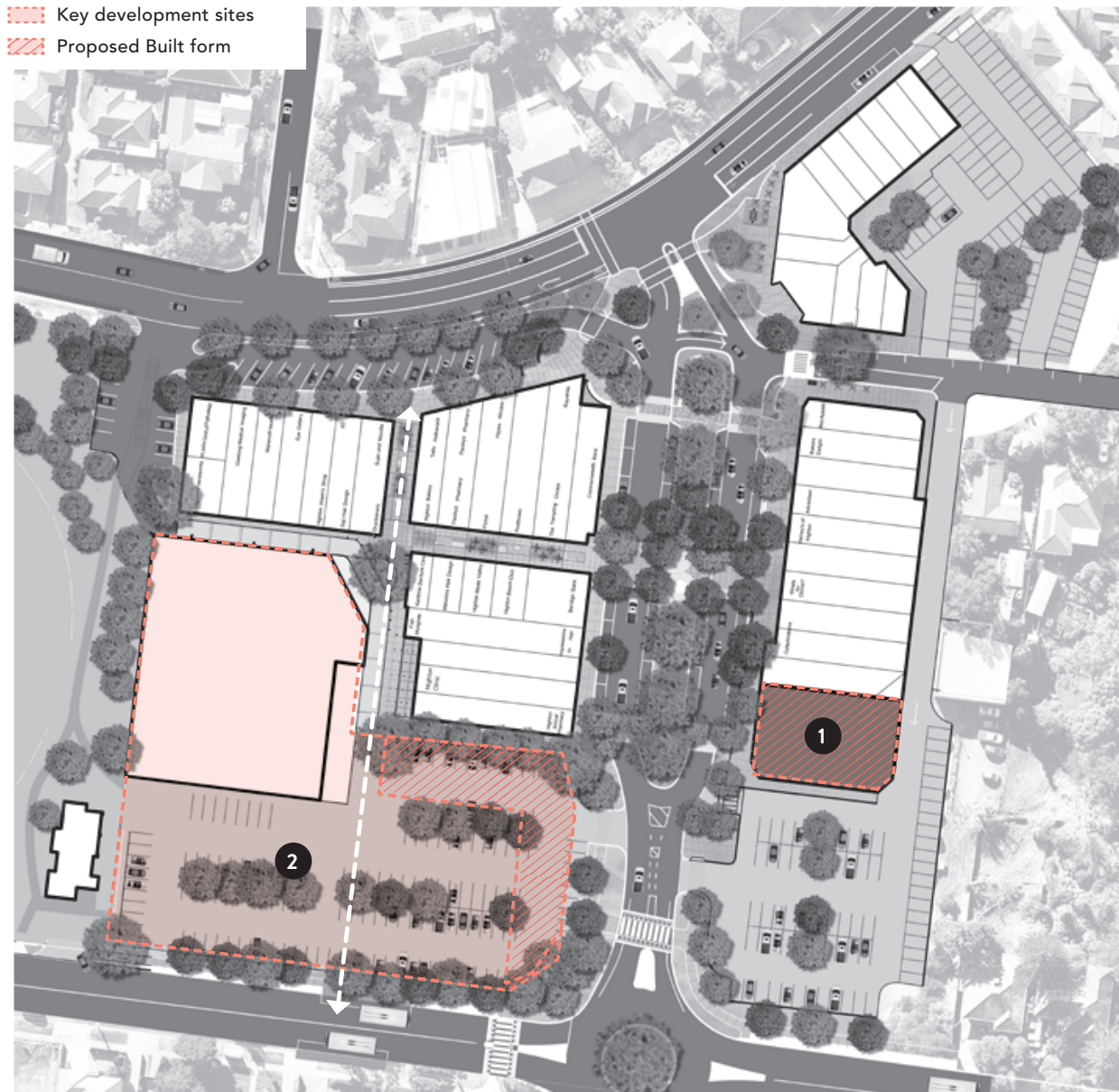


Figure 32. Key Redevelopment Sites



05

MOVEMENT AND ACCESS

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Objectives

Objectives

The following have been identified as important movement and access objectives for the Village:

- Ensure high pedestrian amenity within the commercial core.
- Increase the attractiveness and safety of key pedestrian thoroughfares.
- Ensure parking supply increases commensurate with demand.
- Provide short term parking for customers and long term parking for staff within a short, safe walk.
- Continue to recognise the arterial functionality of Barrabool Road and Roslyn Road and maintain efficient movement of vehicular traffic along these roads.
- Maintain capacity for moderate traffic volumes along Belle Vue Avenue but with low vehicle speeds and pedestrian priority crossing opportunities.
- Ensure service vehicle access and parking needs are accommodated for local businesses.
- Manage car parking intrusion into residential areas.
- Promote sustainable transport choices through high quality infrastructure provision.
- Maximise the convenience and safety of public transport services.
- Provide highest level cycle facilities on the Principle Bike Network corridors (Barrabool Road, Roslyn Road).
- Improve cyclist amenity with improved on-road cycle routes, bicycle treatments at intersections and parking opportunities.
- Encourage the redevelopment / reuse of existing buildings and waiver car parking in appropriate instances with respect to proximity to amenities.
- Redevelopment of public parking facilities should provide replacement spaces within the development or nearby within a short walk.

Road Network

Barrabool Road/Belle Vue Avenue Intersection

An intersection upgrade at Barrabool Road is proposed to maintain through traffic capacity and increase safety performance for all road users.

The overall streetscape concept plan defines a more conventional intersection geometry, with key aspects as follows:

- Line marking for one through lane and one right turn lane on the west approach, enabling the right turn to be partially or fully controlled (right turn arrow signal phase).
- On-road cycle lanes.
- Providing stop line marking on the Barrabool Road west approach to cater for right turn into the Service Road.
- Restrict Taylor Court movements to left in / left out.
- Restrict south bound entry to Belle Vue Ave to one lane.

Note: The final intersection design, as shown in the Streetscape Concept Plan was developed in conjunction with Movement and Place following Trader engagement in 2021. Movement and Place reviewed 4 options which included right/ left turn or left turn only out of Taylor Court, retention or removal of the service lane access from Belle Vue Ave and alternatives for one or two south bound lanes into Belle Vue Ave from Barrabool Road. It was concluded that the option shown in the final Streetscape Concept Plan was the highest performing of the options and safest outcome for all road users.

Barrabool Road Service Road

- Line marking the Service Road with an alternative cross section that places 60 degree angle parking along the southern kerb would increase pedestrian safety by negating the need for any pedestrian movements across the Service Road. It also provides an opportunity to relocate the southern kerb to increase the footpath width in some sections.
- Minimal loss of parking spaces (from the Barrabool Road signalised intersection upgrade).
- At the end of the Service Road, an improved pedestrian connection across Belle Vue Arcade should be provided.

Parking

Capacity

Within Highton Village there is a total of 604 car parking spaces incorporating:

- 355 off-street spaces
- 114 on-street spaces (adjacent commercial land use)
- 135 on-street spaces (adjacent residential land use)

Restrictions

Parking restrictions are consistent with best practice for short term (high turnover) parking within the core of an activity centre and longer term parking around the periphery. The parking restrictions are shown in Figure 29.

Occupancy

Parking areas within the core of Highton Village are currently experiencing high occupancy at peak times. Detailed surveys have been undertaken of parking occupancy (June 2016). The peak occupancy of over 90% was observed at 10am on the surveyed Friday and 11am on the Saturday.

There are also currently relatively low parking occupancies in areas south of the retail core.

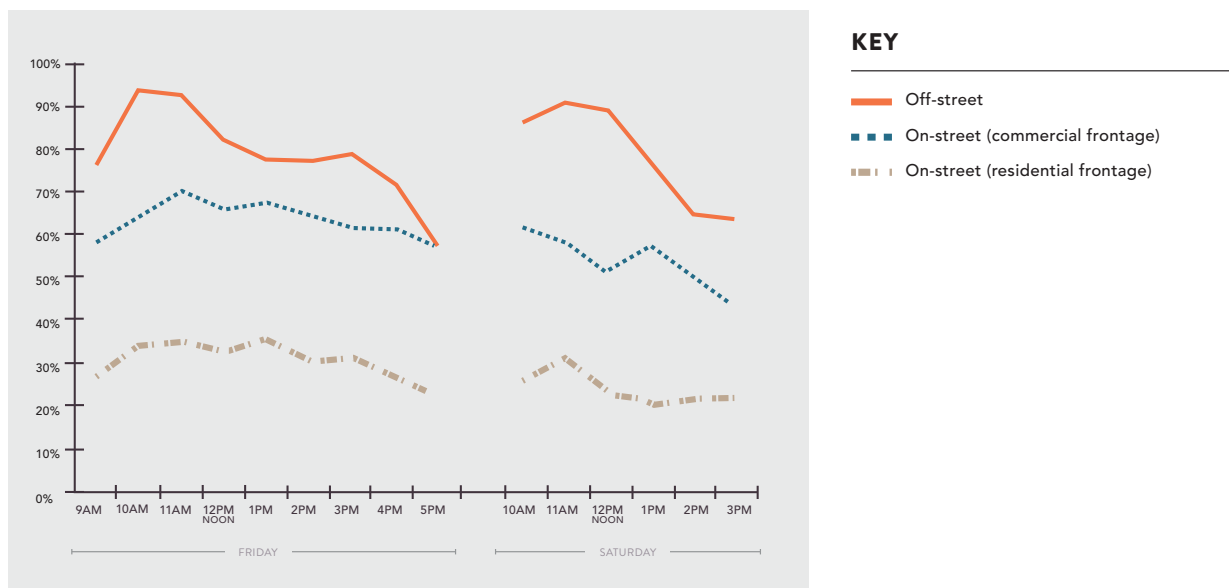


Figure 33. Car Parking Occupancy by Time of the Day



Figure 34. Car parking occupancy by time of the day



KEY

- All Day
- 1 Hour
- 2 Hour
- Restricted Zone
- Disabled Parking

Notes:

1-Hour (area) restrictions typically applicable 9am-5pm Mon-Sat.

2-Hour (area) restrictions typically applicable 9am-5pm Mon-Fri and 9am-12noon Sat.

Parking

Future Supply

South of the Roslyn Road roundabout, Belle Vue Avenue has a drainage pipe easement along its east side, which provides an opportunity to install additional on-street parking spaces, increasing parking provision in this area. These parking spaces would be suitable for long term (staff) parking. Increased staff parking here would free up capacity within the more convenient short term parking facilities. The design could incorporate a bike lane, separation lane and 90 degree spaces within the existing road reserve. An abutting footpath would be within the drainage easement. There is a mature stand of trees within this area which must be protected. A full tree impact assessment must be undertaken at the conceptual stage to ensure protection of trees.

The Council Car Park currently provides 91 formal and 10 informal (gravel area) spaces. The parking configuration efficiently uses available space so opportunities to increase supply within the available area are unlikely. The potential development site shown in the concept plan will result in a loss of approximately 22 car spaces. Long term reconfiguration of the car park is recommended to allow continued access for delivery vehicles if the development site is realised.

Parking Management

Village parking should continue to be managed with short term time restrictions in areas of highest need to increase space turnover. Opportunities include:

- Converting the Barrabool Road Service Road to 1- hour parking.
- Providing some 15/30 minute spaces within Belle Vue Avenue.
- Increasing the extents of timed restrictions with 2-hour parking along Roslyn Road and/or Barrabool Road.
- Limit the provision of free all day parking to locations around the periphery, to balance the needs of workers and residents.
- Increase enforcement activities to discourage misuse of public parking facilities.

It is recommended that a Parking Plan is developed for Highton Village to maximise utilisation of existing parking provision by ensuring that parking controls are tailored to the needs of local businesses and their customers.

Other

Road Safety

- Implement a 30kph speed limit within the Village core to enhance pedestrian and cyclist amenity and safety (Belle Vue Avenue, Taylor Court, Barrabool Road Service Road). Noting that vehicular travel times will not be significantly impacted in these small road lengths.
- Review speed limits along Roslyn Road with a desire to reduce speeds in the Village (eg. 50kph /40kph). Given its sub-arterial function, such a review consider its entire length in collaboration with VicRoads.

Wayfinding

- Review existing pedestrian / cyclist wayfinding signage to determine any need for a new network of wayfinding signage.
- Ensure safe design principles (crime prevention) are incorporated into streetscape projects (particularly Village Walk and Belle Vue Arcade), new car parks and developments.

Public Transport

- Ensure bus stops provide a high standard of amenity (ie. seating, shelter, lighting, flat surfaces).
- Retain bus stops on Roslyn Road. The relocation of stops into Belle Vue Avenue is not recommended given the walk distance benefits are relatively minor and unlikely to outweigh the travel time disadvantages to the bus route.

Taxis

- Provide shelter for the Taxi Zone as part of the streetscape upgrade.

06

IMPLEMENTATION

Implementation

This section identifies the actions to be undertaken to implement the policies and projects established in the UDF.

The following table outlines short, medium and long term implementation milestones to be achieved over the next 10+ years. It is important to note that various council departments and government agencies will influence each of the initiatives. However the principles outlined in the UDF will be used as the overarching guide with a focus on high pedestrian amenity within the commercial core.

Table 3. Implementation Actions

PLANNING	PRIORITY Short Term: 1-5yrs Medium Term 5-10yrs Long Term 10+yrs	RESPONSIBILITY	FUNDING SOURCE
Council adopts UDF	Short Term	Urban Design and Heritage	Not applicable
Prepare an amendment to the Greater Geelong Planning Scheme to apply a new Design and Development Overlay Schedule to implement the built form objectives of the UDF	Short Term	Strategic Implementation	Existing budget
Prepare detailed streetscape design plans for: - Belle Vue Avenue - Village Walk and Belle Vue Arcade - Service Lane - Barrabool Road parking	Short Term	Capital projects/ Engineering Services/ Urban Design and Heritage	Council budget bid
Parking Precinct Plan	Short Term	Capital projects/ Engineering Services/ Urban Design and Heritage	Council budget bid
Work with Regional Roads Victoria to prepare detailed design for Barrabool Road intersection upgrade, including a dedicated right turn from Barrabool into Belle Vue	Short Term	Capital projects/ Engineering Services/ Urban Design and Heritage/ Regional Roads Victoria	Council budget bid Regional Roads Victoria
Develop wayfinding and public art opportunities	Short Term	Capital projects/Arts and Culture	Council budget bid & Grant Applications
Work with the Highton Village Traders Association to promote the Village as a unique destination and host events	Ongoing	Economy, Place & Events Traders Association	N/A
Undertake feasibility study on mixed use development on council car park site	Medium Term	Planning Delivery	Council budget bid

Table 4. Infrastructure Actions

INFRASTRUCTURE	PRIORITY Short Term: 1-5yrs Medium Term 5-10yrs Long Term 10+yrs	RESPONSIBILITY	FUNDING SOURCE
1. Streetscape works – Belle Vue Avenue	Short term	Capital projects/ Engineering Services	Capital funding bid
2a. Streetscape works - Laneways	Short term	Capital projects/ Engineering Services	Budgeted
2b. Streetscape works – Service Lane	Short term	Capital projects/ Engineering Services	Budgeted
3. Streetscape works – Barrabool Road parking	Short term	Capital projects/ Engineering Services	Capital funding bid
Belle Vue Avenue South additional parking	Medium term	Capital projects/ Engineering Services	Capital funding bid
Install public Wi-Fi in the Village	Short term	Engineering Services	Capital funding bid

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Highton Interim Final Urban Design Framework

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Introduction










The City of Greater Geelong (The City) released the Interim Final Highton UDF for public comment on the 12th of July 2021. Following this the Highton community was given a six week period to provide feedback if desired. The City has since assessed the feedback received and determined areas of concerns and support for the UDF. This report outlines the findings from the public consultation which will inform the final Highton UDF.

As part of the Highton UDF the Concept Plan was available on the Have your Say Page during the consultation period (12th of July to 20th of August 2021) for public comment.

KEY

- 1 Existing Field
- 2 Belle Vue Ave. Reconfiguration - Refer to Following Detail Concept
- 3 Laneway Upgrade - Refer to Following Detail Concept
- 4 Service Lane Upgrade
- 5 Barrabool Service Rd Car Park Reconfiguration
- 6 Proposed Relocation of Supermarket Loading Bay
- 7 Proposed Long Term Removal of Existing Library
- 8 Development Site To Include Retail, Office, Residential, Community Uses
- 9 Supermarket Extension
- 10 Potential For Longer Term on Street Parking
- 11 Improve Pedestrian Connections
- 12 Additional Parking. *Access Point to Rosalyn Rd to be Resolved
- 13 Improved Pedestrian Connection and Feature Fence
- 14 Barrabool Rd Bike Lanes & Intersection Upgrade

NOTE: All potential building modifications or redevelopments are subject to coordination with owners

-  Existing trees (dotted centre)
-  Proposed trees (crossed centre)
-  Garden bed
-  New pavement - e.g. concrete with saw cuts to help differentiate space
-  New furniture and fixtures - e.g. lighting, bollards, bins, incorporated seating, benches and bike racks
-  Pedestrian crossings
-  Future opportunity for building modifications/ additions
-  Loading bay
-  Opportunities for local artists engagement
 - murals for blank building facades - art piece, history or aid in wayfinding
 - wayfinding signage
 - pavement 'tapestry' within laneways and in areas of belle vue avenue
 - childrens play elements weaved into the public



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Previous community and stakeholder engagements

The development of Highton’s Urban design Framework has been ongoing since 2016. Throughout the process there has been multiple opportunities for community engagement. We have now completed the final phase of community and stakeholder engagement.

We are here

- | | | |
|-----------|---|-----------------------|
| 01 | Project set up and scope | - Commences 2016 |
| 02 | Technical reports and referrals | - 2016 |
| 03 | Phase 1 - Community engagement | - 2016 |
| 04 | Draft Highton Urban Design Framework | - 2017 |
| 05 | Phase 2 - Community engagement on Draft UDF | - 2017 |
| 06 | Phase 3 - Targeted consultation with Traders | - 2018 |
| 07 | Phase 4 - Targeted consultation with Traders | - 2021 |
| 08 | Interim Final Highton UDF Plan | - 2021 |
| 09 | Phase 5 - Community engagement on the Interim Final UDF | 12 Jul to 20 Aug 2021 |
| 10 | Council to consider endorsing the Final Highton UDF | - Oct 2022 |
| 11 | If endorsed work will commence for Streetscape and laneway upgrades | - 2023 onwards |

Community Engagement and Consultation



Community Workshops, November 2016



Trader Group Workshops, May 2021

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Phase 5: Community Engagement Process

A mailout was sent to every property and trader in the study area on 10 July 2021. It explained how to read the current Urban Design Framework and the ways in which the community could provide feedback. A period of six weeks was given for those wishing to provide comment on the UDF.



What we heard: Elements of the UDF that have community support

- | | |
|--|--|
| <p>35 Support pavement renewal, minor upgrades to Belle Vue Ave and upgrades to laneways.</p> | <p>Local Trader:
 <i>"beautification of the walkway, retention of trees and pavement works would greatly improve the look and safety of pedestrians"</i></p> |
| <p>10 Support Mixed-use development which includes community uses, such as library.</p> | <p>Community Member:
 <i>"The center is becoming 'run down' and needs to be upgraded."</i></p> |
| <p>12 Support traffic and pedestrian changes.</p> | <p>Community Member:
 <i>"Changes to improve traffic flow would be very welcome and it's great to see planned upgrades to footpaths, lighting and the library."</i></p> |
| <p>4 Support four storey development within the village.</p> | <p>Highton Property Owner:
 <i>"There is so much potential in the Highton Village and I look forward to the developments."</i></p> |
| <p>3 Support reduction in car parking and focus on public transport</p> | <p>Highton Property Owner:
 <i>"We need to make it more welcoming for pedestrians & bikes."</i></p> |
| <p>3 Support Flashing Pelican crossing</p> | <p>Highton Property Owner:
 <i>"The suggestion of flashing pedestrian lights is welcomed, acknowledging that this will reduce vehicle flow in the area."</i></p> |

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What we heard: Elements of the UDF that needs improvement

52

Do not support removal of service lane to Barrabool Road to carpark.

Community Member:

"Please don't remove the left slip road from Bellevue to Barrabool. No one will use that space. Better for the traders and vehicle movements."

42

Do not support roundabout changes, Taylor Court intersection changes. Changes to Taylor Court.

Local Trader:

"The new plan to change the roads would be a blow to our community and would be an unnecessary cost"

34

Do not support the multi-use development due to the removal of car parking.

Community Member:

"The proposed development at the Council Car Park should be taken off the books, full stop."

33

Do not support two additional pedestrian crossings.

Community Member:

"Increasing pedestrian crossings will not only contribute to the above traffic concerns, but also poses a number of safety concerns."

9

Do not support alfresco dining.

Local Trader:

"I have never had a customer complain that they had to go elsewhere because of lack of parking, lack of alfresco dining, or poor traffic flow."

8

Do not support high-rise development in Highton village.

Community Member:

"We do not want a huge mess of retail high rise buildings."

8

Do not support works which will close / impact businesses.

Local Trader:

"Businesses have brutally suffered enough due to Covid-19, we need to be supporting the best interests of the community."

4

Do not support library building being removed.

Community Member:

"The library should not be removed, even with some features transferred to the Senior Citizens Centre as suggested."

3

Do not support loading dock, particularly the intersection with Barrabool Road.

Community Member:

"Woolworth Trucks entering or exiting Barrabool Road is very questionable."

3

Do not support removal of the clock tower.

Local Trader:

"The clock tower needs to be restored and maintained."

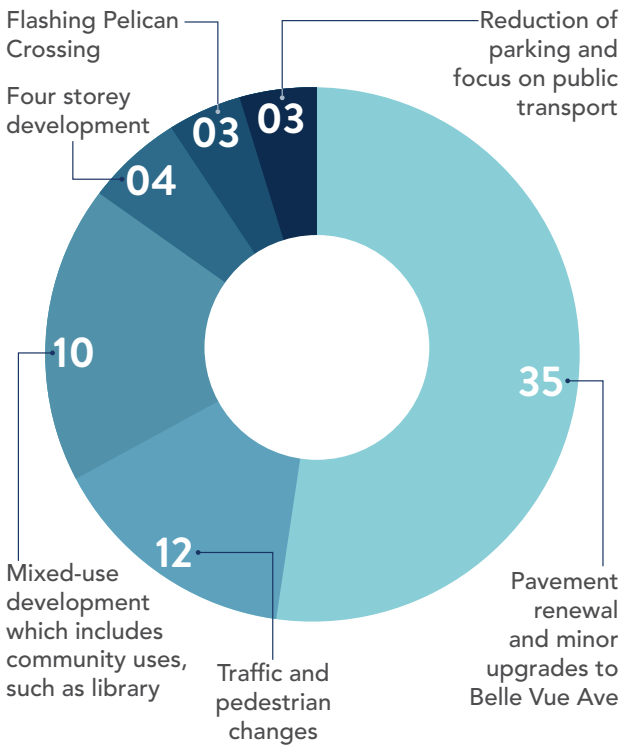
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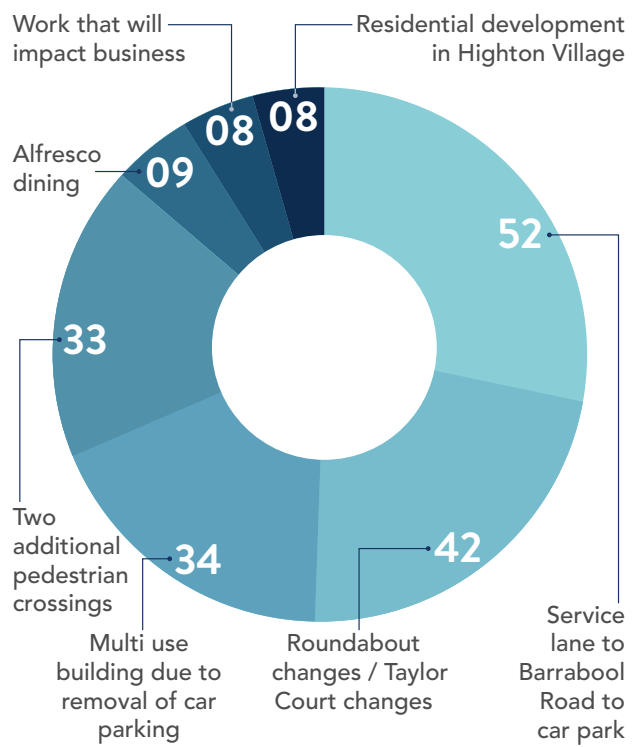
Summary of Public Feedback...

In general there was support for improvements to the Highton streetscape and public realm but concerns over changes to road layouts and car parking.

Support for:



Lack of support for:



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Response to Public Concerns

Service lane to Barrabool Road to car park

This concern has been addressed by retaining the existing service lane access from Belle Vue Road in the plan.

Roundabout changes / Taylor Court changes

As a result of concerns regarding the design of this intersection, we engaged Movement and Place Consulting to review and evaluate the intersection design. Movement and Place Consultants found that the proximities of existing intersections and their configuration creates a concentration of vehicle crossing and merging points in a relatively small area. The existing design leads to high vehicle speeds, and is an unsafe environment for pedestrians. Various intersection concepts were tested by Movement and Place. It was determined that the option presented in the UDF was preferred as it encourages appropriate vehicle speed, pedestrian and vehicle safety, ease of movement for both pedestrians and vehicles, has limited impact on travel times for all road users and improves public realm and amenity.

Multi use building due to removal of car parking

A feasibility study for this proposal is recommended in the UDF. From an urban design perspective, the suggested multi-use development bookends existing development and provides an active frontage to the carpark. The development would result in a loss of approximately 22 spaces.

Two additional pedestrian crossings

After consideration it was determined that additional crossings promote pedestrian activity on the street, encourage visitors to

shop at various destinations and reduce vehicle speeds which makes the street safer for all road users. This aligns with the vision developed by community at the outset of the UDF process.

Alfresco dining

The concerns raised over provision of alfresco dining in the village were analysed in detail. It was derived that no further additions be made, but instead only proposed an increase in capacities of existing alfresco dining. This is deemed appropriate to balance the needs of business and promote an active village feel.

High-rise development

To cater to exponentially increasing demands of density and use, building heights of 3 storeys on main streets, 2 storeys on laneways, and 4 storeys on public open space/ car parking are not considered high rise. The building heights and setbacks identified in the UDF respect the existing low scale-built form of the village and public realm interfaces.

Works which impact businesses

It is acknowledged that construction works may impact businesses in the short term. The long-term benefits of public realm improvements and potential extra visitation to the center weighs towards a higher benefit.

Library moved

This concern has been addressed by retaining the library in its current position and reviewing its long-term future options in 5 years.

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Summary of changes to the UDF following consultation

Changes to the UDF have been made after careful consideration of community feedback, further consultation with Department of Transport and an additional Movement and Place traffic assessment.

Changes from the Interim Final to Final Urban Design Framework include

- The retention of the service lane access from Belle Vue Avenue as a result of the community consultation.
- The removal of proposed loading dock off Barrabool Road as a result of consultation with Department of Transport.
- The addition of strategic cycle corridors along Barrabool Road incorporated as result of consultation with Department of Transport.
- A new pedestrian crossing design and a redesign of the entrance/ exit to the carpark east of Belle Vue Avenue has been incorporated following traffic assessment.
- The library has been retained next to Woolworths car park in response to community feedback.

The changes made to the Urban Design Framework remain in keeping with the vision where-by Highton Village is and will be the heart of the community with a unique, vibrant, leafy feel. High quality public spaces will encourage visitors to linger longer and promote community interaction. The movement network and design of buildings will provide for a range of activities and prioritise the pedestrian experience.